



December 2020

# Sociocultural Effects Evaluation

SR 826/Palmetto Expressway  
From South of NW 36<sup>th</sup> Street (MP 8.355) to North of NW 154<sup>th</sup> Street (MP 17.950)  
Project Development & Environment Study  
Miami-Dade County, Florida

Financial Management Numbers: 447165-1-22-01, 441830-1-22-01, 441831-1-22-01  
Federal Aid Project Number: N/A  
ETDM Number: 14455

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Prepared For:  
Florida Department of Transportation, District VI



December 2020

The environmental review, consultation, and other actions required by applicable federal environmental laws for this project are being, or have been, carried out by FDOT pursuant to 23 USC § 327 and a Memorandum of Understanding dated December 14, 2016, and executed by FHWA and FDOT.

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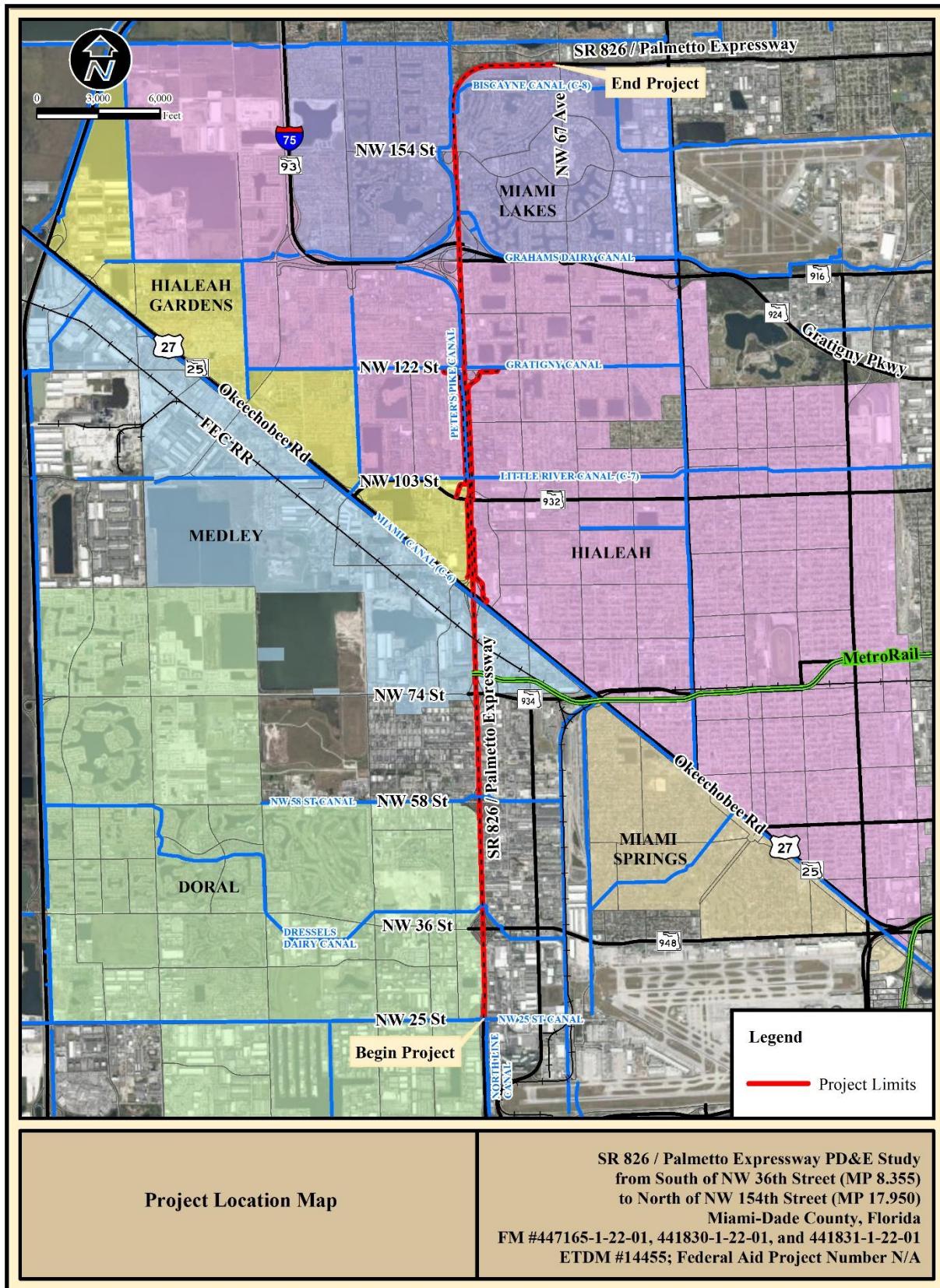
## 1.0 INTRODUCTION

The Florida Department of Transportation (FDOT) District Six is conducting a Project Development and Environment (PD&E) Study, in accordance with the National Environmental Policy Act (NEPA). This roadway project involves the potential addition of general use lanes, conversion of express (managed) lanes to general use lanes, as well as traffic operational and geometrical design improvements on SR 826/Palmetto Expressway (herein referred to as SR 826) from south of NW 36<sup>th</sup> Street (Milepost (MP) 8.355) to north of NW 154<sup>th</sup> Street (MP 17.950) within Miami-Dade County. The improvements are proposed to address existing congestion and higher than expected speed differentials between the general purpose (GP) lanes and the express lanes (EL) along the project corridor, as well as provide additional access to the EL system. The project also includes potential improvements to the frontage road system parallel to and on both sides of SR 826 from US 27/SR 25/Okeechobee Road (herein referred to as US 27) to NW 122<sup>nd</sup> Street.

Connecting population and commercial centers in north-central Miami-Dade County, the project traverses the municipalities of Doral, Medley, Hialeah Gardens, Hialeah, and Miami Lakes, as well as unincorporated portions of the county. SR 826 connects to essential east-west facilities within north-central Miami-Dade County, including SR 836/Dolphin Expressway, SR 948/NW 36<sup>th</sup> Street, SR 934/NW 74<sup>th</sup> Street (herein referred to as NW 74<sup>th</sup> Street), US 27, SR 932/NW 103<sup>rd</sup> Street (herein referred to as NW 103<sup>rd</sup> Street), I-75/SR 93, and SR 924/Gratigny Expressway.

Within the project limits, SR 826 is a principal arterial and consists of six (6) to nine (9) general use lanes and zero (0) to three (3) express (managed) lanes; the typical section varies throughout the project length. In addition, SR 826 is part of the state's emergency evacuation network and is on the National Highway System (NHS), the Strategic Intermodal System (SIS), and the State Highway System (SHS).

The project location map is shown in **Figure 1-1**.



**Figure 1 - 1: Project Location Map**

## **1.1 Purpose and Need**

The purpose of this project is to address various roadway deficiencies causing congestion and large speed differentials between GP lanes and EL along the SR 826 corridor. Proposed improvements are anticipated to increase roadway safety, facilitate the movement of people and goods, and increase the capacity in the GP lanes. Another goal of the project is to improve access to the EL system by relocating an ingress point in northern Miami-Dade County. Additionally, the purpose of the project is to improve the geometry of the expressway frontage road system. The need for the project is based on the following criteria:

### **1.1.1 Roadway Deficiencies: Address Congestion and Speed Differentials**

Following the opening of the SR 826 ELs to traffic in September 2019, additional congestion and higher than expected speed differentials between EL (higher speeds) and GP lanes (lower speeds) were observed in both the northbound and southbound directions during peak travel times. Areas identified for improvement include the NW 103<sup>rd</sup> Street interchange and from NW 74<sup>th</sup> Street to US 27 in the southbound direction. The issues identified in these areas are caused by geometric and operational deficiencies such as the lack of auxiliary lanes, insufficient ramp lengths, and substandard shoulders.

The implementation of operational improvements to correct the identified roadway deficiencies on SR 826 would improve congestion and better align speeds between the EL and GP lanes. According to the Palmetto Express Lanes Modification Summary Report (March 2020), an earlier planning study, the proposed improvements would result in a 56% and 42% reduction in travel times for the southbound and northbound GP lanes, respectively. In addition, throughput would increase a combined 58% and 11% in the southbound and northbound directions, respectively. While speed in the EL is not projected to increase, speeds in the GP lanes are forecasted to rise by 26 miles per hour in the southbound direction and 19 miles per hour in the northbound direction. These metrics illustrate how implementation of the potential improvements would address congestion and speed differentials currently caused by roadway deficiencies.

### **1.1.2 System Linkage: Provide Better Access to the EL System**

Currently the southbound EL lanes begin north of NW 154<sup>th</sup> Street and do not provide southbound ingress for motorists in this area of north-central Miami-Dade County (after NW 67<sup>th</sup> Avenue). To better facilitate the movement of traffic from this area of increased demand into the EL system, the potential project improvements include relocating the existing EL ingress point from north of NW 154<sup>th</sup> Street to south of NW 103<sup>rd</sup> Street. This will create access to the EL system for residents in this portion of Miami-Dade County and create an important linkage to the EL system.

## 2.0 PROJECT DESCRIPTION

The project study area consists of the existing and proposed right-of-way limits for the viable Build Alternative and the No-Build Alternative.

### 2.1 Existing Roadway Conditions

Data gathering focused on the areas of roadway mainline, bridge, frontage road, and environmental characteristics. Assessment of the existing conditions began with the collection and review of data pertaining to the existing facilities which included conducting on-site field inventories, review of existing documents, as well as, review of other pertinent data used for the evaluation of these transportation facilities.

#### 2.1.1 Functional Classification

Within the project limits, SR 826 is functionally classified as an Urban Freeway/Expressway and is part of the NHS, the SIS, and the SHS. It consists of six (6) to nine (9) GP lanes and zero (0) to three (3) ELs.

Connecting population and commercial centers in north-central Miami-Dade County, the project traverses the municipalities of Doral, Medley, Hialeah Gardens, Hialeah, and Miami Lakes, as well as unincorporated portions of the county. SR 826 connects to essential east-west facilities within north-central Miami-Dade County, including SR 836/Dolphin Expressway, SR 948/NW 36<sup>th</sup> Street, NW 74<sup>th</sup> Street, US 27, NW 103<sup>rd</sup> Street, I-75/SR 93, and SR 924/Gratigny Parkway.

In addition, SR 826 is part of the state's emergency evacuation network. As a designated SIS highway corridor, it provides access via NW 74<sup>th</sup> Street (also known as Hialeah Expressway) and surface streets to the Miami Hialeah Florida East Coast (FEC) Railway Intermodal Terminal, a SIS Freight Rail Terminal.

The frontage road system within the project limits is functionally classified as an Urban Local facility. The SR 826 frontage road systems have a Context Classification of C-3C (Suburban Commercial); however, there are some adjacent residential properties.

#### 2.1.2 Access Management

The SR 826 (section 87260000) is designated Access Class 1. The SR 826 frontage roads (sections 87260151, 87260152, 87260298, 87260506, and 87260521) do not have a designated Access Classification.

#### 2.1.3 Typical Sections

##### 2.1.3.1 SR 826 Typical Sections

Under existing conditions, SR 826 varies from six (6) to nine (9) GP lanes and includes up to three (3) ELs throughout the project corridor. The design and posted speeds for this barrier separated

expressway system are 60 mph and 55 mph, respectively. **Table 2-1** depicts the existing typical section characteristics for the project corridor.

**Table 2 - 1: Existing SR 826 Typical Section Characteristics**

Typical Section Element	SR 826 NB	SR 826 SB
Number of Travel Lanes	5	6
Travel Lane Width	11 ft	11 ft
Inside Shoulders Width	Varies 10-12 ft	Varies 3.5-12 ft
Outside Shoulders Width (Bike Lane)	Varies 10-12 ft	Varies 10-12 ft
Median Width	n/a	n/a
Right-of-Way Width	200 ft–270 ft	

In the northbound direction there is one (1) EL from south of US 27 to NW 103<sup>rd</sup> Street. The separate, single lane, I-75 northbound EL system begins north of NW 103<sup>rd</sup> Street on the inside of northbound SR 826. In the northbound direction on SR 826, there are five (5) GP lanes from the begin project to US 27; four (4) GP lanes from US 27 to north of the I-75 interchange; and three (3) GP lanes from north of the I-75 interchange to the end project.

In the southbound direction, there is one (1) EL and three (3) GP lanes from the end project (north of NW 154<sup>th</sup> Street) to the I-75 interchange. Immediately south of the interchange, the I-75 single EL turns into a second EL on SR 826 southbound, both of which extend all the way through the project limits. Also in the southbound direction, there are four (4) GP lanes from south of the I-75 interchange to north of the NW 103<sup>rd</sup> Street interchange; three (3) GP lanes within the NW 103<sup>rd</sup> Street interchange area; and four (4) GP lanes south of NW 103<sup>rd</sup> Street interchange to the begin project (south of NW 36<sup>th</sup> Street).

See **Figure 2-1** for the existing SR 826 typical section in the vicinity of NW 103<sup>rd</sup> Street.

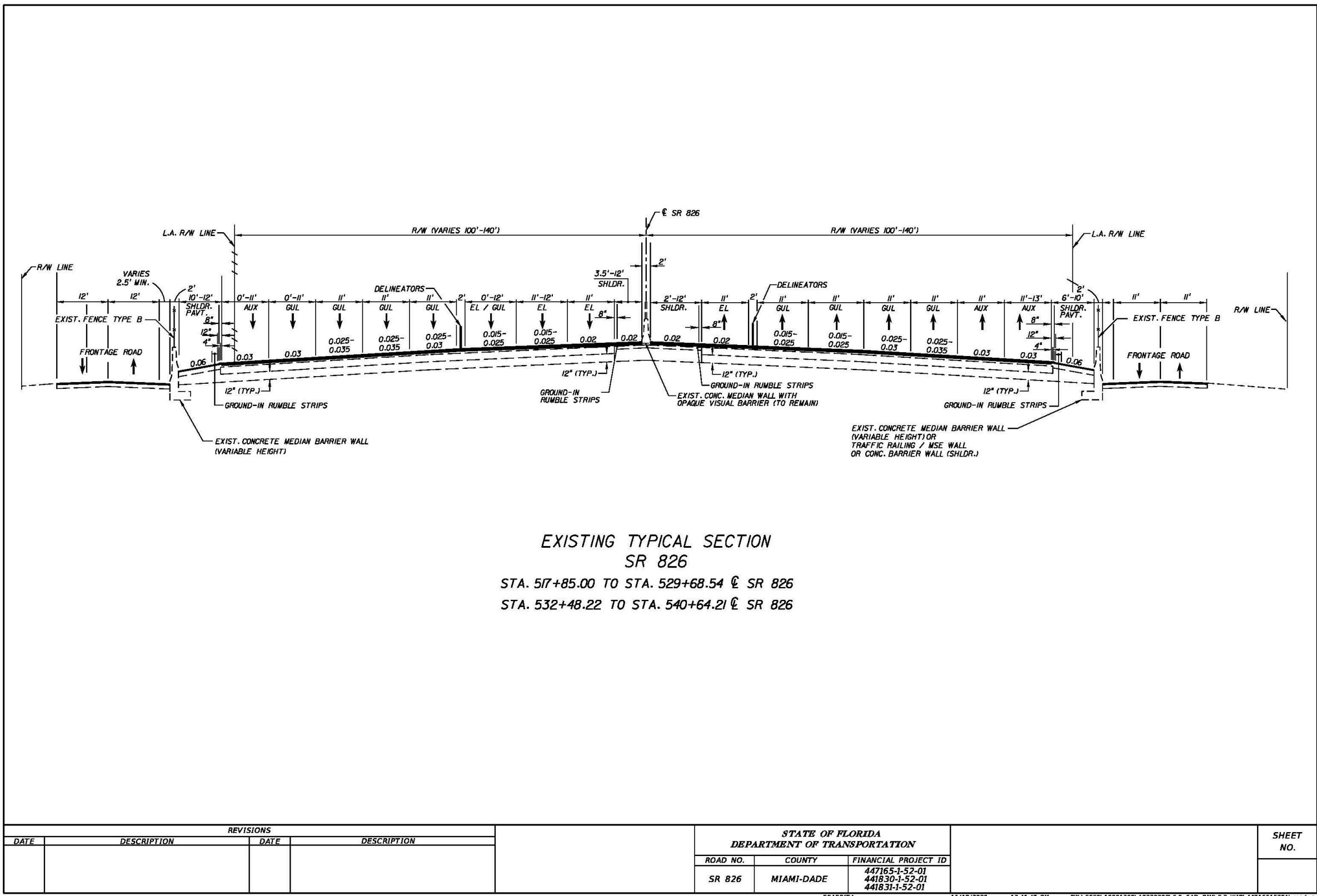


Figure 2 - 1: Existing SR 826 Typical Section

### 2.1.3.2 Frontage Road Typical Sections

Under existing conditions, there is a frontage road system on both sides of SR 826. The frontage road system generally consists of a two-lane undivided typical section with flush shoulders and some segments with one-way undivided or two-way divided typical sections with curb and gutter. The Design Speed varies from 30 mph to 40 mph. The Posted Speed varies from 20 mph to 35 mph.

Within the project limits there are two (2) frontage road systems:

- FPID 441830-1-52-01 – from US 27 to NW 103<sup>rd</sup> Street within the cities of Hialeah and Hialeah Gardens:
  - Section 87260151 (MP 0.000-0.720), W 20<sup>th</sup> Avenue from US 27 to W 44<sup>th</sup> Place.
  - Section 87260152 (MP 0.011-0.715), NW 77<sup>th</sup> Avenue from south of NW 95<sup>th</sup> Street to NW 103<sup>rd</sup> Street.
- FPID 441831-1-52-01 – from NW 103<sup>rd</sup> Street to NW 122<sup>nd</sup> Street within the City of Hialeah:
  - Section 87260298 (MP 0.000-1.391), W 20<sup>th</sup> Avenue East from NW 103<sup>rd</sup> Street/W 49<sup>th</sup> Street to NW 122<sup>nd</sup> Street/W 68<sup>th</sup> Street, including W 67<sup>th</sup> Place from west of W 18<sup>th</sup> Avenue to W 17<sup>th</sup> Court.
  - Section 87260506 (MP 0.000-0.173), NW 77<sup>th</sup> Court from NW 103<sup>rd</sup> Street to south of Little River Canal (C-7).
  - Section 87260521 (MP 0.173-1.342), NW 77<sup>th</sup> Avenue from south of Little River Canal (C-7) to Little River Canal (C-7), W 20<sup>th</sup> Avenue West from Little River Canal (C-7) to NW 122<sup>nd</sup> Street/W 68<sup>th</sup> Street, including W 67<sup>th</sup> Place from west of W 21<sup>st</sup> Court.

**Table 2-2** depicts the existing typical section characteristics for the frontage road systems within the project corridor.

**Table 2 - 2: Existing Frontage Road Typical Section Characteristics**

Typical Section Element	From US 27 to NW 103 <sup>rd</sup> Street (FPID 441830-1-52-01)	From NW 103 <sup>rd</sup> Street to NW 122 <sup>nd</sup> Street (FPID 441831-1-52-01)
Number of Travel Lanes	2	2
Travel Lane Width	Varies 11-12 ft	Varies 11-12 ft
Curb and Gutter	Type F (certain locations)	Type F (certain locations)
Shoulders Width	Varies 0-5 ft paved, 2-6 ft total	Varies 0-5 ft paved, 2-6 ft total
Median Width	n/a	n/a
Sidewalk Width	Varies 5-6 ft on East Frontage Road/W 20 <sup>th</sup> Avenue only	Varies 5-6 ft
Right-of-Way Width	50 ft	46.4 ft-108.9 ft

The following sections (2.1.3.2.1 and 2.1.3.2.2) detail the existing conditions for these two frontage roads where improvements are proposed as part of the viable Build Alternative.

### **2.1.3.2.1 Frontage Roads from US 27 to NW 103rd Street (FM 441830-1)**

Six existing roadway typical sections are identified within the limits of the frontage road system from US 27 to NW 103<sup>rd</sup> Street. See **Figure 2-2** and **Figure 2-3** for the frontage road typical sections.

Existing Roadway Typical Section 1 (Section 87260151), SR 826 East Frontage Road/W 20<sup>th</sup> Avenue from US 27 to south of W 39<sup>th</sup> Street, consists of a two-lane undivided section, with flush unpaved shoulders, and a sidewalk along the right side of the roadway for most of the limits.

Existing Roadway Typical Section 2 (Section 87260151), SR 826 East Frontage Road/W 20<sup>th</sup> Avenue from south of W 39<sup>th</sup> Street to W 41<sup>st</sup> Street, consists of a two-lane undivided section, with flush paved shoulders and a concrete barrier wall on the left side of the roadway adjacent to SR 826 northbound, flush unpaved shoulder and a sidewalk along the right side of the roadway.

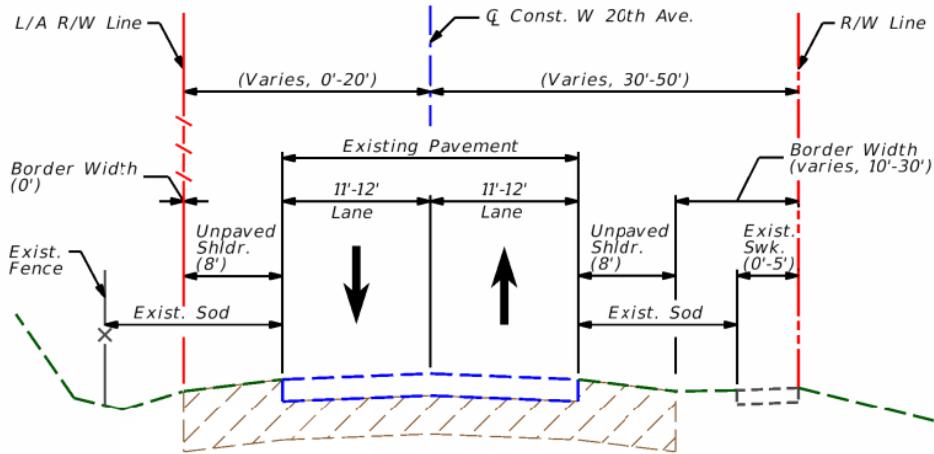
Existing Roadway Typical Section 3 (Section 87260151), SR 826 East Frontage Road/W 20<sup>th</sup> Avenue from W 41<sup>st</sup> Street to W 44<sup>th</sup> Place, consists of a two-lane undivided section, with flush unpaved shoulders and landscaping on both sides, and a sidewalk along the right side of the roadway. The east frontage road system ends prior to NW 103<sup>rd</sup> Street.

Existing Roadway Typical Section 4 (Section 87260152), SR 826 West Frontage Road/NW 77<sup>th</sup> Avenue from the begin project to south of the Walmart Driveway, and from north of NW 95<sup>th</sup> Street to NW 98<sup>th</sup> Street, consists of a two-lane undivided section, with flush unpaved shoulders along both sides of the roadway and a concrete barrier wall on the right side of the roadway adjacent to SR 826 southbound.

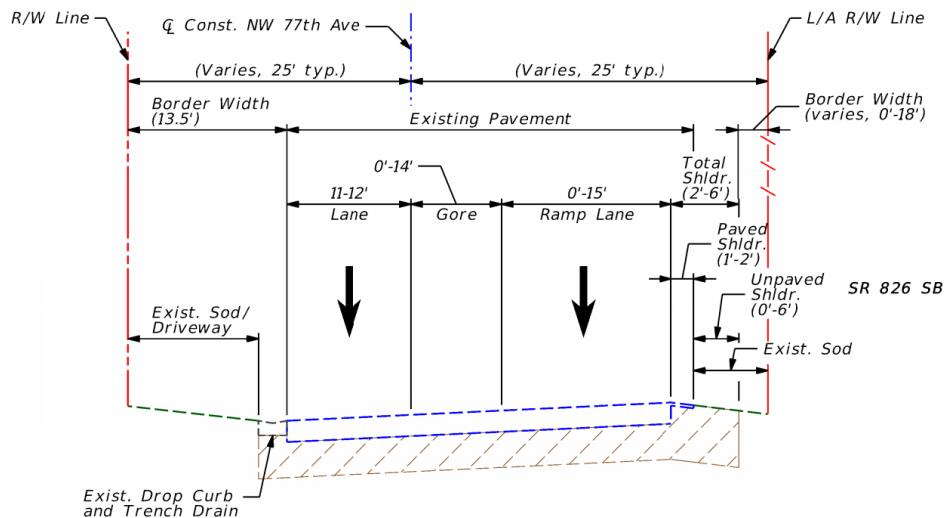
Existing Roadway Typical Section 5 (Section 87260152), SR 826 West Frontage Road/NW 77<sup>th</sup> Avenue from south of the Walmart Driveway to north of NW 95<sup>th</sup> Street, consists of a two-lane undivided section, with flush paved shoulder and a concrete barrier wall on the right side of the roadway adjacent to SR 826 southbound, and flush unpaved shoulder along the left side of the roadway.

Existing Roadway Typical Section 6 (Section 87260152), SR 826 West Frontage Road/NW 77<sup>th</sup> Avenue from NW 98<sup>th</sup> Street to NW 103<sup>rd</sup> Street, consists of a two-lane undivided one-way section with traffic in the southbound direction, with an existing drop curb and trench drain on the left side of the roadway, and a flush paved shoulder along the right side of the roadway, next to the Limited Access right-of-way of SR 826.

**Figure 2 - 2: Existing Frontage Road (Undivided)  
Typical Section 1 (East Frontage Road/W 20<sup>th</sup> Avenue)**



**Figure 2 - 3: Existing Frontage Road (One-Way)  
Typical Section 6 (West Frontage Road/NW 77<sup>th</sup> Avenue)**



#### 2.1.3.2.2 Frontage Roads from NW 103<sup>rd</sup> Street to NW 122<sup>nd</sup> Street (FM 441831-1)

Seven (7) existing roadway typical sections and two (2) existing bridge typical sections are identified within the limits of the frontage road system from NW 103<sup>rd</sup> Street to NW 122<sup>nd</sup> Street. See **Figure 2-4** for the frontage road typical section (applies to both sides of SR 826).

Existing Roadway Typical Section 7 (Section 87260298), SR 826 East Frontage Road/W 20<sup>th</sup> Avenue East from NW 103<sup>rd</sup> Street to south of the Little River Canal (C-7), consists of a two-lane undivided one-way section with traffic in the northbound direction; with a flush shoulder on the left side; and curb and gutter, grass strip, and sidewalk along the right side.

Existing Roadway Typical Section 8 (Section 87260298), SR 826 East Frontage Road/W 20<sup>th</sup> Avenue East from south of the Little River Canal (C-7) to south of W 60<sup>th</sup> Street and from north

of W 60<sup>th</sup> Street to north of W 64<sup>th</sup> Street, consists of a two-lane undivided section, with flush unpaved shoulders on both sides, and a sidewalk along the right side for most of the limits.

Existing Bridge Typical Section 1 (Bridge 870570), SR 826 East Frontage Road/W 20<sup>th</sup> Avenue East over the Little River Canal (C-7), consists of a two-lane undivided section with flush shoulders and post and beam railings on both sides; the right shoulder connects to sidewalks on the bridge approaches.

Existing Roadway Typical Section 9 (Section 87260298), SR 826 East Frontage Road/W 20<sup>th</sup> Avenue East from south of W 60<sup>th</sup> Street to north of W 60<sup>th</sup> Street, consists of a two-lane undivided section with a left-turn lane, with curb and gutter on both sides, and a sidewalk along the right side.

Existing Roadway Typical Section 10 (Section 87260298), SR 826 East Frontage Road/W 67<sup>th</sup> Place from north of W 64<sup>th</sup> Street to NW 122<sup>nd</sup> Street/W 68<sup>th</sup> Street, consists of a two-lane divided section with auxiliary lanes in both directions, a raised median, curb and gutter along both sides, and a sidewalk along the right side.

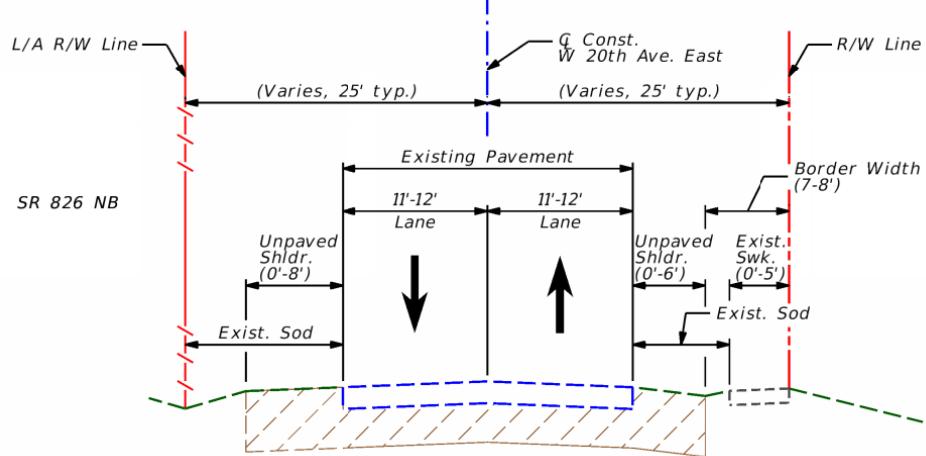
Existing Roadway Typical Section 11 (Section 87260506), SR 826 West Frontage Road/NW 77<sup>th</sup> Court from north of NW 103<sup>rd</sup> Street to south of the Little River Canal (C-7), consists of a two-lane undivided section, curb and gutter along both sides, and a sidewalk along the left side.

Existing Roadway Typical Section 12 (Section 87260521), SR 826 West Frontage Road/W 20<sup>th</sup> Avenue West from south of the Little River Canal (C-7) to south of W 60<sup>th</sup> Street and from north of W 60<sup>th</sup> Street to NW 122<sup>nd</sup> Street/W 68<sup>th</sup> Street, consists of a two-lane undivided section, with flush unpaved shoulders along both sides, guardrail along the right side at the Peter's Pike Canal, and a sidewalk along the left side for most of the limits.

Existing Bridge Typical Section 2 (Bridge 870569), SR 826 West Frontage Road/W 20<sup>th</sup> Avenue West over the Little River Canal (C-7), consists of a two-lane undivided section with wide curb and post and beam railings on both sides.

Existing Roadway Typical Section 13 (Section 87260152), SR 826 West Frontage Road/W 20<sup>th</sup> Avenue West from south of W 60<sup>th</sup> Street to north of W 60<sup>th</sup> Street, consists of a two-lane undivided section with a left-turn lane, with curb and gutter and sidewalk along the left side, and paved shoulder and shoulder barrier along the right side at the Peter's Pike Canal.

**Figure 2 - 4: Existing Frontage Road (Undivided)  
Typical Section 8 (East Frontage Road/W 20<sup>th</sup> Avenue East)**



## 2.1.4 Right-of-Way

Existing right-of-way along the SR 826 corridor ranges from approximately 235 feet to 455 feet or more in width near the existing interchanges. The existing right-of-way along the frontage road systems is typically 50 feet, with a minimum of 46.4 feet on the East Frontage Road/W 20<sup>th</sup> Avenue at W 64<sup>th</sup> Street and maximum of 108.9 feet on the West Frontage Road/NW 77<sup>th</sup> Avenue at NW 122<sup>nd</sup> Street.

## 2.1.5 Pavement Type and Operational Conditions

FDOT performs the annual Pavement Condition Survey (PCS) of the entire SHS to support of the FDOT's Resurfacing Program. The PCS ratings for Crack, Ride, and Rut assess the condition and performance of the pavement as well as to predict future rehabilitation needs. The existing pavement in this segment of SR 826 is in good condition, because the pavement was resurfaced in 2019 by project FPID 432687-1-52-01. The PCS 2020 ratings for the SR 826 mainline (section 8726000 MP 8.355-17.950) are Crack Rating 10, Ride Rating 8.2-8.5, and Rut Rating 10.

The frontage road segments are not included in the annual PCS ratings conducted by the State Materials Office. In 2017, District 6 conducted a pavement evaluation along the frontage road segments, indicating the existing pavement along the frontage roads is in poor condition and warrants resurfacing.

## 3.0 PROJECT ALTERNATIVES

Alternatives evaluated during the PD&E Study include the No-Build Alternative and the Build Alternative as described below. Alternatives were developed and evaluated based on the ability to meet the project purpose and need.

### 3.1 No-Build Alternative

The No-Build Alternative assumes that no proposed improvements from the project would be implemented within the project corridor. It serves as a baseline for comparison against the Build Alternative. It includes on-going construction projects and funded or programmed improvements scheduled to be opened to traffic in the analysis years being considered. These improvements must be part of the FDOT's adopted Five-Year Work Program, Miami-Dade County Transportation Planning Organization (TPO), Long Range Transportation Plan (LRTP), transportation elements of Local Government Comprehensive Plans (LGCP), or developer-funded transportation improvements specified in approved development orders. This alternative is considered to be a viable alternative to serve as a comparison to the study's proposed Build Alternative.

The advantage of the No-Build Alternative is that it requires no expenditure of public funds from the project for design, right-of-way acquisition, construction or utility relocation. In addition, there would be no disruptions due to construction from the project and no direct or indirect impacts to the environment and/or the socio-economic characteristics of the project area. However, the No-Build Alternative does not address the purpose and need of the project and operational and safety conditions within the project area will become progressively worse as traffic volumes continue to increase.

### 3.2 Build Alternative

#### 3.2.1 SR 826

Within the project limits, northbound SR 826 improvements are limited to within the NW 103<sup>rd</sup> Street interchange. The proposed northbound improvements include widening of the mainline at the NW 103<sup>rd</sup> Street interchange to correct the existing substandard inside and outside shoulder widths; widening along the northbound NW 103<sup>rd</sup> Street off-ramp; widening of the northbound bridge over NW 103<sup>rd</sup> Street; and milling and resurfacing of the area.

There are no changes to the typical section along SR 826 northbound other than the provision of wider shoulders within the NW 103<sup>rd</sup> Street interchange. Similarly, the existing northbound EL system will not be modified by this project.

The proposed southbound SR 826 improvements include widening the SR 826 mainline from NW 74<sup>th</sup> Street to south of the US 27 interchange and from north of US 27 to north of NW 103<sup>rd</sup> Street. The EL system will be modified to relocate the EL ingress point from north of the I-75 interchange to NW 103<sup>rd</sup> Street, completely eliminating the SR 826 EL north of NW 103<sup>rd</sup> Street (converting it to a GP lane) and providing enhanced access to the EL system to the local community. At the NW 103<sup>rd</sup> Street interchange, the pier of the NW 103<sup>rd</sup> Street westbound flyover on-ramp to SR

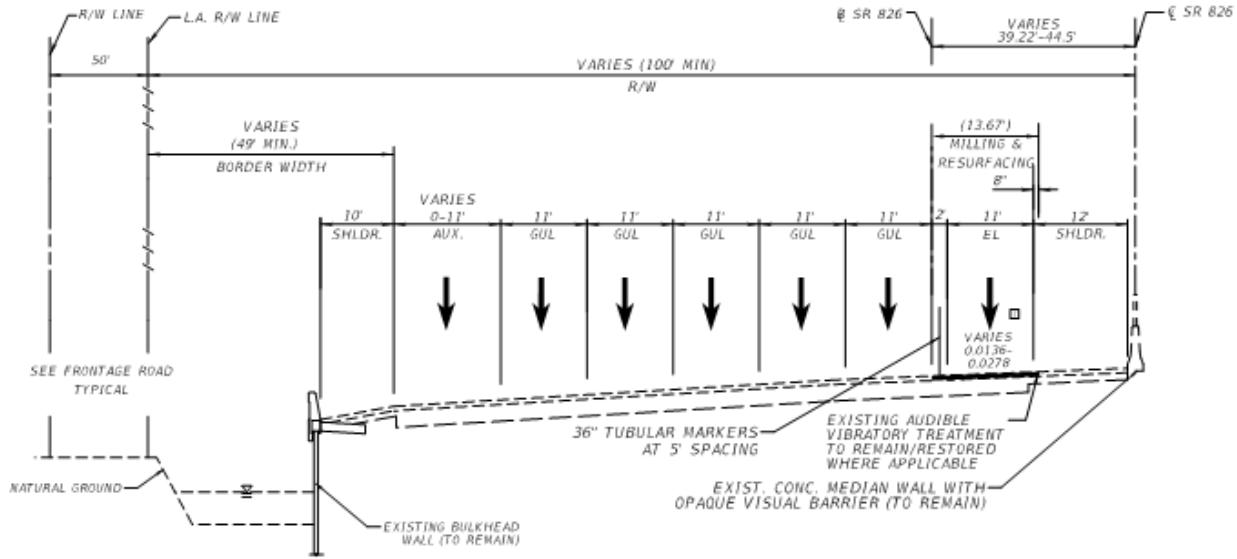
826 southbound will be modified to accommodate SR 826 southbound mainline widening. In addition, the NW 103<sup>rd</sup> Street on/off ramps will be widened, pushing out the mechanically stabilized earth (MSE) walls. Four (4) SR 826 mainline bridges are required to be widened over NW 74<sup>th</sup> Street, Metrorail, FEC Railroad, and NW 103<sup>rd</sup> Street. The improvements also include performing a comprehensive milling and resurfacing plan throughout the project limits.

The proposed SR 826 southbound typical section will consist of four (4) GP lanes from the end project (north of NW 154<sup>th</sup> Street) to the I-75 interchange; a single I-75 EL and five (5) GP lanes from the I-75 Interchange to NW 103<sup>rd</sup> Street interchange; and two (2) EL and four (4) GP lanes from the NW 103<sup>rd</sup> Street interchange to the begin project (south of NW 36<sup>th</sup> Street).

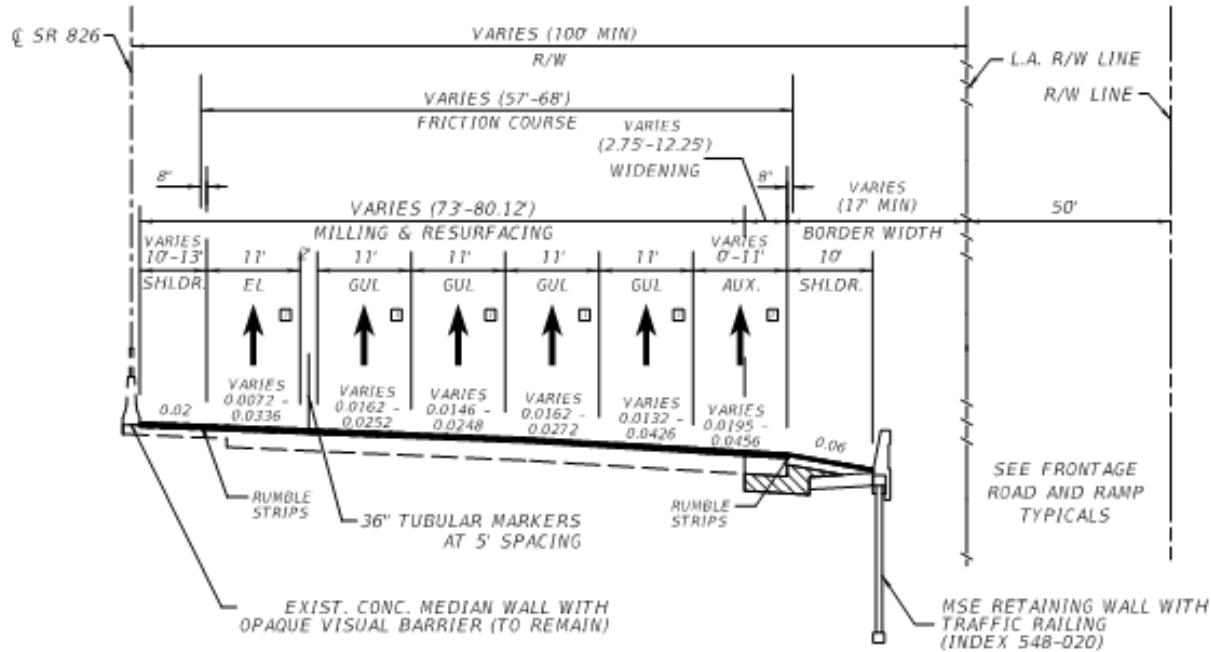
No new right-of-way (northbound and/or southbound) will be required to accommodate the proposed improvements associated with the Build Alternative.

See **Figure 3-1** and **Figure 3-2** for the proposed SR 826 typical sections in the vicinity of NW 103<sup>rd</sup> Street.

**Figure 3 - 1: Proposed SR 826 Southbound Typical Section**



**Figure 3 - 2: Proposed SR 826 Northbound Typical Section**



### 3.2.2 Frontage Roads

The proposed project will improve the frontage road system on both sides of SR 826 between US 27 and NW 122<sup>nd</sup> Street to correct deficient pavement conditions; upgrade sub-standard ground-mounted signs and pavement markings; comply with the American with Disabilities Act (ADA); replace existing guardrail; upgrade bridge railings; and construct new paved shoulders to meet criteria and improve overall safety of the corridor. Additionally, the proposed frontage road improvements will upgrade signals and landscaping in the area.

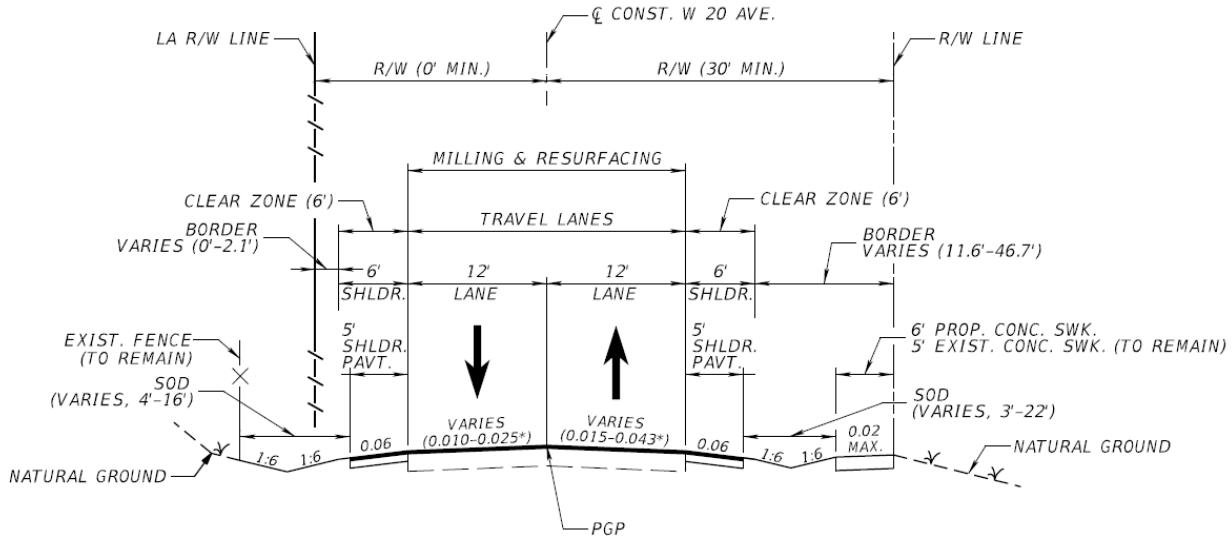
The proposed SR 826 southbound mainline widening and the modification of the pier of the NW 103<sup>rd</sup> Street westbound flyover on-ramp to SR 826 southbound will require the realignment of the West Frontage Road/NW 77<sup>th</sup> Avenue. The realignment of this two-way frontage road begins south of the NW 98<sup>th</sup> Street intersection and ends south of the NW 103<sup>rd</sup> Street intersection.

The frontage road improvements include milling and resurfacing the existing pavement; adjusting existing storm drain manhole tops, utility manhole tops, and utility valves within the limits of the resurfacing; upgrading sub-standard pedestrian curb ramps and detectable warning surfaces along East Frontage Road/W 20<sup>th</sup> Avenue to meet ADA criteria; constructing new sidewalk along East Frontage Road/W 20<sup>th</sup> Avenue to connect the gaps between existing sidewalk segments and to reconstruct damaged or uneven sidewalks; upgrading the guardrail, guardrail terminals, guardrail transition connections to the bridge traffic railings, and guardrail-to-rigid barrier transition connections; constructing 5-foot-wide paved shoulders in segments without existing roadside landscaping or utility impacts; adjusting existing ditch-bottom inlets impacted by the proposed shoulder widening; and constructing additional ditch-bottom inlets where required along the shoulder widening areas. No new right-of-way will be required to accommodate the proposed frontage roads improvements associated with the Build Alternative.

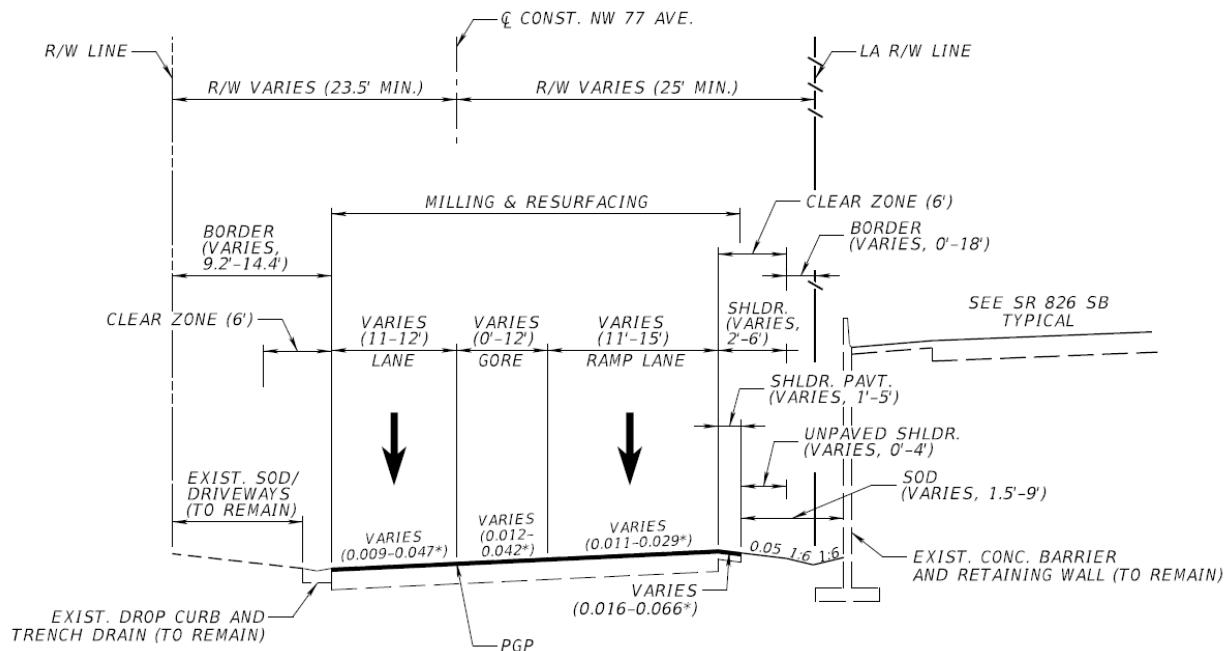
The project includes upgrading existing signing and pavement markings along the frontage road systems. Proposed signalization improvements will consist of mast arm signal poles and safety measures at the two (2) signalized intersections at NW 103<sup>rd</sup> Street; as well as the upgrade of the pedestrian signals at the two (2) signalized intersections at W 60<sup>th</sup> Street. Area landscaping modifications will include tree relocations in areas impacted by the proposed shoulder widening.

See **Figure 3-3 – Figure 3-5** for proposed frontage road typical sections.

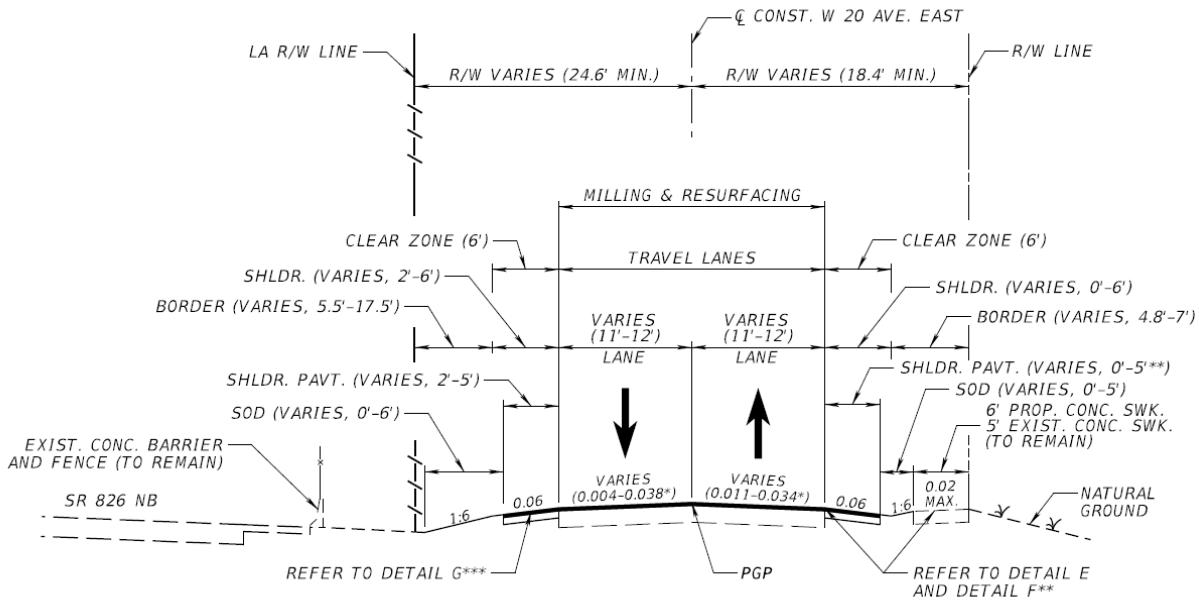
**Figure 3 - 3: Proposed Frontage Road (Undivided)  
Typical Section 1 (East Frontage Road/W 20<sup>th</sup> Avenue)**



**Figure 3 - 4: Proposed Frontage Road (One-Way)  
Typical Section 6 (West Frontage Road/NW 77<sup>th</sup> Avenue)**



**Figure 3 - 5: Proposed Frontage Road (Undivided)  
Typical Section 8 (East Frontage Road/W 20<sup>th</sup> Avenue East)**



### 3.2.3 Intelligent Transportation System (ITS)

Within the project limits, the Intelligent Transportation System (ITS) will be adjusted to support the earlier mentioned roadway changes. ITS improvements include the installation of a new tolling site along both the northbound and southbound directions that will require a specific pavement design to be implemented 50 feet north and south of the gantry location. The project will also include the installation and replacement of impacted Closed-Circuit Television (CCTV) cameras, Dynamic Message Signs (DMS), Microwave Vehicle Detection Systems (MVDS), and Ramp Signal Systems as necessary.

## 4.0 COMMUNITY CHARACTERISTICS

### 4.1 Process Overview

A Sociocultural Effects Evaluation (SCE) is a process used to evaluate and address the effects of a transportation action on a community and its quality of life. The SCE evaluation process assesses the following characteristics:

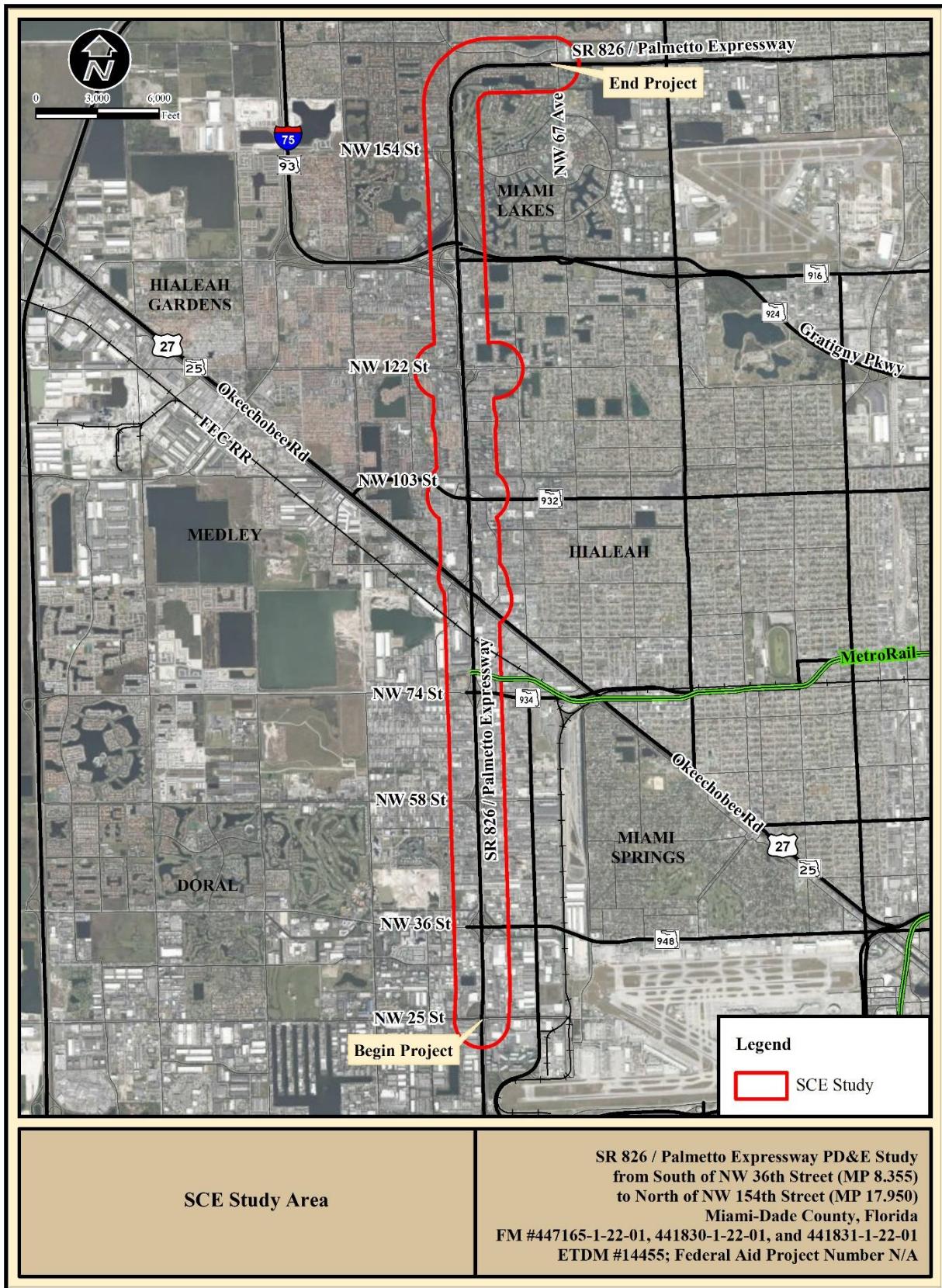
The SCE evaluation process assesses social, economic, land use changes, mobility, aesthetics effects, and relocations, including potential issues associated with Environmental Justice, Civil Rights, and other nondiscrimination laws. Project benefits and effects are assessed in the SCE evaluation with special consideration for minority, low-income, and other potentially underrepresented populations.

The SCE Evaluation process is supported by the development of a Community Characteristics Inventory (CCI). The CCI is a comprehensive summary of community attributes used to support a better understanding of the affected community by describing the sociocultural context of the project area including community facilities/services; presence of certain population groups; and indications of community values, concerns, and preferences.

The data used for the CCI and SCE evaluation is downloaded from the Florida Geographic Data Library (FGDL), and other sources as referenced in this document. This report was prepared in accordance with the FDOT PD&E Manual, Part 2, Chapter 4, Sociocultural Effects Evaluation.

### 4.2 SCE Evaluation Study Area

The project is located within the municipalities of Doral, Hialeah, Hialeah Gardens, Medley, Miami Lakes, and unincorporated Miami-Dade County and extends along SR 826 from south of NW 36<sup>th</sup> Street to north of NW 154<sup>th</sup> Street. The SCE evaluation study area incorporates an area that extends a quarter of a mile from the centerline of the aforementioned roadway. The SCE study area features urban land uses. **Figure 4-1** illustrates the location and extent of the SCE study area.



**Figure 4 - 1: Sociocultural Effects Evaluation Study Area**

### 4.3 Demographics

Demographic data describes a population based on a range of characteristics, and is primarily collected by local, state, or federal agencies; such as the Census Bureau. The demographic data covers various topics about people in communities including:

- Count
- Age
- Race and ethnicity
- Income
- Education
- Daily behavior
- Geographic distribution

This data assists in designing public participation, outreach, and education strategies that reflect the background of the community. Demographic data also aids in developing context sensitivity for the project and targeting potential effects including avoidance, minimization and mitigation activities.

The 2014-2018 American Community Survey Five-Year Estimate developed by the US Census Bureau serves as the basis for the demographic data reported below. **Tables 4-1** through **4-6** summarize characteristics of the population within the SCE study area, which includes portions of 39 different US Census Block Groups. The tables also provide county and state population estimates to support a regional comparison. **Appendix A** provides additional context by showing the distribution of low-income, minority, and elderly populations within the SCE study area.

While not as racially diverse as Miami-Dade County as a whole (see **Table 4-1**), the SCE study area supports a larger Hispanic and minority population percentage (90.7 percent and 93.8 percent, respectively). Additionally, the study area has a larger white population (91.5 percent) and higher median age (42.1) when compared to Miami-Dade County; however, the median age is approximately the same when compared to the state.

**Table 4 - 1: Demographic Comparison – Total Population**

Evaluation Criteria	SCE Study Area	Miami-Dade County	Florida
Total Population	87,930	2,715,516	20,598,139
Percent White	91.5%	75.2%	75.4%
Percent Black	9.8%	17.7%	16.1%
Percent Asian	0.8%	1.6%	2.7%
Percent Other*	4.5%	5.3%	5.5%
Percent Hispanic (regardless of race)	90.7%	68.0%	25.2%
Percent Minority**	93.8%	86.6%	45.6%
Percent Under the Age of 18	19.1%	20.4%	20.1%
Percent Age 65 or Older	17.7%	15.6%	19.7%
Median Age	42.1	39.7	41.9

\*Population includes persons identified as American Indian and Alaska Native, Native Hawaiian and Other Pacific Islander, Some Other Race, Two or More Races.

\*\*Combines Race and Ethnicity to identify the total population that is a member of either a racial or ethnic minority.

**Table 4-2** shows that the SCE study area has a lower median household income (\$44,090) when compared to Miami-Dade County (\$48,982) and a notably lower household income when compared to the state (\$53,267). The percent of the population with income below the poverty line (19.0%) is also far higher than the state (14.8%) and slightly higher than the County (18.0%).

**Table 4 - 2: Demographic Comparison – Income**

Evaluation Criteria	SCE Study Area	Miami-Dade County	Florida
Median Household Income	\$44,090	\$48,982	53,267
Percent of Population with Income Below the Poverty Line	19%	18.0%	14.8%

**Table 4-3** shows lower educational attainment in the SCE study area compared to Miami-Dade County, with only 79.5 percent of those having completed high school or received a General Educational Development (GED).

**Table 4 - 3: Demographic Comparison – Education**

Evaluation Criteria	SCE Study Area	Miami-Dade County	Florida
Percent of the Population 25 Years or Older with Less Than a High School Diploma or Equivalent	20.5%	18.5%	12.0%
Percent of the Population 25 Years or Older with a High School Diploma or Higher	79.5%	81.5%	88.0%
Percent of the Population with a Bachelor's, Master's, Doctorate, or Professional Degree	24.9%	28.8%	29.2%

**Table 4-4** shows that the study area supports a significantly higher portion of the population that is considered Limited English Proficient (LEP) (46.6%) when compared to the County (34.8%). This is nearly four (4) times greater than the state of Florida.

**Table 4 - 4: Demographic Comparison – Language**

Evaluation Criteria	SCE Study Area	Miami-Dade County	Florida
Percent of the Population that Speaks Only English	11%	25.7%	70.9%
Percent of the Population that Is Considered to be Limited English Proficient	46.6%	34.8%	11.9%

**Table 4-5** illustrates that the study area has higher housing occupancy percentage (90.9%) when compared to the County (85.6%). The median owner-occupied home values within the study area (\$192,725) are approximately \$75,000 less than the County (\$268,200).

**Table 4 - 5: Demographic Comparison – Households and Housing Units**

Evaluation Criteria	SCE Study Area	Miami-Dade County	Florida
Total Number of Households	28,902	870,051	7,621,760
Average Household Size – Occupied Units	2.94	3.07	2.66
Total Number of Housing Units	31,780	1,016,653	9,348,689
Percent of Housing Units Occupied	90.9%	85.6%	81.5%
Percent of Occupied Housing Units, Owner-Occupied	48.5%	51.6%	65.0%
Median Owner-Occupied Home Value	\$192,725	\$268,200	\$196,800

**Table 4-6** shows a lower percentage of automobile access for the study area (9.7%), but a slightly higher percentage of automobile use for work trips (86.1%) when compared to county-wide percentages (10.4% and 85.7%).

**Table 4 - 6: Demographic Comparison – Transportation**

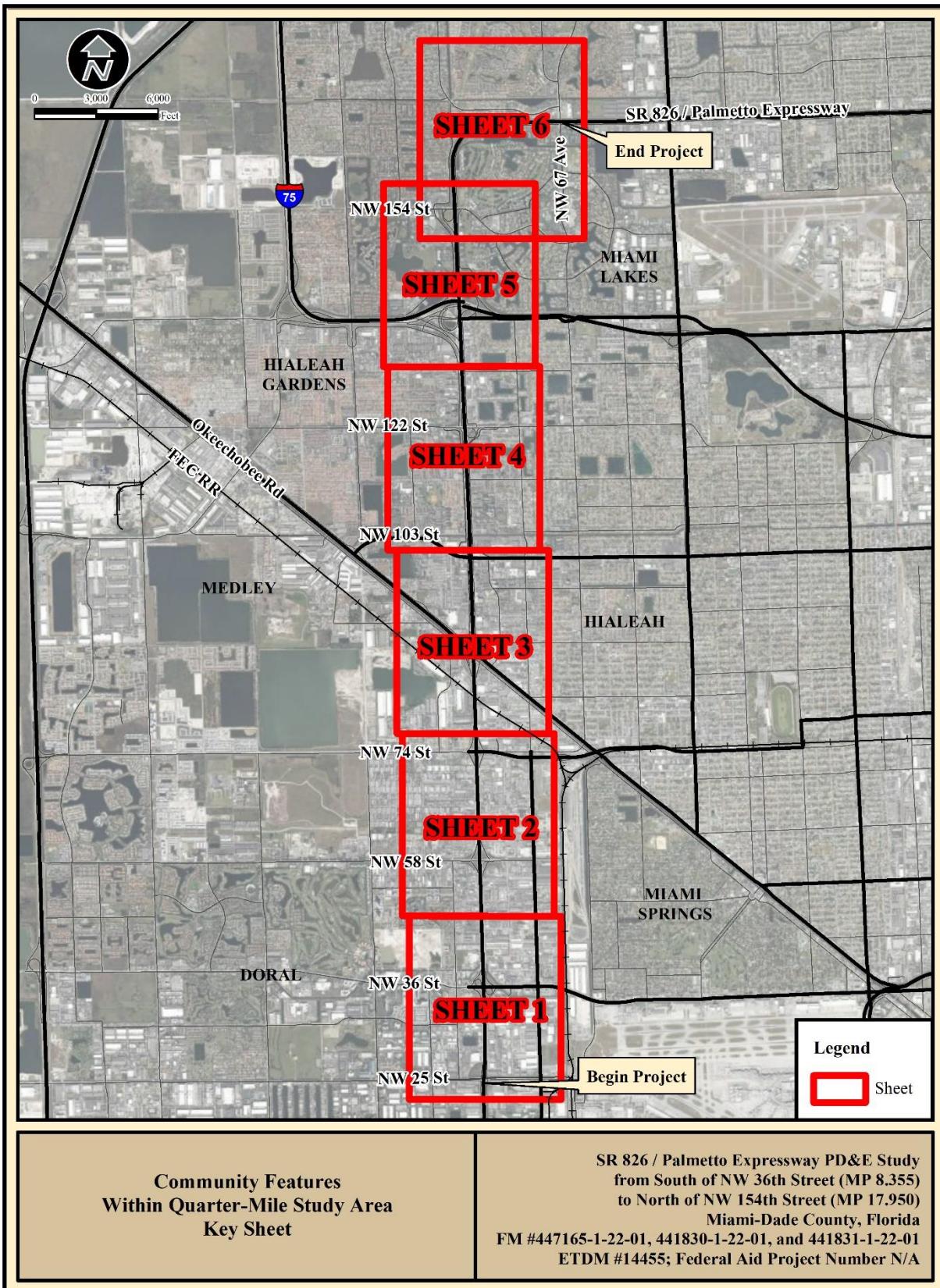
Evaluation Criteria	SCE Study Area	Miami-Dade County	Florida
Percent of the Population that Commute to/from Work via a Car, Truck, or Van	86.1%	85.7%	88.3%
Percent of the Population that Walks to/from Work	1.8%	2.0%	1.4%
Percent of the Population that Takes Public Transportation	1.4%	5.0%	1.9%
Percent of the Population that Travels to Work/from via “Other” Means	0.7%	2.1%	2.2%
Percent of the Population that Works from Home	9.5%	5.2%	5.8%
Percent of 0 vehicle households	9.7%	10.4%	6.5%

#### 4.4 Community Features

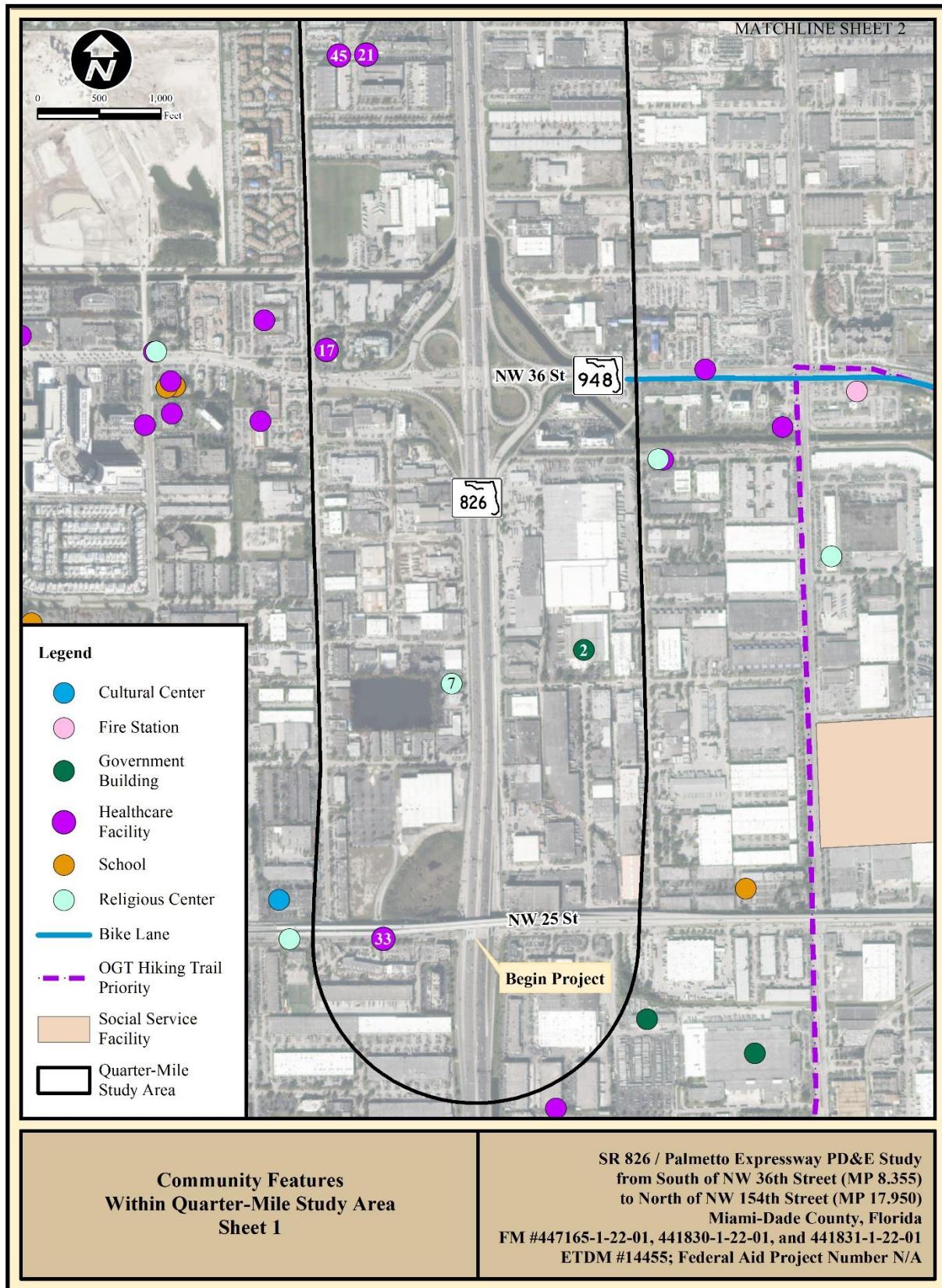
Community features are public or private facilities, organizations, or locations that often hold special importance to local residents. **Figure 4-2** and **Figure 4-3** depict the facility location and corresponds with the tables listed in the section. **Tables 4-7** through **4-16** describe the community focal points present within the quarter-mile SCE study area. These types of facilities include:

- Cemetery
- Civic Center
- Cultural Center
- Existing Recreational Trail
- Fire Station
- Law Enforcement Facility
- Government Building
- Group Care Facility
- HealthCare Facility
- Hospital
- Parks and Recreational Facility
- School
- Social Services Facility
- Religious Facility

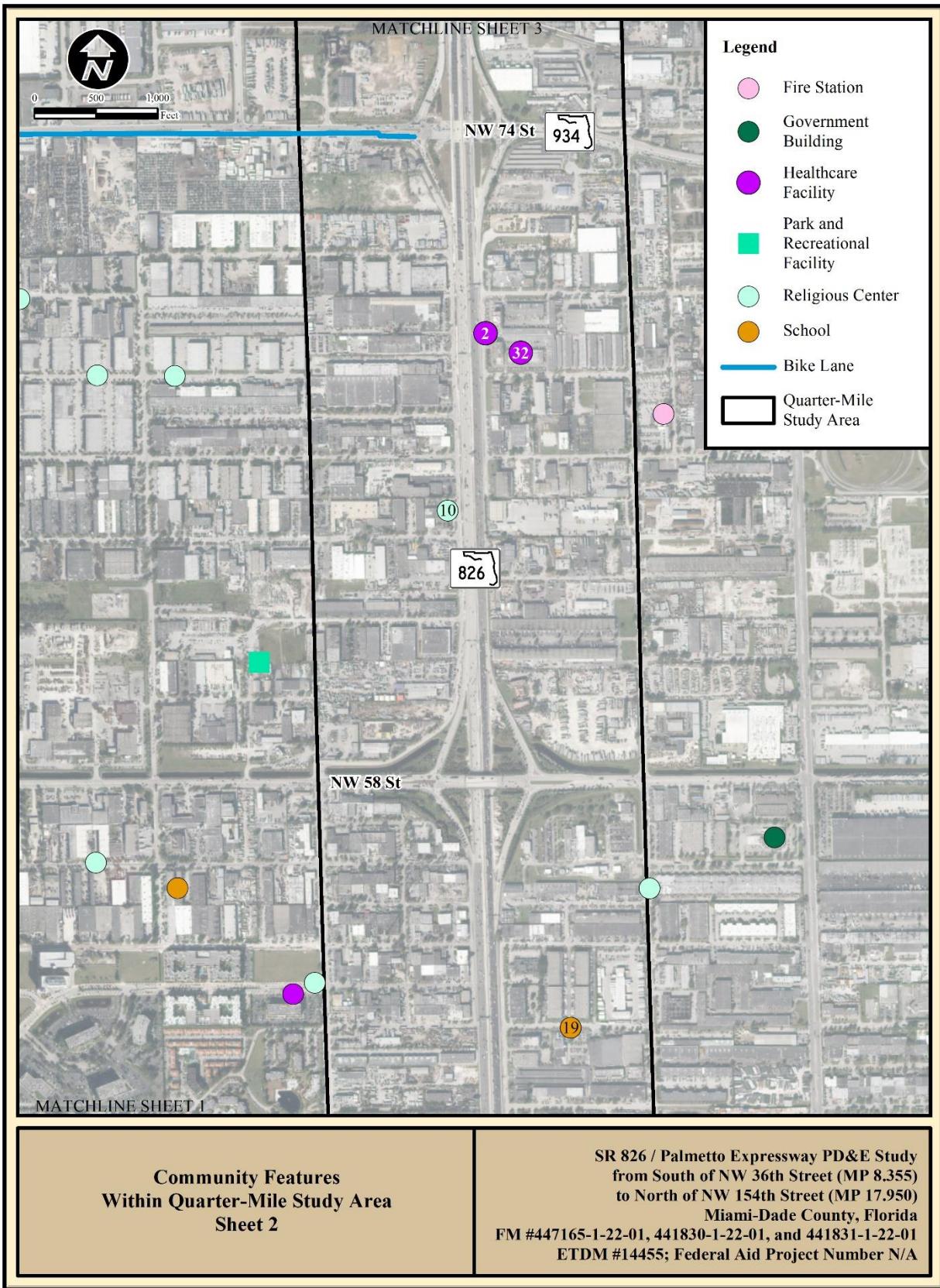
No community features will be displaced as a result of the proposed improvements. No negative impacts to community feature access have been identified.



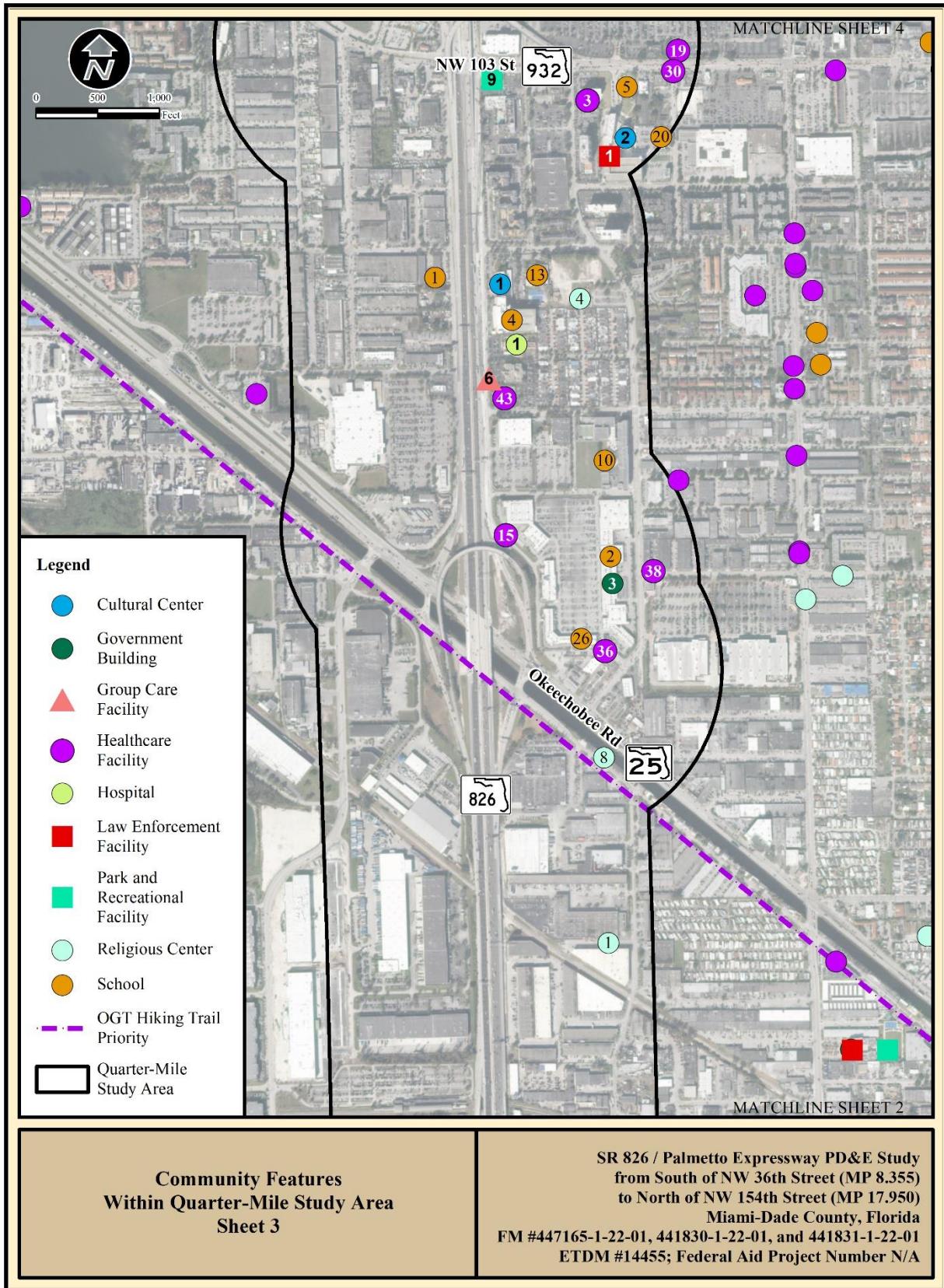
**Figure 4 - 2: Community Features Key Sheet**



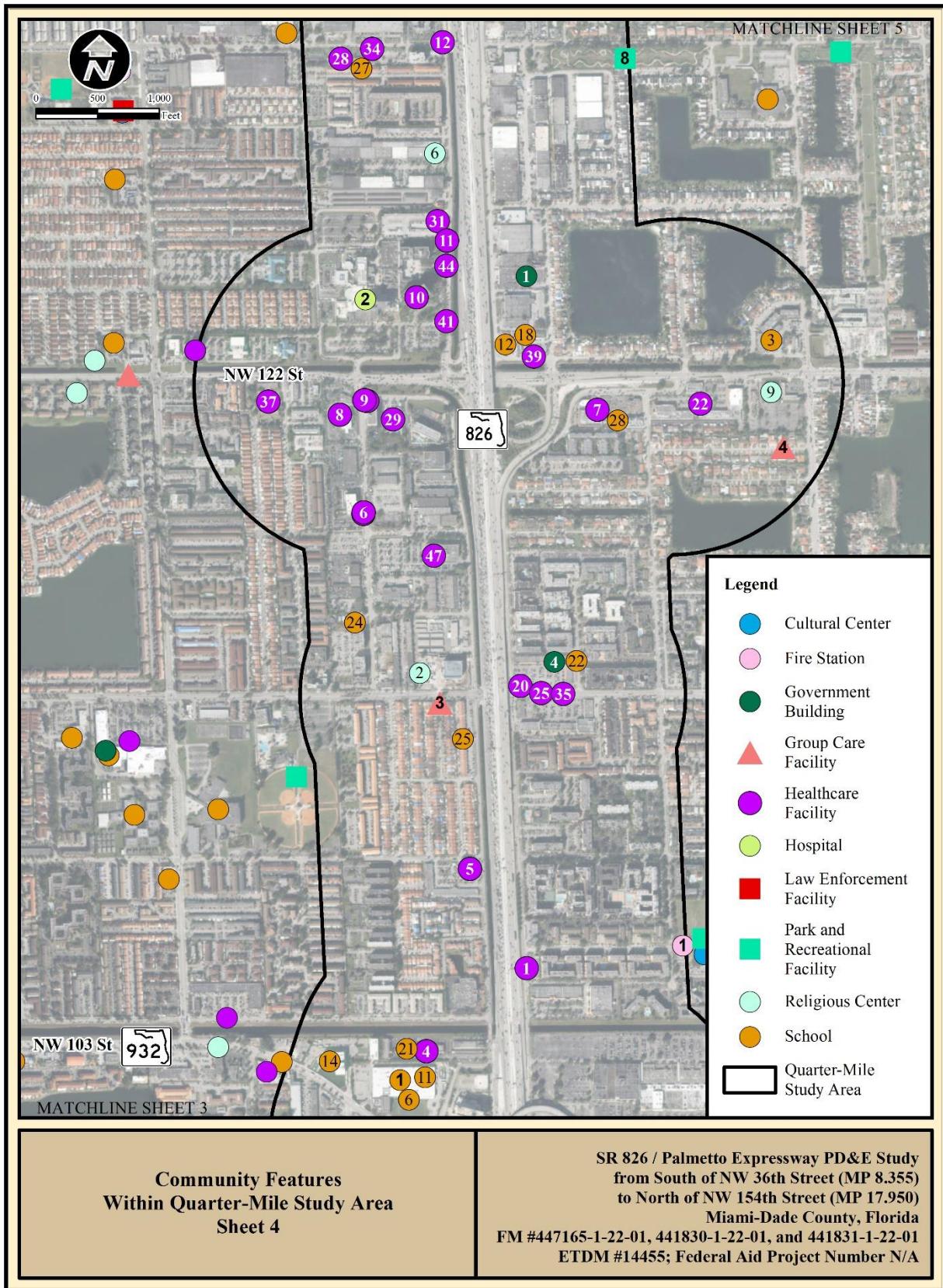
**Figure 4 - 3: Community Features (1 of 6)**



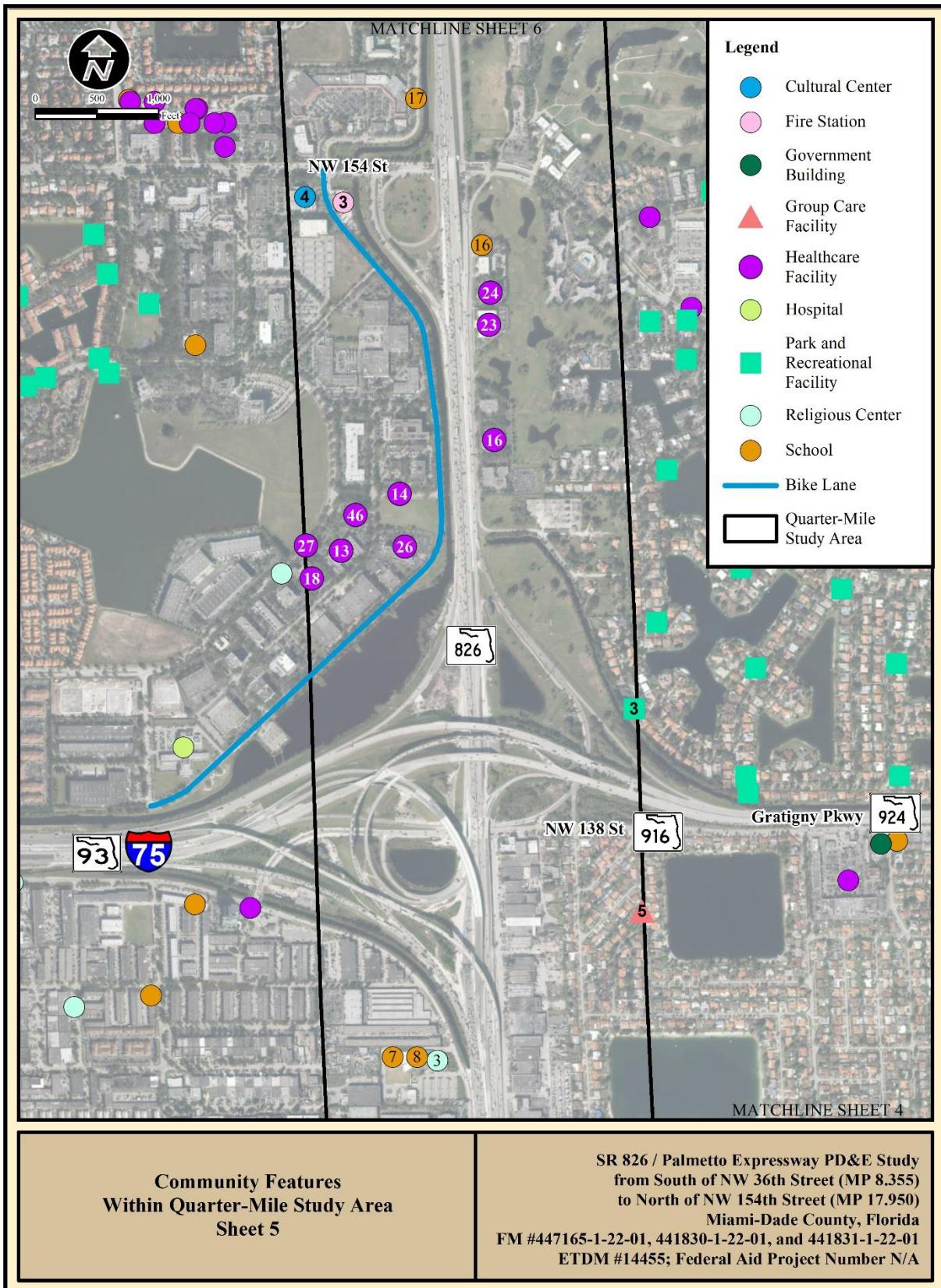
**Figure 4 - 3: Community Features (2 of 6)**



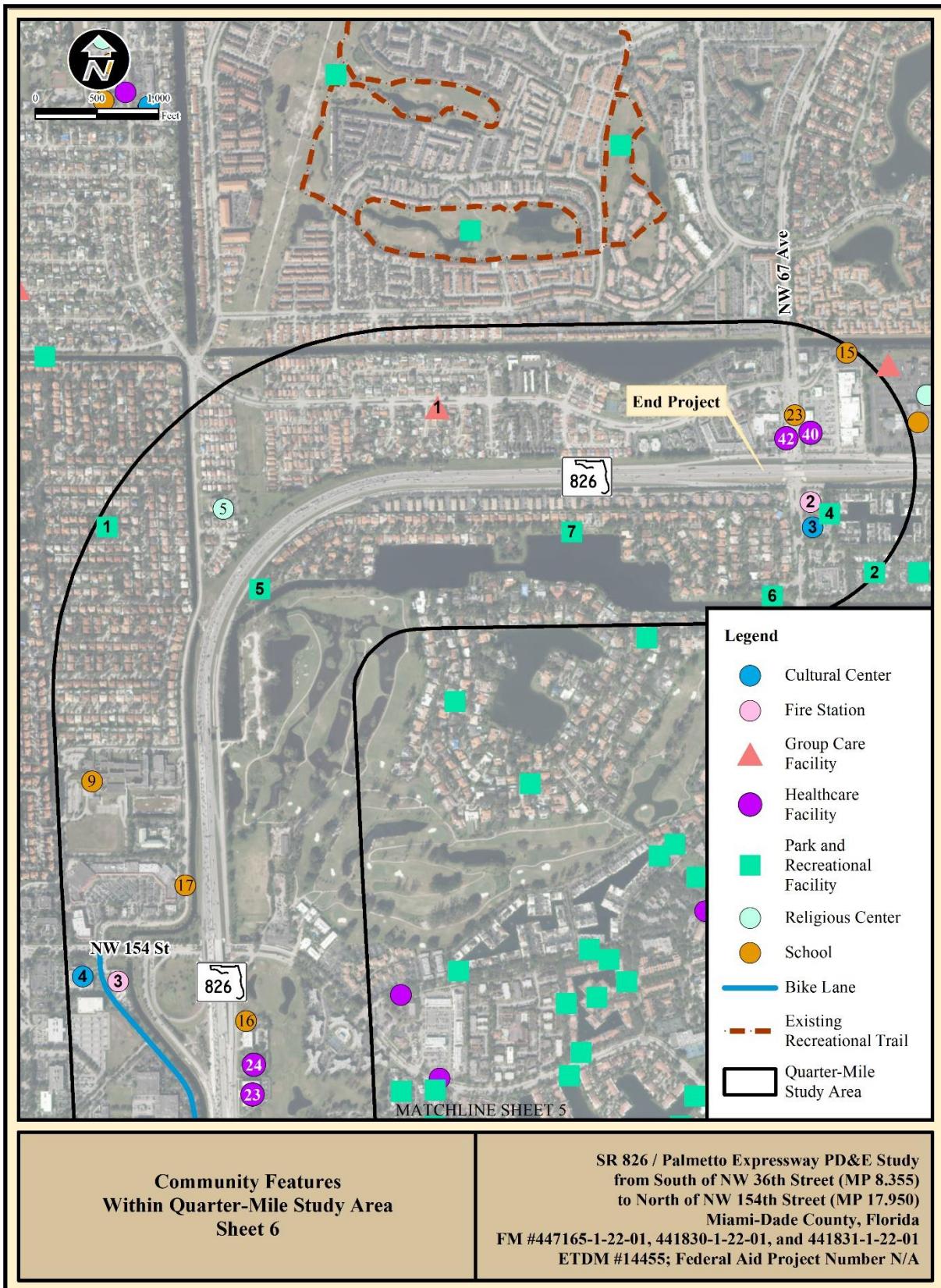
**Figure 4 - 3: Community Features (3 of 6)**



**Figure 4 - 3: Community Features (4 of 6)**



**Figure 4 - 3: Community Features (5 of 6)**



**Figure 4 - 3: Community Features (6 of 6)**

**Table 4 - 7: Group Care Facilities in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	6	Golden Palm A.L.F. Inc.	7280 NW 169 Street
2	3	Citrus Health Network - Adult Crisis Unit	4175 W 20 Avenue
2	3	Citrus Health Network - Children's Crisis Unit	4175 W 20 Avenue
3	4	Sweet Paradise ALF	5991 W 20 Lane
4	4	Our Loving Mother	1635 W 65 Street
5	5	New World ALF	8184 W 18 Avenue

**Table 4 - 8: Park and Recreational Facilities in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	6	Miami Lakes Mini Park (P-86)	6851 Main Street
2	6	Miami Lakes Mini Park (P-56)	16331 Ravenwood Place
3	5	Miami Lakes Mini Park (P-28)	7350 Sabal Drive
4	6	Miami Lakes Mini Park (P-55)	16695 NW 67 Avenue
5	6	Miami Lakes Mini Park (P-50)	7599 Loch Ness Drive
6	6	Miami Lakes Mini Park (P-52)	6700 Loch Ness Drive
7	6	Miami Lakes Mini Park (P-51)	6970 Loch Ness Drive
8	4	Garden of The Arts Park	1850 W 76 Street
<b>9</b>	6	Don Shula's Golf Club	7601 Miami Lakes Drive

**Table 4 - 9: Law Enforcement Facilities in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	3	Miami-Dade College Police Department - Hialeah Campus	1780 W 49 Street (Rm 1114)

**Table 4 - 10: Hospitals in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	3	Southern Winds Hospital	4225 W 20 Avenue
2	4	Palmetto General Hospital	2001 W 68 Street

**Table 4 - 11: Healthcare Facilities in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	4	Perez D.O. Pa Gerardo C.	5355 W 20 Avenue
2	2	Leah Medical Center Inc.	6917 NW 77 Avenue
2	2	Mdd Medical Center	6905-07 NW 77 Avenue
3	3	Mora Elba M.D. P.A.	1840 W 49 Street, Suite 516

MAP ID #	Map #	Facility	Address
3	3	Luis Lugo-Arrendell M.D. P.A.	1840 W 49 Street, Suite 510
3	3	Esperanza Arce- Nunez M.D.	1840 W 49 Street, Suite 420
3	3	Multimed Care Inc	1840 W 49 Street, Suite 700
3	3	International Research Associates, LLC	1840 W 49th Street, Suite 103
3	3	American Family Health Center	1840 W 49 Street, Suite 425
3	3	Savin Medical Group/ Carlos Aguilar	1840 W 49 Street, Suite 229
3	3	Isabel C. Gomez, M.D.	1840 W 49 Street, Suite 517
3	3	Luzmed Clinical Research Institute	1840 W 49 Street, Suite 605
3	3	Spera, Carlos, M.D., P.A.	1840 W 49 Street, Suite 411
3	3	Vicente A. Chavarria Md	1840 W 49 Street, Suite 103
3	3	Eduardo G. Gomez M.D., P.A.	1840 W 49 Street, Suite 607
3	3	Rodrigo Belacazar, M.D., P.L.L.C.	1840 W 49 Street, Suite 601
3	3	Community Clinical Center Inc	1840 W 49 Street, Suite 105
3	3	Faith Medical Group Inc	1840 W 49 Street 514a
4	4	Nuconcept Clinic LLC	10550 NW 77 Court, Suite 312
4	4	Centro De Terapia Avanzada Inc.	10550 NW 77 Court, Suite 308
4	4	Context Medical Group	10550 NW 77 Court, Suite 305
5	4	Fraga Pediatrics & Associates Dba/Prime Care Fc	5590 W 20 Avenue, Suite 100
5	4	South Florida Pediatrics Inc.	5590 W 20 Avenue, Suite 300
5	4	Eduardo A Reyes M.D. P.A	5590 W 20 Avenue, Suite 401
5	4	Yanssel Delgado DPM	5590 W 20 Avenue, Suite 404
6	4	Maria T. Pol-Carballo M.D. P.A.	6450 W 21 Court, Suite 301
6	4	Victores Md Pa Lorenzo	6450 W 21 Court, Suite 205
6	4	Power Pediatrics Inc	6450 W 21 Court, Suite 306
6	4	Cordoves M.D. Lourdes A.	6450 W 21 Court, Suite 305
6	4	21 <sup>st</sup> Century Oncology, LLC Dba Southeast Florida	6450 W 21 Court, Stop 205
7	4	Miguel A. Chamah M.D. Pa.	1800 W 68 Street, Suite 127
7	4	John A. Serpa MD PA	1800 W 68 Street, Suite 115
8	4	Mt. Sinai Cardiac	2150 W 68 Street, Suite 115
8	4	Chen Medical Hialeah	2150 W 68 Street, Suite 200
9	4	Urology Specialists LLC	2140 W 68 Street, Suite 200
9	4	Tenet Florida Physician Services, LLC	2140 W 68 Street, Suite 403
9	4	Moya M.D. Pa Roberto A.	2140 W 68 Street, Suite 201

MAP ID #	Map #	Facility	Address
9	4	Fernandez M.D. P.A. Benedicto	2140 W 68 Street, Suite 406
9	4	Digestive Medicine Assoc.	2140 W 68 Street, Suite 300
9	4	Ravelo Jorge E.	2140 W 68 Street, Suite 307
9	4	Julian Marquez MD PA	2140 W 68 Street, Suite 401A
9	4	Carrillo Pedro L. Jr. M.D.	2140 W 68 Street, Suite 308
9	4	Imaging Center The	2140 W 68 Street 103
9	4	Palmetto ASC L.P. The	2140 W 68 Street, Suite 102
9	4	Aran Eye Association	2140 W 68 Street, Suite 405
9	4	America Mobile Health Service Inc.	2134 W 68 Street
9	4	Urology Specialists, LLC	2140 W 68 Street, Suite 302
10	4	Siguanea LLC	7100 W 20 Avenue, Suite 312
10	4	Allergy and Asthma Center of Miami P.A.	7100 W 20 Avenue, Suite 706
10	4	Rovira, M.D. Juan J.	7100 W 20 Avenue, Suite 404
10	4	Jorge L. Diaz Do Pa	7100 W 20 Avenue, Suite 403
10	4	Salinas M.D. Pa Hugo	7100 W 20 Avenue, Suite 516
10	4	Uro-Medix Inc.	7100 W 20 Avenue, Suite 703
10	4	Orthopedic Specialists of South Fl	7100 W 20 Avenue, Suite 101
10	4	Torres M.D. Julio D.	7100 W 20 Avenue, Suite 105
10	4	Pagan Luis M.D.	7100 W 20 Avenue, Suite G-176
10	4	Fernandez & Molina M.D.	7100 W 20 Avenue, Suite 602
10	4	Halperin, Scott B. M.D.	7100 W 20 Avenue, Suite 213
10	4	Shalhub M.D. Don	7100 W 20 Avenue, Suite 414
10	4	Alhambra Medical Group Inc.	7100 W 20 Avenue, Suite 506
10	4	Campos M.D. Pa Jaime E.	7100 W 20 Avenue, Suite 303
10	4	Tenet Florida Physician Services/Dr. Sangita Gogat	7100 W 20 Avenue, Suite 107
10	4	Jose Pinero, Md	7100 W 20 Avenue, Suite 314
10	4	J.V. Ferreira M.D. P.A.	7100 W 20 Avenue, Suite 801
10	4	Advanced Gastroenterology of South Florida, P.A.	7100 W 20 Avenue, Suite 301
10	4	West Dade Pediatrics Inc.	7100 W 20 Avenue, Suite 411
10	4	Font Jose M.D. P.A.	7100 W 20 Avenue, Suite 806
10	4	Reyes Md Franklin A.	7100 W 20 Avenue, Suite 616
10	4	Andres Vega M.D. P.A.	7100 W 20 Avenue, Suite 601
10	4	Alvarez M.D. Pa Hernando	7100 W 20 Avenue, Suite 304

MAP ID #	Map #	Facility	Address
10	4	Kenneth Strubbe, MD	7100 W 20 Avenue, Suite 107
10	4	Physicians' Management GP. Of Dade, Inc.	7100 W 20 Avenue, Suite 110
10	4	Inpatient Healthcare Group	7100 W 20 Avenue, Suite G-126
10	4	Miami Ob/Gyn Dreams	7100 W 20 Avenue, Suite 803
10	4	Jose Rodriguez-Valdes M.D.	7100 W 20 Avenue, Suite 401
10	4	Steven Fields M.D. P.A	7100 W 20 Avenue, Suite 311
10	4	Northwest Center for Infertility and Reproductive	7100 W 20 Avenue, Suite 205
10	4	Neurology-Neurosurgery of Dade And Broward Inc.	7100 W 20 Avenue, Suite 702
10	4	Miami Interventional Spine Specialists	7100 W 20 Avenue, Suite 210
10	4	Martinez-Alba Jr M.D. Jose R.	7100 W 20 Avenue, Suite 514
10	4	Ason M.D. Rafael	7100 W 20 Avenue, Suite 501
10	4	C & C Medical	7100 W 20 Avenue, Suite 412
10	4	Martinez-Catinchi MD PA Fernando	7100 W 20 Avenue, Suite 402
10	4	Vidal Angel F. M.D. P.A.	7100 W 20 Avenue, Suite 703
10	4	Gilberto Concepcion MD PA	7100 W 20 Avenue, Suite 806
10	4	Sofia E. Vasquez - Solomon M.D	7100 W 20 Avenue, Suite 214
10	4	Cesar A. O'phelan M.D.	7100 W 20 Avenue, Suite 201
11	4	Jaraki Medical Care, P.A.	7150 W 20 Avenue, Suite 318
11	4	Alvarez Pedro G. D.O. P.A.	7150 W 20 Avenue, Suite 607
11	4	Tenet Florida Physician Services LLC	7150 W 20 Avenue, Suite 615
11	4	Gonzalez Abreu &Fernandez MD. PA.	7150 W 20 Avenue, Suite 202
11	4	Shuman Md Pa Joseph	7150 W 20 Avenue, Suite 408
11	4	Dr. Victoria Garcia And Associates LLC	7150 W 20 Avenue, Suite 501
11	4	South Florida Eye Associates	7150 W 20 Avenue, Suite 411
11	4	Florida Center for Allergy & Asthma	7150 W 20 Avenue, Suite 106
11	4	Hamady Ghassan T. M.D.	7150 W 20 Avenue, Suite 406
11	4	Miami Lakes Medical Center Assoc. PA	7150 W 20 Avenue, Suite 615
11	4	Oncology & Radiation Assoc. Pa.	7150 W 20 Avenue, Suite 214
11	4	Pombo, Md, Hector	7150 W 20 Avenue, Suite 313
11	4	Rodriguez M.D. J. Ramon	7150 W 20 Avenue, Suite 402
11	4	Palmetto Artificial Kidney Center	7150 W 20 Avenue, Suite 109
11	4	Rodolfo A. Perez, M.D	7150 W 20 Avenue, Suite 304
11	4	South Florida Gyn\Oncology	7150 W 20 Avenue, Suite 313

MAP ID #	Map #	Facility	Address
11	4	Robert O'Neill MD	7150 W 20 Avenue, Suite 612
11	4	Miami Spine Care	7150 W 20 Avenue, Suite 209
12	4	Karelia Ruiz	7600 W 20 Avenue, Suite 106
12	4	Dr. Mary Jo Villar	7600 W 20 Avenue, Suite 104
13	5	Canton Enrique J. MD.	14505 Commerce Way, Suite 800
13	5	Renovation of Life Dialysis	14505 Commerce Way, Suite 600
13	5	De Quesada Gonzalo Md	14505 Commerce Way, Suite 800
13	5	Rives Elvira J. MD. PA.	14505 Commerce Way, Suite 800
13	5	Diamond Health Medical Care Center Inc	14505 Commerce Way, Suite 750
14	5	B & C Family Health Group P.A	7761 NW 146 Street
14	5	Lysette Iglesias M.D. PA.	7761 NW 146 Street
15	3	Community Medical Group of Hialeah	3805 W 20 Avenue, Suite 105
16	5	Baptist Medical Plaza Miami Lakes Urgent Care	14701 NW 77 Avenue
17	1	Tenet Florida Physician Services	3901 NW 79 Avenue, Suite 222
18	5	Power G Medical Center Corp	14411 Commerce Way, Suite 305
19	3	Dynamic Medical Services	1685 W 49 Street, Suite 1104
20	4	Hialeah Diagnostic Inc.	1991 W 60 Street
21	1	Doral Kidney Center	7755 NW 48 Street, Suite 120
22	4	University Health Care Hialeah, Inc	1700 W 68 Street
23	5 & 6	Miami Children's Hospital, Miami Lakes Outpatient	15025 NW 77 Avenue
24	5 & 6	Interamerican Medical Center	15105 NW 77 Avenue, Floor 4
25	4	Carlos L. Delgado	1957 W 60 Street
26	5	South Fl Pediatric Partners	14400 NW 77 Court, Suite 102
27	5	Universal Imaging and Radiology	14462 Commerce Way
28	4	Westland Medical Center	2150 W 76 Street, Suite 110
29	4	All M.D. Medical Center LLC	2100 W 68 Street
30	3	IQuest Surgery Center Inc	1738 W 49 Street, Suite 8,9,10
31	4	BMA Of Hialeah	7170 W 20 Avenue
32	2	Life Medical Center & Research	6801 NW 77 Avenue, Suite 105
33	1	Concentra Medical Center	7800 NW 25 Street
34	4	Palmetto Lakes Surgical Center	2100 W 76 Street, Suite 101
35	4	A Woman's Option	1933 W 60 Street
36	3	The Medical City	1905 W 35 Street, Suite 105

MAP ID #	Map #	Facility	Address
37	4	Palmetto Rehab and Health Center	6750 W 22 Court
38	3	Florida Health Solution Center	1779 W 37 Street, Suite 1
39	4	Elite Health & Rehabilitation Ctr.	1949 W 68 Street, Suite 200
40	6	Palm Plaza Medical Center, LLC	16795 NW 67 Avenue
41	4	Perez Pediatrics P.A.	7100 W 20 Avenue, Suite G154
42	6	Bridgewater Women Center LLC	16782 NW 67 Avenue
43	3	Citrus Health Network Inc	4125 W 20 Avenue
44	4	Cardiology Diagnostic Serv. So. Fl.	6450 W 21 Court, Suite 201
45	1	Doral Imaging Institute LLC.	7775 NW 48 Street, Suite 150
46	5	Barry M.D. Patrick J.	7850 NW 146 Street, Suite 508
47	4	Leon Medical Centers Inc	2020 W 64 Street

**Table 4 - 12: Government Buildings in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	4	U S Post Office - Hialeah Annex	7000 W 19 Court
2	1	U S Post Office - Miami Periodical Annex	7499 NW 31 Street
3	3	U S Post Office - Promenade	3690 W 18 Avenue
4	4	Florida Department of Highway Safety and Motor Vehicles Service Center	1923 W 60 Street

**Table 4 - 13: Fire Stations in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	4	Hialeah Fire Department and Rescue Station 8	5405 W 18 Avenue
2	6	Miami-Dade County Fire Department and Rescue Station 1 (Miami Lakes)	16699 NW 67 Avenue
3	5 & 6	Miami Lakes West Fire Department Station 64	15325 NW 77 Court

**Table 4 - 14: Cultural Centers in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	3	Florida National College Library	4425 Jose Regueiro Ave
2	3	Miami Dade College - Hialeah Campus Library	1780 W 49 Street
3	6	Miami Lakes Library	6699 Windmill Gate Road
4	5 & 6	Jay I Kislak Foundation Inc	7900 Miami Lakes Drive W

**Table 4 - 15: Schools in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	4	Mater Academy	7700 NW 98 Street
2	3	Florida Career College-Hialeah	3750 West 18 Avenue
3	4	Acceleration Academies LLC Hialeah	1665 W 68 Street, Suite 109
4	3	Florida National College	4425 W 20 Avenue
5	3	Miami Dade College - Hialeah Campus	1776 W 49 Street
6	4	Mater Performing Arts & Entertainment Academy	7901 NW 103 Street
7	5	Youth Co-Op Charter School	7700 West 20 Avenue
8	5	Youth Co-Op Preparatory High School	7700 West 20 Avenue
9	6	Bob Graham Education Center	15901 NW 79 Avenue
10	3	Westland Hialeah Senior High School	4000 W 18 Avenue
11	4	Mater Academy Charter Middle	7901 NW 103 Street
12	4	American Advanced Technicians Institute	6801 W 20 Street
13	3	Our Lady of Charity Private School	1900 W 44 Place
14	4	Mater Academy Elementary	8003 NW 103 Street
15	6	Little College at Ludlam C. Corp.	6545 NW 169 Street
16	5 & 6	Little College at Royal Oaks Plaza	15225 NW 77 Avenue
17	5 & 6	Children's Paradise Learning Center #5	15520 NW 77 Court Rear
18	4	Happy Children Academy	1937 West 68 Street
19	2	Blessed Pediatric Care	7500 NW 52 Street, Suite 100
20	3	Petite World	4680 W 17 Court 5
21	4	Tiny Smile Learning Center	10550 NW 77 Court, Unit 208-212
22	4	Little Children's Learning Academy	1917 W 60 Street
23	6	America's Christian Future Child Care Learning Ctr	16931 NW 67 Avenue
24	4	Paradise Christian School	6184 W 21 Court
25	4	American Christian School & Art	5888 W 20 Avenue
26	3	Miss Carusi Learning Center	1905 W 35 Street
27	4	Steps for Learning Preschool Center	2150 W 76 Street, Suite 101
28	4	Gratigny Day Care Center Corporation	1800 W 68 Street

**Table 4 - 16: Religious Centers in SCE Study Area**

MAP ID #	Map #	Facility	Address
1	3	Centro Cristiano Internacional	7540 NW 82 Street
2	4	Church of Jesus Christ The Almighty	6050 W 20 Avenue
3	5	Church of Christ Hialeah	7700 W 20 Avenue
4	3	San Lazaro Parish	4400 W 18 Avenue
5	6	Orthodox Cathedral of Christ The Savior	16601 NW 77 Court
6	4	Iglesia Cristiano Del Alma	7306 W 20 Avenue
7	1	Comunidad Servicios De Cristo	3100 NW 77 Court
8	3	Iglesia Bautista	8551 NW South River Drive
9	4	Primera Hispana Iglesia	1650 W 68 Street
10	2	Bezerra De Menezes Kardec Spiritist Association	6450 NW 77 Court

## 5.0 COMMUNITY PLANS, GOALS, and ENGAGEMENT

### 5.1 Community Plans and Goals

Local planning documents including local comprehensive plans, LRTPs, and sub-area plans help local governments establish priorities in investment and identify specific initiatives. These local plans are often guided by policies or other statements intended to implement local goals.

#### 5.1.1 SR 826 Mainline (FM 447165-1-22-01)

The project is included in the FDOT FY 2020 - FY 2024 Work Program for a PD&E Study and Railroads and Utilities that continues through 2021 and Preliminary Engineering in 2022. The project is also included in the FDOT FY 2020 - FY 2024 State Transportation Improvement Program (STIP) for a PD&E Study in 2021. The project is not included in the Miami-Dade TPO's FY 2021 - FY 2025 Transportation Improvement Program (TIP). The project is not included in the Miami-Dade TPO's 2045 LRTP nor in the FDOT's SIS First Five-Year Plan.

#### 5.1.2 SR 826/Frontage Roads (FM 441831-1-22-01 and 441830-1-22-01)

Both projects are included in the FDOT FY 2020 - FY 2024 Work Program for Preliminary Engineering in 2021 and Contract Incentives and Design Build in 2024. Both projects are also included in the FDOT FY 2020 - FY 2024 STIP for Preliminary Engineering in 2021 and Contract Incentives and Design Build in 2024. The projects are additionally included in the Miami-Dade TPO's FY 2021 - FY 2025 TIP for Resurfacing. The project is not included in the Miami-Dade TPO's 2045 LRTP nor in the FDOT's SIS First Five-Year Plan.

#### 5.1.3 Community Engagement

In addition to local plans, public engagement is an effective means of identifying community goals. Reevaluation of the original plan put forth by FDOT for SR 826 and evaluation of additional means to improve the operation of the roadway in this area comes from community input and engagement.

During the Efficient Transportation Decision Making (ETDM) Programming Screen phase the Florida Environmental Technical Advisory Team (ETAT) had an opportunity to provide comments on the project in its infancy during the 45-day review period. The Florida ETAT is made up of representatives from Metropolitan Planning Organizations (MPOs)/TPOs, federal and state agencies, and participating Native American Tribes. ETAT members use the Environmental Screening Tool (EST) to review project information, identify potential project effects, and submit comments to FDOT. The comments and other information are made available to the public on the ETDM Public Access Site (<https://etdmpub.fl-estat.org/est/>).

FDOT has received input from the public regarding EL, access to the EL system, and tolling in the Miami-Dade County area. Feedback, both from the public and their official representatives, included unacceptable/significant delays in commute times; concern for the quality of life for families and businesses; and an interest in additional options for motorists, including transit. FDOT was asked to seek alternative transportation solutions with more toll-free lanes; to address

uncapped costs to use the managed lanes all day; and to return SR 826 to a toll-free roadway. Input from some areas indicated traffic has improved; other areas noted congestion has worsened; and in general, limited access points into the express lanes is an issue.

Upon preliminary evaluation of existing traffic data and physical constraints, roadway congestion and large differentials between higher EL speeds and lower GP lane speeds have been observed in both the northbound and southbound directions during peak travel times. Based on conducting preliminary analysis, receiving feedback from the public (both for and against EL), and addressing potential public controversy regarding the use of tolled roadways and EL, FDOT determined a potential need for modifications to SR 826 to enhance safety, provide additional GP lane capacity, and improve EL access. To address the input received and identified needs, this PD&E Study was programmed.

On December 2, 2020 the SR 826 Bus Tour was held which included FDOT, TPO, and Miami-Dade staff as well as TPO Board members. The purpose of the event was to provide a description of what was completed under Palmetto Expressway Phase 1 modifications and to go over the Build Alternative considered for this project by driving to the locations of different completed and proposed improvements.

A community engagement plan was developed and is being implemented for this PD&E Study. This plan is entitled the Public Involvement Plan (PIP), a companion document to this PD&E study. The PIP is a working document which will be updated and amended throughout the project development process to incorporate the latest public involvement policies and techniques as they evolve during the life of the project. The PIP outlines the public involvement approach and activities required to be undertaken with the project, including lists of the contact persons, such as citizens, private groups (residential/business), officials, agencies, stakeholders, and media, and the means used to involve them in the process.

Public information dissemination began in the summer of 2020 and has continued throughout the study process. Exhibits and project information has been and will continue to be provided for public review and comment. FDOT representatives have been and will continue to be available to discuss the project and answer questions, as well as members of the consultant team.

The following techniques have been used to notify the public of the proposed transportation improvement and to solicit public input into the project development process.

- Newspaper advertisements (The Miami Herald and El Nuevo Herald)

In addition to working with the media, different notification techniques have been used throughout the project development process. A brief description of these techniques is provided below.

- Letters to elected and appointed officials, property owners/tenants, business owners/operators, and interested parties as necessary via mail or hand delivery.
- Press release submitted by the District's Public Information Office (PIO) seven (7) days prior to the public hearing.
- Public notices/legal display advertisements published in the area newspapers with the largest circulation (The Miami Herald and El Nuevo Herald) twice prior to the public

hearing. Notices will also be published in the Florida Administrative Register (FAR) and on the FDOT's Public Meeting Notices Website at least seven (7) days prior to the hearing.

- Public announcements via flyers to organizations such as neighborhood and civic groups, municipalities, and Miami-Dade County, to publish in their existing newsletters and websites. Any such correspondence will be coordinated through the District's PIO.
- Direct mail for notifications
  - Properties within 300 feet on either side of the centerline of the Build Alternative (Section 339.155 FS), as well as other local citizens who may be impacted by the construction of this project.
  - Elected and appointed public officials.
  - Individuals who request to be placed on the mailing list for this project.
  - Public and private groups, organizations, agencies, or businesses that request to be placed on the mailing list for this project.
- Presentations to local officials, as requested
- Public hearing, as required by Federal Regulations and State Law, will be held on January 12, 2021. The public hearing will include a 10-day comment period.
- Unscheduled meetings with the public, elected and appointed officials, public agencies, or civic groups only if deemed necessary by the FDOT.

Project Schedule Milestones are below:

08/31/2020	Mon	Project Execution
12/14/2020	Mon	Elected Official mailer arrives (-29 days) Criteria: -25-30 days
12/20/2020	Sun	Newspaper Ad # 1 publication (-23 days) Criteria: -15-30 days
12/21/2020	Mon	Property Owner mailer arrives (-22 days) Criteria: NLT -20 days
12/22/2020	Tue	Documents placed on display (-21 days)
01/03/2021	Sun	Newspaper Ad # 2 publication (-9 days) Criteria: -7-12 days
01/05/2021	Tue	Press Release distribution (-7 days)
01/05/2021	Tue	FAR notification publication (-7 days)
01/05/2021	Tue	FDOT website notification publication (-7 days)
01/12/2021	Tue	Public Hearing
01/22/2021	Fri	Public comment period ends (+10 days)
01/25/2021	Mon	SEIR complete (+13 days)

## 6.0 EVALUATION OF POTENTIAL EFFECTS

### 6.1 Project Effects

Transportation projects may result in various environmental effects both positive and negative. Guidance published in 40 CFR §§1500-1508 describes three (3) general effect categories including: direct, indirect, and cumulative. Each effect category is differentiated based on causation and timing of the effect. A description of effects are as follows:

- **Direct effects** are caused by the action and occur at the same time and place.
- **Indirect effects** are caused by the action and are later in time or farther removed in distance but still reasonably foreseeable.
- **Cumulative effects** result from the incremental effects of an action when added to other past, present, and reasonably foreseeable actions regardless of which agency or person undertakes the action.

As part of its ETDM process, the FDOT characterizes project effects. The Degree of Effect (DOE) indicates the relative intensity of a potential project effect and serves to standardize the description of effects in impact assessments. The DOE classification system will be used in this report to support the evaluation of direct, indirect, and cumulative effects. **Table 6-1** below introduces DOE classifications used by FDOT.

**Table 6 - 1: Project Degree of Effect Classifications**

N/A	Not applicable or no involvement
Enhanced (1)	Positive effect on resources
None (0)	No effect on resources
Minimal (2)	Little adverse effect on resources
Moderate (3)	Resources are affected; avoidance, minimization options available
Substantial (4)	Substantial interaction required; avoidance, minimization, mitigation
Dispute (5)	Does not conform to agency requirements

### 6.2 Efficient Transportation Decision Making (ETDM) Screening

The proposed project has been subjected to previous evaluation for sociocultural effects. The FDOT evaluated the project (ETDM #14455) as part of the ETDM programming screening process in 2020. Results of the previous evaluation are recorded in a project Summary Report included in **Appendix B**. The ETDM Summary Report includes preliminary assessment of environmental effects and documentation of regulatory agency coordination. **Table 6-2** summarizes the degree effect assigned by the participating and coordinating agencies including comments made by: US Environmental Protection Agency (EPA), National Parks Service (NPS), Natural Resources Conservation Service (NRCS), National Marine Fisheries Service (NMFS), US Army Corps of

Engineers (USACE), US Fish and Wildlife Service (FWS), US Coast Guard (USCG), South Florida Water Management District (SFWMD), FDOT District Six, Florida Department of Agriculture and Consumer Services (FDACS), Florida Fish and Wildlife Conservation Commission (FWC), Florida Department of Environmental Protection (FDEP), Florida Department of State, and Florida Department of Economic Opportunity.

**Table 6 - 2: Summary Degree of Effect Assigned During the ETDM Screening**

Social and Economic							Cultural			Natural				Physical						
Social	Economic	Land Use Changes	Mobility	Aesthetic Effects	Relocation Potential	Farmlands	Section 4(f) Resources	Historic and Archaeological Sites	Recreational and Protected Lands	Wetlands and Surface Waters	Water Resources	Floodplains	Protected Species and Habitat	Coastal and Marine	Noise	Air Quality	Contamination	Infrastructure	Navigation	Special Designations
2	2	2	1	2	2	N/A	N/A	2	2	2	2	2	2	2	2	3	3	0	N/A	

Topics described in the ETDM screening relevant to the SCE evaluation generally highlight minimal environmental effects. Results describe no effect to Navigation and no involvement regarding Farmlands, Section 4 (f) Resources, and Special Designations. Mobility is expected to be enhanced, while there is a moderate effect to both Contamination and Infrastructure. Minimal involvement is to be expected regarding the 14 remaining topics.

Moderate concern was expressed by the EPA related to potential contamination impacts to the Biscayne Sole Source Aquifer as well as that soils, groundwater, and surface waters have the potential to be negatively affected by contaminated sites. The EPA recommended that sampling and analysis of soils and groundwater be conducted should petroleum storage tanks be impacted or removed; corrective action should be completed before commencement of project activities. The FDEP stated that the project is not expected to significantly affect potential contaminated sites and that a Contamination Screening Evaluation similar to Phase I and Phase II Audits may need to be performed along the proposed project right-of-way.

Furthermore, the EPA noted concern that the project may result in adverse effects to minority and low-income populations along the project corridor.

USACE identified wetlands along the project corridor and noted the project will likely require the Standard Individual Permit but depending on the project development and planning process a Regional General Permit or a Nationwide permit could be used as the project develops and planning moves forward. USACE further noted that SFWMD canals C-6, C-7, and C-8 could trigger additional 408 reviews regarding proposed activities at these crossings.

The SCE effects discussion discussed in the following sections are categorized into a series of topics that represent elements of the human environment. These topics include Social, Economic, Land Use, Mobility, Aesthetics and Relocation.

### **6.3 Social**

This section presents the potential social impacts of the project on the communities within the study area. The analysis here considers the potential for effects on community groups and resources, potential demographic shifts including the influx or departure of populations, and the effect of the project in the creation/elimination of barriers to community interaction. Consideration is also given in this section to safety and emergency response impacts.

Based on the information included in the following subsections and primarily as a result of no needed right-of-way expansion, a Summary Degree of Effect of Minimal has been assigned to the Social issue.

#### **6.3.1 Demographics**

The project occurs in a highly developed, urbanized area of Miami-Dade County that supports little available vacant land. Much of the area in the southern half of the corridor supports light industrial use and warehousing, and the northern half supports primarily residential use.

Demographic analysis identifies the presence of historically disadvantaged and potentially vulnerable groups that include low-income and minority populations. Household income levels within the study area are slightly below the countywide average. Approximately 91 percent of the population present is Hispanic or Latino. Approximately 47 percent of the population indicates Limited English Proficiency.

Though potentially vulnerable populations are present, the proposed improvements include alterations along an existing corridor, primarily within the existing right-of-way. Some access and mobility benefits will result from the proposed action, but neither Alternative is likely to alter the existing transportation system to the extent that land use changes or population shifts occur. In or out-migration as a result of the project is not likely. Population composition and size are expected to be generally unaffected.

#### **6.3.2 Community Cohesion**

Community cohesion refers to the quantity and quality of interaction among people in a community and is exhibited by the degree to which residents know and care about their neighbors and participate in neighborhood activities. The community surrounding the corridor is developed and highly urbanized. Commercial use and Warehousing are the primary uses present along the southern half of the corridor. Residential development is prevalent north of US 27. Existing neighborhoods are oriented so that community edges and boundaries are generally defined by major roadways, including SR 826.

The proposed improvements to SR 826 occur primarily within the existing right-of-way, and would not result in the displacement of any homes or businesses. Additionally, none of the resources identified in the Community Features Inventory, included in **Section 4.4**, would be affected by the project. The proposed modification of lanes and structures within the existing corridor will not serve to divide or isolate an existing population or community. The project is anticipated to have little effect on community cohesion.

### **6.3.3 Safety/Emergency Response**

Proposed northbound safety improvements include widening the mainline within the NW 103<sup>rd</sup> Street interchange to provide the minimum 10-foot recommended shoulder width for both the inside and outside shoulders. In an effort to improve the existing weaving condition, three (3) southbound bridges (over NW 74<sup>th</sup> Street, Metrorail, and FEC Rail) will be widened in the Build Alternative.

Regarding emergency evacuations, SR 826 is designated as an emergency evacuation route by the Florida Division of Emergency Management. Emergency evacuation will be enhanced through the added capacity.

### **6.3.4 Community Goals and Quality of Life**

The Build Alternative is compatible with the Miami Dade Comprehensive Development Master Plan (CDMP). Objective TC-1 states that the County should strive to operate its roadway network at a level of service (LOS) higher than the base level. The Build Alternative fulfills this by improving congestion through operational improvements. Objective TE-2C of the Transportation Element notes that in road construction and reconstruction projects the design shall promote pedestrian comfort where the Land Use Element seeks to provide activity along roadway frontages. Additionally, TE-2H states that comfortable and safe sidewalk conditions for pedestrians should be ensured by the County. The Build Alternative will do so by infilling sidewalk gaps along West Frontage Road/NW 77<sup>th</sup> Avenue and East Frontage Road/W 20<sup>th</sup> Avenue. Fulfilling these objectives should improve the quality of life of residents and visitors of Miami-Dade County.

### **6.3.5 Special Community Designations**

One Opportunity Zone (Census Tract 91) is located West of SR 826, north of US 41, and south of US 27/ SR 25 within the quarter mile study area. These zones were established by Congress in 2017 as a tax incentive for reinvestment in low income communities. Qualified Opportunity Zones retain their designation for 10 years. Within each zone, investors can defer taxes on financial gains, so long as the gain is reinvested in a Qualified Opportunity Fund. Opportunity Zones are expected to spur public-private partnerships in disadvantaged communities. None of Miami Dade's 11 community redevelopment areas (CRAs) are located within the study area.

The transportation investment proposed is supportive of the private sector investment targeted within the Opportunity Zone. The Build Alternative is intended to adjust access to the managed lane system to expanding the area of northern Miami-Dade County served system. In turn, supporting additional access to areas targeted for investment.

## 6.4 Economic

This section presents a summary of the potential economic impacts of the project in the study area. Potential project effects on business and employment activity in the study area. Consideration is given to potential impacts to routes and access to identified businesses, employment centers, or community facilities.

### 6.4.1 Business and Employment

Multiple Industrial and freight warehouses as well as construction companies and retailers are located between NW 25<sup>th</sup> Street and NW 103<sup>rd</sup> Street. These include Dolphin Carpet and Tile, Petro Solutions, and FPE Forklift Parts and Equipment. The Westland Mall is adjacent to SR 826 and NW 103<sup>rd</sup> Street with Palmetto General Hospital located at the corner of SR 826 and NW 122<sup>nd</sup> Street.

Based on figures produced by the US Census Bureau reported in the Longitudinal Employer-Household Dynamics database, the quarter-mile area surrounding the corridor currently supports 33,826 jobs (**Table 6-3**). Health care and social assistance; retail trade; wholesale trade; and administration and support, waste management and remediation are the predominant sectors of employment along the corridor.

**Table 6 - 3: SCE Study Area Jobs by NAICS Industry Sector**

SCE Type	Sector	Percent
Agriculture, Forestry, Fishing and Hunting	38	0.1%
Mining, Quarrying, and Oil and Gas Extraction	3	0.0%
Utilities	127	0.4%
Construction	1,537	4.5%
Manufacturing	2,532	7.5%
Wholesale Trade	4,266	12.6%
Retail Trade	4,861	14.4%
Transportation and Warehousing	1,997	5.9%
Information	679	2.0%
Finance and Insurance	2,191	6.5%
Real Estate and Rental and Leasing	786	2.3%
Professional, Scientific, and Technical Services	1,109	3.3%
Management of Companies and Enterprises	312	0.9%
Administration & Support, Waste Management and Remediation	3,502	10.4%
Educational Services	628	1.9%
Health Care and Social Assistance	5,737	17.0%
Arts, Entertainment, and Recreation	154	0.5%

SCE Type	Sector	Percent
Accommodation and Food Services	2,286	6.8%
Other Services (excluding Public Administration)	1,038	3.1%
Public Administration	43	0.1%
Total	33,826	

Nearly two thirds of the jobs present earn less than \$3,333 dollars per month. As noted in **Table 4-6**, 86.1 percent of daily work trips made by residents are made using an automobile. Relocating the ingress point into the EL system to south of NW 103<sup>rd</sup> Street will benefit commuters by allowing those between NW 103<sup>rd</sup> Street and NW 154<sup>th</sup> Street access to the EL system. The anticipated reduction in congestion at NW 103<sup>rd</sup> Street by providing an extra throughput lane is expected to improve drive times for commuters to and from work.

According to the Texas A&M Transportation Institute's 2019 Urban Mobility Report the average auto commuter in 2017 spent 54 hours in congestion and wasted 21 gallons of fuel at a cost \$1,080. The average auto commuter in the Miami Urban Area experienced 69 hours of delay, used 34 gallons of excess gas, at a cost of \$1,412 in wasted time and fuel. Virtually every employment sector is dependent upon the efficient operation of the transportation system to support commerce. Anticipated operations and access improvements resulting from the Build Alternative will support the continued operation of businesses in the area.

#### 6.4.2 Tax Base

Due to the project requiring no additional right-of-way, no loss in revenue by Miami-Dade County is expected. Local businesses and neighborhoods will experience improved operational conditions resulting in a reduction in travel times, which may increase property values over time.

#### 6.4.3 Traffic Patterns

To better facilitate the movement of traffic into the EL system, the proposed Build Alternative will include relocating the existing EL ingress point from north of NW 154<sup>th</sup> Street to south of NW 103<sup>rd</sup> Street. This will create access to the EL system for residents of Hialeah as well as other residents in this portion of Miami-Dade County. This is expected to change traffic patterns in this region for those who choose to utilize the EL system. GP lane throughput improvements at NW 103<sup>rd</sup> Street may cause an increase in commuters utilizing SR 826 as opposed to secondary roadways due to the reduced congestion

#### 6.4.4 Business Access

Business access is expected to be improved overall as speed differential between the general-purpose lanes and ELs equalize. Additionally, those living within Hialeah will now have access to the ELs when heading southbound. The southbound ELs currently begin north of NW 154<sup>th</sup> Street and do not provide another ingress point within the project limits. The Build Alternative includes relocating the existing ELs ingress point from north of NW 154<sup>th</sup> Street to south of NW 103<sup>rd</sup>

Street. This will create better access for residents of Hialeah as well as other residents in this portion of Miami-Dade County. Pedestrian access to businesses along West Frontage Road/NW 77<sup>th</sup> Avenue and East Frontage Road/W 20<sup>th</sup> Avenue will also be enhanced through continuous sidewalk coverage within the frontage road project limits. Businesses that operate along the corridor may experience temporary impacts to travel during construction; however, in the long term, mobility is expected to be enhanced.

#### **6.4.5 Special Needs Patrons**

Providing an additional throughput lane at NW 103<sup>rd</sup> Street is expected to reduce congestion not only for commuters, but for trucks moving freight as SR 826 is a part of the SIS highway network. As a designated SIS highway corridor, SR 826 provides access via NW 74<sup>th</sup> Street (also known as Hialeah Expressway) and surface streets to the Miami Hialeah Florida East Coast (FEC) Railway Intermodal Terminal, a SIS Freight Rail Terminal. According to the Texas A&M Transportation Institute's 2019 Urban Mobility Report the cost of increased travel time, operating cost, and diesel for freight in the Miami Urban Area in 2017 is \$565 million.

### **6.5 Land Use**

The following section identifies the project's consistency with local and regional land use and transportation plans and evaluates the project's consistency with the physical character of the area. The land use analysis considers the project's compatibility with the community's existing/planned land use patterns and urban form. Additionally, the evaluation includes the identification of the potential for effects on unique community features (e.g., historic landmarks/structures), and changes in acreage devoted to public spaces including conservation lands and parks.

Based on information included in the following subsections, and general consistency of the project with local plans, a Summary Degree of Effect of Minimal has been assigned to the land use issue.

#### **6.5.1 Land Use – Urban Form**

Existing and future land use was assessed through review of statewide generalized land use files. Geographic Information Systems (GIS) shapefiles were downloaded from the FGDL and compared to the SCE study area. **Table 6-4** reports generalized existing land use by zoning description found within the study area. The predominant land use present is Industrial (29.71%), followed by Residential (20.26%), and Retail Office (19.10%).

**Table 6 - 4: SCE Study Area Existing Land Use**

Existing Land Use	Acres	Percent
Acreage Not Zoned for Agriculture	81.59	2.93%
Agricultural	9.73	0.35%
Centrally Assessed	6.74	0.24%
Industrial	827.02	29.71%
Institutional	87.83	3.15%
Parcels with No Values	3.47	0.12%
Public/Semi-Public	149.45	5.37%
Recreation	71.32	2.56%
Residential	563.91	20.26%
Retail/Office	531.67	19.10%
Right-Of-Way	0.07	0.00%
Vacant Nonresidential	102.88	3.70%
Vacant Residential	326.13	11.71%
Water	22.22	0.80%

According to **Table 6-5**, which reports generalized future land use, the study area is expected to see an increase in industrial uses. Occurring well within the Miami-Dade urban growth boundary, SR 826, a limited access expressway, is consistent with the character and intensity of surrounding development. This project is not expected to affect the current or future land use of the area. No additional right-of-way will be required. Therefore, the acreage per land use type is not expected to change as a direct result of the project. Furthermore, no impacts to recreation and open space are expected by the Build Alternative.

**Table 6 - 5: SCE Study Area Future Land Use**

Future Land Use	Acres	Percent
Commercial	296.11	12.17%
Industrial	1268.80	52.13%
Institutional	29.67	1.22%
Mixed Use - General	23.27	0.96%
Recreation	94.50	3.88%
Residential High (> 12 DU/AC)	403.89	16.59%
Residential Medium (5.01 - 12 DU/AC)	211.70	8.70%
Transportation/Utilities	24.39	1.00%
Water	81.72	3.36%

## 6.5.2 Local Plan Consistency

Local planning documents including LRTPs, local comprehensive plans, and sub-area plans help local governments establish priorities in investment and identify specific initiatives.

SR 826 Mainline, FM# 447165-1-22-01: The project is included in the FDOT FY 2020 - FY 2024 Work Program for a PD&E Study and Railroads and Utilities that continues through 2021 and Preliminary Engineering in 2022. The project is also included in the FDOT FY 2020 - FY 2024 STIP for a PD&E Study in 2021. The project is not included in the Miami-Dade TPO's FY 2021

- FY 2025 TIP. The project is not included in the Miami-Dade TPO's 2045 LRTP nor in the FDOT's SIS First Five-Year Plan.

SR 826/Frontage Roads, FM# 441831-1-22-01 and 441830-1-22-01: Both projects are included in the FDOT FY 2020 - FY 2024 Work Program for Preliminary Engineering in 2021 and Contract Incentives and Design Build in 2024. Both projects are also included in the FDOT FY 2020 - FY 2024 STIP for Preliminary Engineering in 2021 and Contract Incentives and Design Build in 2024. The projects are additionally included in the Miami-Dade TPO's FY 2021 - FY 2025 TIP for Resurfacing. The projects are not included in the Miami-Dade TPO's 2045 LRTP.

### **6.5.3 Open Space and Focal Points**

Due to the project improvements taking place within existing right-of-way no impacts are expected to community open space or focal points. Access to and the enjoyment of the these and other community features listed in **Section 4.4** will likely be enhanced due to reduced congestion and improved traffic flow within the general-purpose lanes.

### **6.5.4 Growth Patterns and Trends**

A population and employment analysis conducted by the Miami-Dade TPO for the 2045 LRTP shows that between 2015 and 2045 population is expected to grow by approximately 920,000 (34%) and employment is expected to grow by over 500,000 (38%) within the County. Traversing two (2) of the seven (7) transportation planning areas (Central and Northwest) the greatest employment growth within the County is expected to occur within the Northwest transportation planning area. This area expects an employment increase of 44.1 percent and a modest increase in population of 27.1 percent. The Central transportation planning area expects a lower rate of population and employment growth (32.7% and 36.6% respectively) when compared to the countywide averages. The projected growth in the area will result in a significant increase in travel demand and further deteriorate the conditions on the already congested SR 826 corridor.

## **6.6 Mobility**

This section identifies potential project effects on mobility and accessibility in the study area with emphasis on non-driving population groups (i.e., elderly, young, disabled, and low-income individuals). Changes to existing travel patterns, traffic circulation, or accessibility were assessed. The effects of tolling on low-income populations were considered based on the modification of the EL system.

Based on information included in the following subsections and improvement of accessibility and mobility overall, a Summary Degree of Effect of Enhanced has been assigned to the Mobility issue.

### **6.6.1 Modal Choices**

The Palmetto Metrorail Station is located adjacent to SR 826 and NW 77<sup>th</sup> Street. The Build Alternative will not result in any changes to the Metrorail line or the Palmetto Station. Metrorail is a dual track heavy rail system that operates throughout Miami-Dade County. Sixteen bus routes

operate with the quarter-mile study area. Of these routes, Route 175 operates within SR 826 as an express service between the new park and ride lot at I-75 and Miami Gardens Drive to the Palmetto Metrorail Station via I-75 and Miami Gardens Drive. However, this route is temporarily suspended due to Covid 19. FDOT allows Miami Dade transit vehicles to use the express lanes free of charge, which improves frequency and reliability.

Additionally, the Flamingo and Marlin routes and Route 29 operates within the frontage road improvement limits. Pedestrian access to businesses along West Frontage Road/NW 77<sup>th</sup> Avenue and East Frontage Road/W 20<sup>th</sup> Avenue will also be enhanced through continuous sidewalk coverage within the frontage road project limits. This will make it easier for pedestrians to access the bus stops along the frontage road from their point of origin. Due to limited right-of-way, continuous bike lanes are not a part of this project.

### **6.6.2 Transportation Disadvantaged**

SR 826 is an existing facility that is a part of Florida's turnpike system. The EL improvements proposed for the Build Alternative would involve variable tolled pricing otherwise known as congestion pricing. Congestion pricing bases the amount tolled on how congested the adjacent lanes (non-variable priced lanes) are and vice versa. Any motorists who choose to travel in the ELs could experience an economic impact.

ELs can benefit all drivers, including disadvantaged populations as congestion is reduced in the general-purpose lanes by those who chose to drive in the variable-priced lane. Regardless of income category, the County benefits from these lanes through enhanced service for public service vehicles (police, fire, and ambulances). Disadvantaged populations are historically more likely to use transit as a mode of transportation. Transit users do not pay to use the ELs due to an agreement between the transit agencies and FDOT. As stated earlier, this improves the frequency and reliability.

### **6.6.3 Connectivity and Traffic Circulation**

The proposed southbound improvements include an additional general-purpose lane through the NW 103<sup>rd</sup> Street interchange and an extension of the auxiliary lane between NW 74<sup>th</sup> Street and US 27 interchanges. This will take place by modifying the NW 103<sup>rd</sup> Street westbound to southbound flyover ramp pier, thereby widening the SR 826 mainline bridge and mechanically stabilized earth (MSE) walls. By milling and resurfacing, restriping, and moving the delineators an additional general-purpose lane would exist southbound between NW 103<sup>rd</sup> Street and NW 154<sup>th</sup> Street. To improve weaving the southbound bridges over NW 74<sup>th</sup> Street, Metrorail, and FEC Rail will be widened in the Build Alternative. In addition, the existing southbound EL ingress currently located at north of NW 154<sup>th</sup> Street is expected to be relocated to NW 103<sup>rd</sup> Street in order to provide better access to residents in Hialeah, FL. In order to provide the minimum 10-foot recommended shoulder width northbound for both the inside and outside shoulders the mainline of the NW 103<sup>rd</sup> Street interchange will be widened.

## **6.6.4 Public Parking**

As previously noted, no additional right-of-way will be required for this project. Therefore, no official public parking for any residence or business will be affected. It should be noted when looking at aerial imagery dated February 1, 2020 cars were parked on the East Frontage Road/W 20<sup>th</sup> Avenue north of W 44<sup>th</sup> PL in Hialeah, FL, which is within FDOT right-of-way. Based on property appraiser records the parked cars appear to be residents or visitors of the Towers of Westland Condominium Complex. Coordination with the complex has been ongoing and should continue as the PD&E Study moves forward.

## **6.7 Aesthetics**

This section assesses the project's compatibility with the community's aesthetic values related to noise, vibration, and physical appearance. The section examines the type and intensity of project impacts on noise sensitive sites (e.g., residential areas, hotels, nursing homes, and parks); vibration sensitive sites (e.g., residential uses, eye clinics, dentist offices, and hospitals); special viewsheds and vistas; community focal points; historic structures, districts, and landmarks; and community character.

Based on information included in the following subsections describing noise and visual impacts, a Summary Degree of Effect Minimal has been assigned to the Aesthetics issue.

### **6.7.1 Noise and Vibration**

A traffic noise study was performed in accordance with 23 CFR 772: Procedures for Abatement of Highway Traffic Noise and Construction Noise, the FDOT's PD&E Manual, Part 2, Chapter 18, Highway Traffic Noise, FDOT's Traffic Noise Modeling and Analysis Practitioners Handbook, and the FHWA guidance document Highway Traffic Noise: Analysis and Abatement Guidance.

Per the Noise Study Report (NSR), design year (2045) traffic noise levels for the Build Alternative will approach [i.e., within 1 dB(A)], meet, or exceed the Noise Abatement Criteria (NAC). Residences are expected to range from approximately 53.7 to 79.0 dB(A) during the project's design year. Build Alternative traffic noise levels at the non-residential/special-use sites are expected to range from approximately 37.2 dB(A) inside the Palmetto General Hospital to 76.7 dB(A) at the Mater Academy Elementary School playground. The worst-case design year traffic noise levels with the Build Alternative are predicted to be no more than 1.9 dB(A) greater than existing levels and 1.5 greater than the design year No-Build noise levels.

Traffic noise levels in the design year are expected to exceed the NAC at 416 residencies [67 dB(A)]. Additionally, noise levels will approach or exceed the NAC at the following sites: Florida National University (2) – outdoor tables and playground [NAC-67 dB(A)]; Don Quijote Plaza – park [NAC-67 dB(A)]; Mater Academy Elementary School – playground [NAC-67 dB(A)]; and, Don Camaron Seafood – Restaurant Outdoor seating area [NAC-72 dB(A)].

Noise abatement measures were considered for all sites (see **Table 6-6**). Although a number of sites approach or exceed the NAC, the proposed improvements did not result in any substantial noise increases [i.e., at least 15 dB(A) over existing levels]. Of the 9 locations that noise barriers (see **Table 6-7**) were considered, none are recommended for construction due to not meeting FDOT's Noise Reduction Design Goal or were not feasible for construction.

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**Table 6 - 6: Modeled Noise Receptor Locations and Noise Analysis Results**

Representative Model Receptor	Location	Type	Description (Noise Abatement Activity Category)	FDOT Noise Abatement Approach Criteria [dB(A)]	Location (Station)	Number Of Noise Sensitive Sites	Distance To Nearest Traffic Lane* [Existing/ No-Build/Build] (Feet)	Predicted Traffic Noise Levels		
								[LAeq1h, dB(A)]		
								Existing (2019)	Design Year (2045)	
									No-Build	Build
SR 826 – US 27 to NW 103 <sup>rd</sup> Street										
CFN	Citrus Family Network	Medical	Medical Facility Interior (D)	51	505+00	1	160/160/160	47.3	47.8	48.2
SWH	Southern Winds Hospital	Medical	Medical Facility Interior (D)	51	507+80	1	175/175/175	46.9	47.4	47.8
FNU-Tables	Florida National University	School Common Area	School (C)	66	514+00	SLU	165/165/165	69.0	69.5	69.7
FNU-PG	Florida National University	School Playground	School Playground (C)	66	514+00	SLU	230/230/230	67.3	67.8	68.1
ToW R1(a,b,c,d,e)	Towers of Westland	MFR	Residential (B)	66	516+60	2/2/2/2	190/190/190	66.6, 69.3, 69.9, 70.8, 71.3	67.0, 69.8, 70.4, 71.2, 71.7	67.4, 70.1, 70.8, 71.6, 72.2
ToW R2 (a,b,c,d,e)		MFR	Residential (B)	66	516+60	2/2/2/2	300/300/300	63.6, 66.8, 67.7, 68.2, 69.1	64.0, 67.3, 68.2, 68.7, 69.6	64.4, 67.6, 68.6, 69.2, 70.0
ToW R3 (a,b,c,d,e)		MFR	Residential (B)	66	516+80	1/1/1/1	175/175/175	71.1, 72.9, 73.6, 74.3, 74.6	71.6, 73.4, 74.1, 74.7, 75.0	72.0, 73.7, 74.5, 75.3, 75.6
ToW R4 (a,b,c,d,e)		MFR	Residential (B)	66	518+00	7/7/7/7	230/230/225	68.3, 69.2, 70.5, 71.1, 71.6	68.7, 69.7, 70.9, 71.5, 72.1	69.2, 70.1, 71.5, 72.0, 72.6
ToW R5 (a,b,c,d,e)		MFR	Residential (B)	66	519+80	6/6/6/6	115/115/110	70.6, 74.4, 75.3, 75.4, 75.6	71.1, 74.8, 75.8, 75.9, 76.1	71.7, 75.2, 76.4, 76.6, 76.8
ToW R6 (a,b,c,d,e)		MFR	Residential (B)	66	521+20	4/4/4/4	60/60/55	67.8, 76.2, 77.5, 77.7, 77.8	68.4, 76.7, 78.1, 78.2, 78.3	69.3, 77.4, 78.8, 78.9, 79.0
ToW R7 (a,b,c,d,e)		MFR	Residential (B)	66	521+80	2/2/2/2	75/75/80	65.1, 69.4, 72.1, 72.9, 73.2	65.5, 69.9, 72.7, 73.4, 73.7	66.8, 71.3, 73.1, 74.0, 74.4
ToW R8 (a,b,c,d,e)		MFR	Residential (B)	66	521+80	2/2/2/2	145/145/140	64.5, 67.0, 69.0, 70.4, 70.7	64.9, 67.5, 69.6, 70.9, 71.2	65.5, 68.5, 70.0, 71.3, 71.7
RAM-P	Ramada Inn	Hotel Pool	Sensitive Commercial (E)	71	526+00	SLU	180/180/175	63.0	63.4	63.0
DQPlaza	Don Quijote Plaza	Park	Park (C)	66	529+40	SLU	95/95/90	71.0	71.3	71.0
DCR-Tables	Don Camaron Seafood	Outdoor Seating Area	Sensitive Commercial (E)	71	504+60	SLU	130/130/130	72.1	73.0	73.5
MPG	Mater Academy Elementary	School Playground	School Playground (C)	66	512+80	SLU	65/65/65	75.7	76.5	76.7
SR 826 – NW 103 <sup>rd</sup> Street to NW 122 <sup>nd</sup> Street										
HJPool	Howard Johnson Pool	Hotel	Sensitive Commercial (E)	71	533+40	SLU	245/245/245	66.1	66.2	66.5
WE1 (b,c,d,e)	Westland Eden	MFR	Residence (B)	66	546+00	5,5,5,5	140/140/140	74.9, 75.7, 76.1, 76.1	75.2, 76.0, 76.4, 76.5	75.4, 76.3, 76.7, 76.8
WE2 (b,c,d,e)		MFR	Residence (B)	66	547+20	2,2,2,2	175/175/175	71.4, 72.3, 72.9, 73.1	71.6, 72.6, 73.3, 73.5	71.9, 72.8, 73.4, 73.7
WV1	West Lake, Westland Village	MFR	Residence (B)	66	546+00	2	175/175/175	70.4	71.0	71.9
WV2		MFR	Residence (B)	66	546+00	2	250/250/250	67.7	68.2	69.1
WV3		MFR	Residence (B)	66	547+80	6	150/150/150	71.5	72.1	72.5
WV4		MFR	Residence (B)	66	548+00	7	260/260/260	61.5	62.2	62.3
WV5		MFR	Residence (B)	66	550+20	4	190/190/190	67.3	68.2	68.3
WV6		MFR	Residence (B)	66	551+80	2	270/270/270	65.8	66.6	66.7
WV7		MFR	Residence (B)	66	551+80	4	190/190/190	69.0	69.8	69.9
MG1(a,b,c)	Meadowgreen	MFR	Residence (B)	66	549+00	5,5,5	245/245/245	64.0, 66.6, 69.6	64.3, 66.8, 69.9	65.1, 67.2, 70.1
PWG1(b,c,d,e)	Palm West Gardens	MFR	Residence (B)	66	551+80	6,6,6,6	120/120/120	75.1, 75.9, 76.2, 76.3	75.3, 76.2, 76.6, 76.7	75.5, 76.4, 76.8, 76.8
PWG2(b,c,d,e)		MFR	Residence (B)	66	554+20	6,6,6,6	125/125/125	74.9, 75.7, 76.2, 76.2	75.1, 76.0, 76.4, 76.5	75.3, 76.1, 76.6, 76.7
CH1	Unnamed Community	SFH	Residence (B)	66	555+40	5	160/160/160	72.6	73.3	73.5
CH2		SFH	Residence (B)	66	555+40	5	325/325/325	63.3	64.0	64.0
CH3		SFH	Residence (B)	66	559+00	9	165/165/165	70.8	71.3	71.5
CH4		SFH	Residence (B)	66	559+00	9	325/325/325	61.2	61.8	62.0
CH5		SFH	Residence (B)	66	561+20	5	165/165/165	69.6	70.0	70.2

Representative Model Receptor	Location	Type	Description (Noise Abatement Activity Category)	FDOT Noise Abatement Approach Criteria [dB(A)]	Location (Station)	Number Of Noise Sensitive Sites	Distance To Nearest Traffic Lane* [Existing/ No-Build/Build] (Feet)	Predicted Traffic Noise Levels		
								[LAeq1h, dB(A)]		
								Existing (2019)	Design Year (2045)	
									No-Build	Build
CH6		SFH	Residence (B)	66	561+20	5	320/320/320	63.6	64.2	64.4
CH7		SFH	Residence (B)	66	565+00	20	300/300/300	56.7	57.3	56.5
WG1(a,b,c,d)	Westland Gardens	MFR	Residence (B)	66	556+40	4,4,4,4	155/155/155	71.2, 73.7, 74.7, 75.3	71.3, 73.9, 74.9, 75.5	<b>71.5, 74.0, 75.0, 75.6</b>
WG2(a,b,c,d)		MFR	Residence (B)	66	555+60	2,2,2,2	210/210/210	64.9, 68.1, 68.8, 69.8	65.1, 68.2, 69.0, 70.0	65.3, <b>68.4, 69.2, 70.1</b>
WG3(a,b,c,d)		MFR	Residence (B)	66	561+00	4,4,4,4	150/150/150	69.1, 71.6, 74.2, 75.0	69.2, 71.8, 74.3, 75.2	<b>69.6, 72.0, 74.5, 75.4</b>
WG4(a,b,c,d)		MFR	Residence (B)	66	558+00	2,2,2,2	300/300/300	65.2, 68.1, 69.6, 70.0	65.4, 68.2, 69.7, 70.2	65.7, <b>68.4, 69.9, 70.3</b>
WG5(a,b,c,d)		MFR	Residence (B)	66	560+60	2,2,2,2	200/200/200	65.7, 68.1, 70.5, 71.0	65.9, 68.2, 70.6, 71.2	<b>66.2, 68.5, 70.8, 71.4</b>
CP1(a,b)	Conquistador Park	MFR	Residence (B)	66	563+40	6,6	85/85/85	69.8, 72.4	69.9, 72.5	<b>70.1, 72.8</b>
PGN1(b,c,d)	Palmetto, Palmetto Gardens North	MFR	Residence (B)	66	572+40	11,11,11	85/85/85	71.8, 75.8, 76.9	71.9, 75.9, 77.1	<b>72.2, 76.1, 77.3</b>
VL1(b,c,d)	Villa Luisa, Andes	MFR	Residence (B)	66	576+00	12,12,12	80/80/80	74.1, 76.6, 77.1	74.3, 76.8, 77.4	<b>74.6, 77.0, 77.6</b>
PSL1	Palm Springs Lakes	SFH	Residence (B)	66	582+00	2	150/150/150	68.0	68.3	<b>68.6</b>
PSL2		SFH	Residence (B)	66	582+00	2	70/70/70	72.9	73.1	<b>73.4</b>
PSL3		SFH	Residence (B)	66	584+20	2	110/110/110	71.5	71.7	<b>72.1</b>
PSL4		SFH	Residence (B)	66	584+20	2	190/190/190	66.6	66.9	<b>67.4</b>
LA1(a,b,c)	Los Arboles	MFR	Residence (B)	66	587+40	4,4,4	405/405/405	64.4, 68.3, 69.7	64.8, 68.7, 70.0	65.3, <b>69.1, 70.4</b>
LA2(a,b,c)		MFR	Residence (B)	66	587+40	4,4,4	600/600/600	59.9, 62.8, 64.9	60.2, 63.0, 65.1	60.7, 63.4, 65.4
LA3(a,b,c)		MFR	Residence (B)	66	588+40	3,3,3	465/465/465	64.1, 66.5, 67.2	64.5, 66.9, 67.6	65.1, <b>67.3, 68.1</b>
LA4(a,b,c)		MFR	Residence (B)	66	589+60	2,2,2	720/720/720	61.3, 63.4, 64.0	61.7, 63.8, 64.4	62.2, 64.3, 64.9
HI-Pool	Holiday Inn Pool	Hotel	Sensitive Commercial (E)	71	589+20	SLU	430/430/430	61.2	62.2	62.5
SR 826 – NW 122 <sup>nd</sup> Street to I-75										
PGH	Palmetto General Hospital	Medical	Medical Facility Interior (D)	51	603+00	SLU	530/530/530	36.1	37.3	37.2
RP	Royal Palms	MFR	Residence (B)	66	816+00	10	225/225/225	52.9	53.5	53.7
PRV1	Poinciana Royale Villas	MFR	Residence (B)	66	817+80	4	160/160/160	70.2	71.0	<b>71.2</b>
PRV2	Poinciana Royale Villas	MFR	Residence (B)	66	819+60	4	155/155/155	70.2	71.2	<b>71.2</b>
HCofC <sup>†</sup>	Hialeah Church of Christ	Religious Facility	Church Interior (D)	51	827+40	SLU	255/255/255	45.5	46.4	46.5
SR 826 – I-75 to NW 154 <sup>th</sup> Street										
BMP	Baptist Medical Plaza	Medical	Medical Facility Interior (D)	51	877+80	SLU	85/85/85	46.3	46.5	43.2
CH	Catholic Hospice	Medical	Medical Facility Interior (D)	51	881+00	SLU	85/85/85	46.4	46.6	45.7
NCML	Nicklaus Children's Miami Lakes	Medical	Medical Facility Interior (D)	51	887+80	SLU	95/95/95	46.9	47.1	48.6

Notes: \* = To existing edge-of-pavement of the nearest travel lane. **Bold/Red** numbers indicate Build Alternative noise levels equal or exceeding FDOT Noise Abatement Criteria  
 SFH = Single-Family Home, MFR = Multi-Family Residential (i.e., apartments, condominiums), SLU = Special Land Use site  
 a = 1<sup>st</sup> floor, b = 2<sup>nd</sup> floor, c = 3<sup>rd</sup> floor, d = 4<sup>th</sup> floor and e = 5<sup>th</sup> floor.

**Table 6 - 7: Noise Barrier Recommendations**

General Location (Cross Streets or Address)	Noise Barrier Conceptual Design	Noise Barrier Type	Height (feet)	Length (feet)	Limits (Begin/End Station Number)	Number of Benefited Receptors (Impacted/Not Impacted/Total)	Average (Maximum) Noise Reduction for all Benefited Receptor Sites dB(A)	Estimated Overall Cost (Based on \$30 per square foot Unless Otherwise Noted)	Estimated Cost/Site Benefited	Meets FDOT's Reasonable Cost Criteria (\$42,000/Site Benefited Unless Otherwise Noted)	Meets FDOT's Noise Reduction Design Goal	Noise Barrier Recommended for Further Consideration and Community Input	Comments
CNE-FNU Florida National University Picnic Tables and Playground East Side – W 42 <sup>nd</sup> Place to W 44 <sup>th</sup> Place	FNU-CD1	Shoulder-Mounted	14	610	510+80 to 516+90	1/0/0	6.4 (6.4)	\$397,800	N/A	N/A	No	No	Not Recommended – Does not achieve 7.0 dB(A) at any of the benefited sites. Also, based on needed usage, cost exceeds FDOT's Noise Barrier Cost Reasonable Criteria for Special Use Sites.
		Structure-Mounted	8	590	516+90 to 5522+85								
CNE-TOW Towers of Westland Patios and Balconies East Side – W 44 <sup>th</sup> Place and NW 103 <sup>rd</sup> Street	ToW-CD4	Shoulder-Mounted	14	510	511+80 to 516+90	28/2/30	6.2 (7.8)	\$383,400	\$12,780	Yes	No	No	Not Recommended – Achieves FDOT's noise reduction design goal of at least a 7.0 dB(A) reduction for at least one impacted receptor site. A detailed cost estimate has been prepared for this noise barrier design concept to account for additional noise barrier-specific cost such as modifying the retaining wall, drainage, utilities, signing, Intelligent Transportation Systems (ITS) and lighting that are required in order to construct this noise barrier. This cost estimate can be found in Appendix D.
		Structure-Mounted	8	705	516+90 to 5523+90								
CNE-DCS Don Camaron Seafood Outdoor seating area West Side – NW 95 <sup>th</sup> Street	DCS-CD2	Shoulder-Mounted	14	900	500+80 to 509+80	1/0/1	7.1 (7.1)	\$378,000	See Table 5.5	Cost exceeds reasonableness criteria for Special Use Sites	Yes	No	Not Recommended – Based on needed usage, cost exceeds FDOT's Noise Barrier Cost Reasonable Criteria for Special Use Sites.
CNE-MPG Mater Academy Elementary Playground West Side – NW 98 <sup>th</sup> Street	MPG-CD2	Shoulder-Mounted	14	700	508+80 to 515+80	1/0/1	7.0 (7.0)	\$294,000	See Table 5.7	Cost exceeds reasonableness criteria for Special Use Sites	Yes	No	Not Recommended – Based on needed usage, cost exceeds FDOT's Noise Barrier Cost Reasonable Criteria for Special Use Sites.
CNE-E1 Westland Eden, Meadowgreen, Palm West Gardens, Westland Gardens and Conquistador Park W 54 <sup>th</sup> Street to W 60 <sup>th</sup> Street Patios and Balconies East Side – W 54 <sup>th</sup> Street to W 60 <sup>th</sup> Street	E1-CD3	Shoulder-Mounted	14	1,300	543+80 to 556+80	36/7/43	6.5 (8.1)	\$738,000	\$17,163	Yes	No	No	Not Recommended - Achieves FDOT's noise reduction design goal of at least a 7.0 dB(A) reduction for at least one impacted receptor site. A detailed cost estimate has been prepared for this noise barrier design concept to account for additional noise barrier-specific cost such as modifying the retaining wall, drainage, utilities, signing, Intelligent Transportation Systems (ITS) and lighting that are required in order to construct this noise barrier. This cost estimate can be found in Appendix D.
		Structure-Mounted	8	800	556+80 to 564+80								
CNE-W1 West Lake, Westland Village and Unnamed Townhomes Patios West Side – W 53 <sup>rd</sup> Street to W 58 <sup>th</sup> Street	W1-CD1	Shoulder-Mounted	8	2,690	6535+80 to 566+80	0/0/0	N/A	\$645,600	N/A	N/A	No	No	Not Recommended – Does not achieve 7.0 dB(A) at any of the benefited sites.
CNE-E2 Palmetto, Palmetto Gardens North, Villa Luisa, Andes, Palm Springs Lakes and Los Arboles Patios and Balconies East Side – W 60 <sup>th</sup> Street to NW 122 <sup>nd</sup> Street	E2-CD1	Structure-Mounted	8	2,400	568+80 to End of NB Off-Ramp to NW 122 <sup>nd</sup> Street	0/0/0	N/A	\$576,000	N/A	N/A	No	No	Not Recommended – Does not achieve 7.0 dB(A) at any of the benefited sites.
CNE-PRV Poinciana Royale Villas Patios West Side – W 75 <sup>th</sup> Street to W 76 <sup>th</sup> Street	PRV-CD1	Structure-Mounted	8	1,300	611+85 to 624+85	0/0/0	N/A	\$312,000	N/A	N/A	No	No	Not Recommended – Does not achieve 7.0 dB(A) at any of the benefited sites.

To be deemed reasonable, a noise barrier must, at a minimum, meet two (2) important FDOT criteria:

- The estimated construction cost cannot exceed the FDOT's reasonable cost criteria of \$42,000 per benefited receptor site; and,
- According to the FDOT's noise reduction reasonableness criteria, the noise barrier must reduce noise levels by at least seven (7) dB(A) at one (1) or more impacted receptor sites.

Based on the noise analyses performed to date, there are no apparent solutions available to mitigate the noise impacts at any of the 416 impacted residences and the five (5) impacted special land use sites. The traffic noise impacts to these noise sensitive sites are considered to be an unavoidable consequence of the project. For additional information please see the NSR in the SWEPT file at FDOT offices.

### **6.7.2 Viewsheds and Compatibility**

Impacts of views/vistas from/of SR 826 are anticipated to be limited as the roadway is an existing facility. The Build Alternative involves bridge widening at NW 103<sup>rd</sup> Street, however, the impacts are expected to be minimal. This section of the SR 826 corridor is approximately 9.5 miles and is urbanized, supporting a wide range of land uses at varying densities. Development along the corridor incorporates larger structures such as the Westland Mall, Palmetto General Hospital, and numerous hotels and office buildings. These larger buildings along with transportation infrastructure including rail lines, bridges, and roadways all contribute to the existing visual character of the area where large built elements are not uncommon. Residential parcels make up approximately 20 percent of the land use within a quarter mile of the project and occur at a smaller scale. As existing structures these residences are unlikely to experience viewshed effects. Some homeowners and businesses may see effects from the removal of trees, which often serve as a visual and sound barrier between roadways and homeowners. However, the Miami-Fort Lauderdale-Port St Lucie Metropolitan Area is among the ten (10) largest metropolitan areas in the nation. Major infrastructure is common in major cities and would not be unique to SR 826. The proposed roadways would generally fit with the character of other major infrastructure elements found in Miami-Dade County.

Continued public coordination is recommended to identify context sensitive design solutions that lessen the visual impact of the Build Alternative. Based on the limited potential for contention related to project aesthetics, a Summary DOE Minimal has been assigned to the Aesthetics issue.

### **6.8 Relocations**

The conceptual plans presented for the Build Alternative identify no areas of expanded right-of-way. Therefore, no relocations would occur as a result of the Build Alternative and a Summary Degree of Effect of Minimal has been assigned to the Relocations issue.

The proposed project, as presently conceived, will not displace any residences or businesses within the community. Should this change over the course of the project, the FDOT will carry out a Right

of Way and Relocation Assistance Program in accordance with Florida Statute 421.55, Relocation of displaced persons.

## 7.0 ENVIRONMENTAL JUSTICE, CIVIL RIGHTS, and RELATED ISSUES

### 7.1 Protected Populations in the Study Area

SCEs are conducted to identify and address impacts resulting from a proposed action on the surrounding communities and engage those communities in the project development process. In this effort, special attention is paid to historically disadvantaged groups.

Executive Order 12898, “Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations,” signed by the President on February 11, 1994, directs Federal agencies to identify and address, as appropriate, disproportionately high and adverse health or environmental effects of their programs, policies, and activities on minority populations and low-income populations.

The EPA’s Council on Environmental Quality (CEQ) provides guidance in identifying the presence of protected populations at rates more likely to result disproportionate negative effect. Those thresholds are described below.

- A 50 percent criterion population analysis to determine those area geographies where minority and/or low-income individuals equal to or exceeded 50 percent of the population.
- A meaningfully greater criterion analysis in which minority and/or low-income population percentages within individual geographies (census block groups) were compared to the reference population (County) and found to exceed the reference area population.

The demographic analysis included in **Section 4.3** shows that the minority population (race and Hispanic combined) is 93.8 percent, while the County average is 86.6 percent. Additionally, 90.7 percent of the population is Hispanic, which is larger than the County average of 68 percent. The presence of such a large minority population makes the occurrence of a disproportionate effect more likely within the project study area. It should be noted that the percent of the population below the poverty level is similar between the study area and the County (19% and 18% respectively). The presence of a relatively similar poverty level makes the occurrence of a disproportionate negative effect on that population less likely.

### 7.2 Summary of Effects

The SCE Evaluation is intended to be a planning tool that incorporates community values, quality of life, and socioeconomic interests into the transportation planning process. As documented in this evaluation, development of the proposed lane modifications, and frontage road improvements would result in a range of community effects.

Project effects related to six (6) key socioeconomic areas are documented in **Table 6-1**. As summarized in the table, the effects of the project on the surrounding community are expected to be limited. The greatest impacts of the proposed action include expected reductions in traffic congestion and improvements in access and mobility. Negative impacts focus primarily on minor

increases in existing noise levels. No right-of-way will be required to support the proposed action thus proximity impacts are minimal.

### **7.3 Disproportionate Adverse Effects**

Two factors must coincide for a disproportionate adverse effect to occur, presence of adverse effects and presence of a protected population. The CEQ provides guidance on identifying a disproportionate adverse effect on minority and/or low-income population. Stating this effect occurs when:

- The adverse effect occurs primarily to a minority and/or low-income population or
- The adverse effect suffered by the minority and/or low-income population is more severe or greater in magnitude than the adverse effect suffered by the non-minority and/or low-income populations.

As noted above the disadvantaged populations present occur at a higher rate for the minority population, notably Hispanics. However, a lower African American (9.8%) and higher White population percentage are within the SCE study area compared to the County (17.7% and 75.2% respectively). A similar population below the poverty level also exists between the SCE study area and the County (19% and 18% respectively). No adverse effects regarding noise exist, as the worst-case design year traffic noise levels with the Build Alternative are predicted to be no more than 1.9 dB(A) greater than existing levels and 1.5 dB(A) greater than the design year No-Build noise levels. Additionally, no right-of-way will be required under the Build Alternative and SR 826 is an existing facility. Currently, no disproportionate negative effect is expected to occur as a result of the proposed project.

## 8.0 CONCLUSIONS AND RECOMMENDATIONS

The SCE Evaluation is intended to be a planning tool that incorporates community values, quality of life, and socioeconomic interests into the transportation planning process. As documented in this evaluation, the improvement of SR 826 would result in a range of community effects. Upon completion, the project is expected to increase quality of life through improved access and reduced congestion.

Project effects related to six (6) key socioeconomic areas are documented in **Table 8-1**. In addition to the listing of effects, a characterization of effect for each SCE area is included to convey the overall character of expected impacts.

**Table 8 - 1: Summary of Sociocultural Effects**

SCE Issue	Effect	Characterization of Impact
Social	<ul style="list-style-type: none"><li>• Temporary construction impacts</li><li>• High minority low income population</li><li>• No division or isolation of existing populations</li><li>• Community focal points not affected</li><li>• No right-of-way acquisition</li></ul>	Minimal
Economic	<ul style="list-style-type: none"><li>• No expected reduction in taxable land or values</li><li>• Improves freight movement</li><li>• Improves access in specially designated “Opportunity Zone”</li><li>• Expected commute times to local businesses shortened</li></ul>	Enhanced
Land Use	<ul style="list-style-type: none"><li>• Project occurs in existing urban environment</li><li>• Existing land use will remain the same</li><li>• No direct negative impacts to community focal points</li></ul>	Minimal
Mobility	<ul style="list-style-type: none"><li>• Temporary disruptions to traffic</li><li>• Reduced traffic congestion</li><li>• Adds consistent sidewalk facilities along frontage roads</li><li>• Improves safety</li></ul>	Enhanced
Aesthetics	<ul style="list-style-type: none"><li>• 416 impacted residences</li><li>• 5 impacted special land use sites</li><li>• Limited impacts to views vistas</li><li>• Temporary construction noise</li><li>• Existing facility</li></ul>	Moderate
Relocations	<ul style="list-style-type: none"><li>• No right-of-way acquisition</li></ul>	Minimal

### 8.2 Conclusions

The purpose of this project is to address various roadway deficiencies that cause large speed differentials between the GP and ELs and improve access to the EL system by relocating the ingress point to NW 103<sup>rd</sup> Street. The need for this project is based on congestion, safety, and system linkage. Both the West Frontage Road/NW 77<sup>th</sup> Avenue and East Frontage Road/W 20<sup>th</sup> Avenue will be enhanced through milling and resurfacing, drainage, and pedestrian improvements.

### **8.3 Recommendation for Resolving Issues**

The project is expected to have positive effects on local economic activity and mobility with limited negative sociocultural effects. Moderate impact is expected on Noise and Vibration as there are no apparent solutions available to mitigate the noise impacts at any of 416 impacted residences and five (5) impacted special land use sites. Due to the limited scope of the project's improvements, limited available right-of-way, and poor abatement performance, none of the noise barriers evaluated for the nine (9) impacted Common Noise Environments (CNE) are recommended for construction at this time. It should be noted that worst-case design year traffic noise levels with the Build Alternative are predicted to be no more than 1.9 dB(A) greater than existing traffic noise levels. Specific noise abatement measures will be reevaluated during final design.

Activities will result in temporary air, noise, traffic flow, and visual impacts for those residents, businesses, and travelers within the immediate vicinity of the project. The impacts will be effectively controlled in accordance with FDOT's Standard Specifications for Road and Bridge Construction.

Continued public engagement efforts are suggested to identify the level of concern present within the community related to Noise and Vibration. The traffic noise impacts to these noise sensitive sites are considered to be an unavoidable consequence of the project. Impacts will be managed and addressed as appropriate before, during, and after the project in accordance with FDOT and other agency policies, programs, and procedures.

### **8.4 Project Commitments**

There are currently no commitments related to SCE topics.

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## **Appendix A**

Demographics Maps

Figure A-1: Percent Age 65 and Older Key Sheet

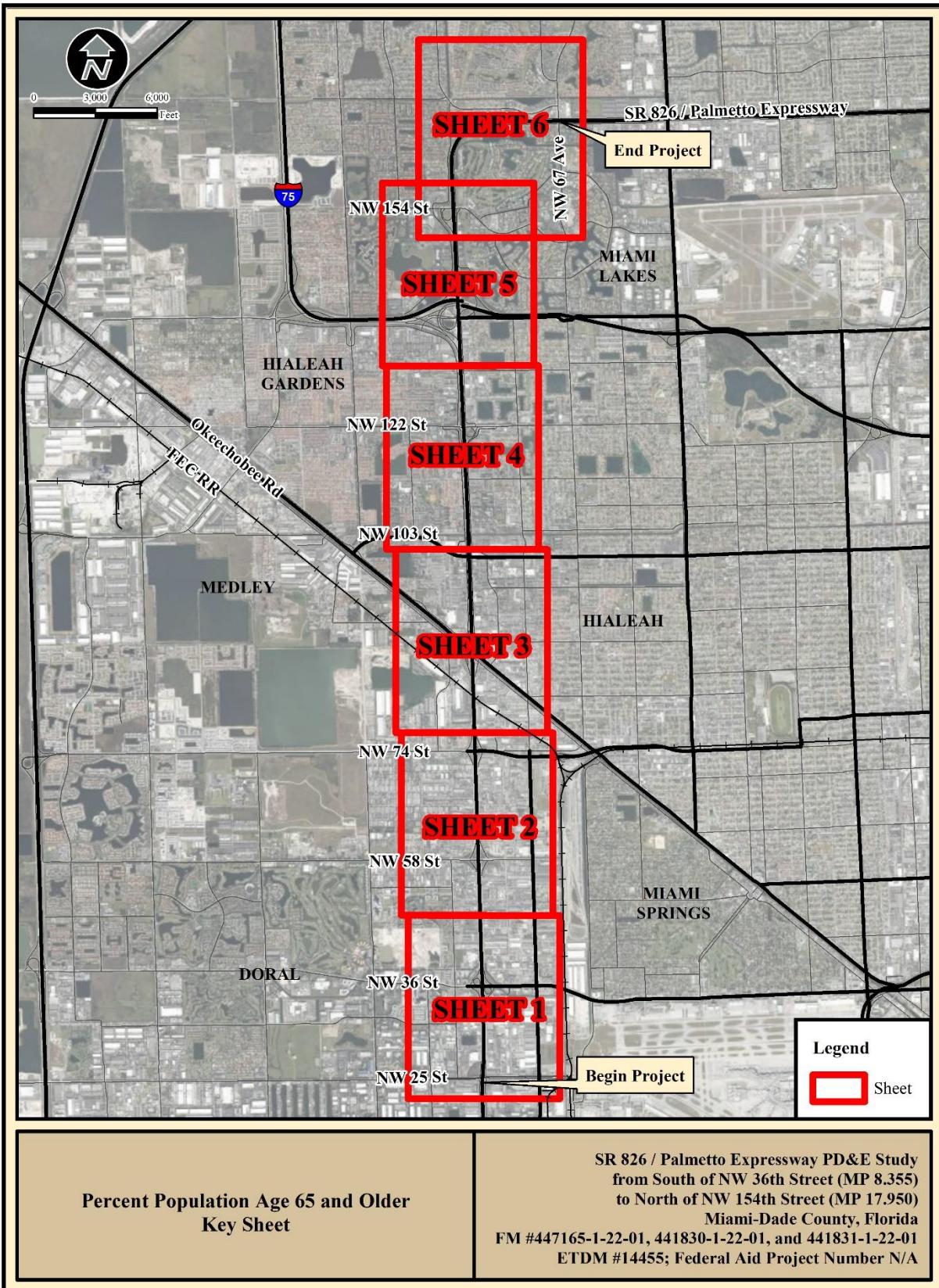
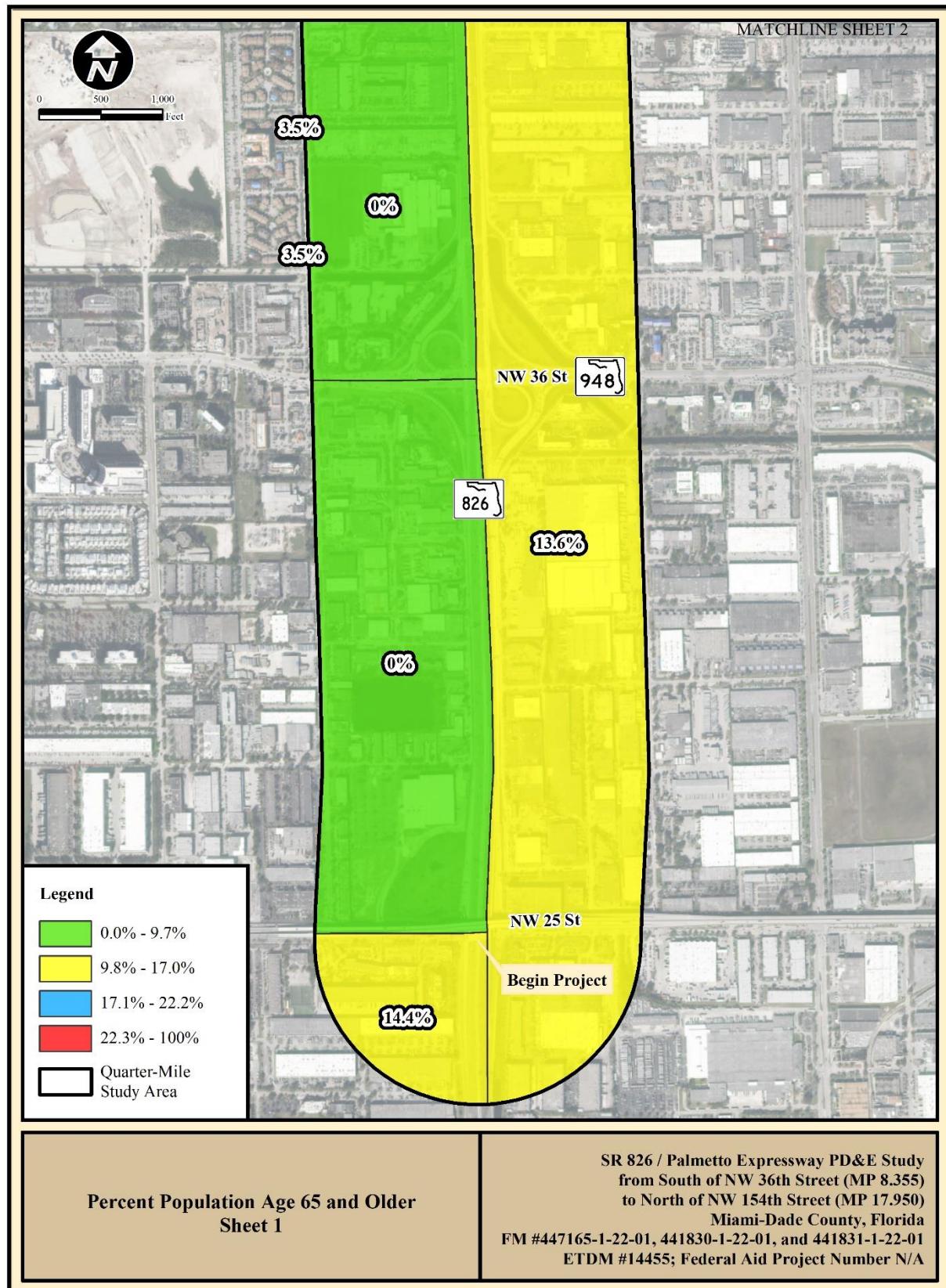


Figure A-2: Percent Age 65 and Older Map 1



**Figure A-2: Percent Age 65 and Older Map 2**

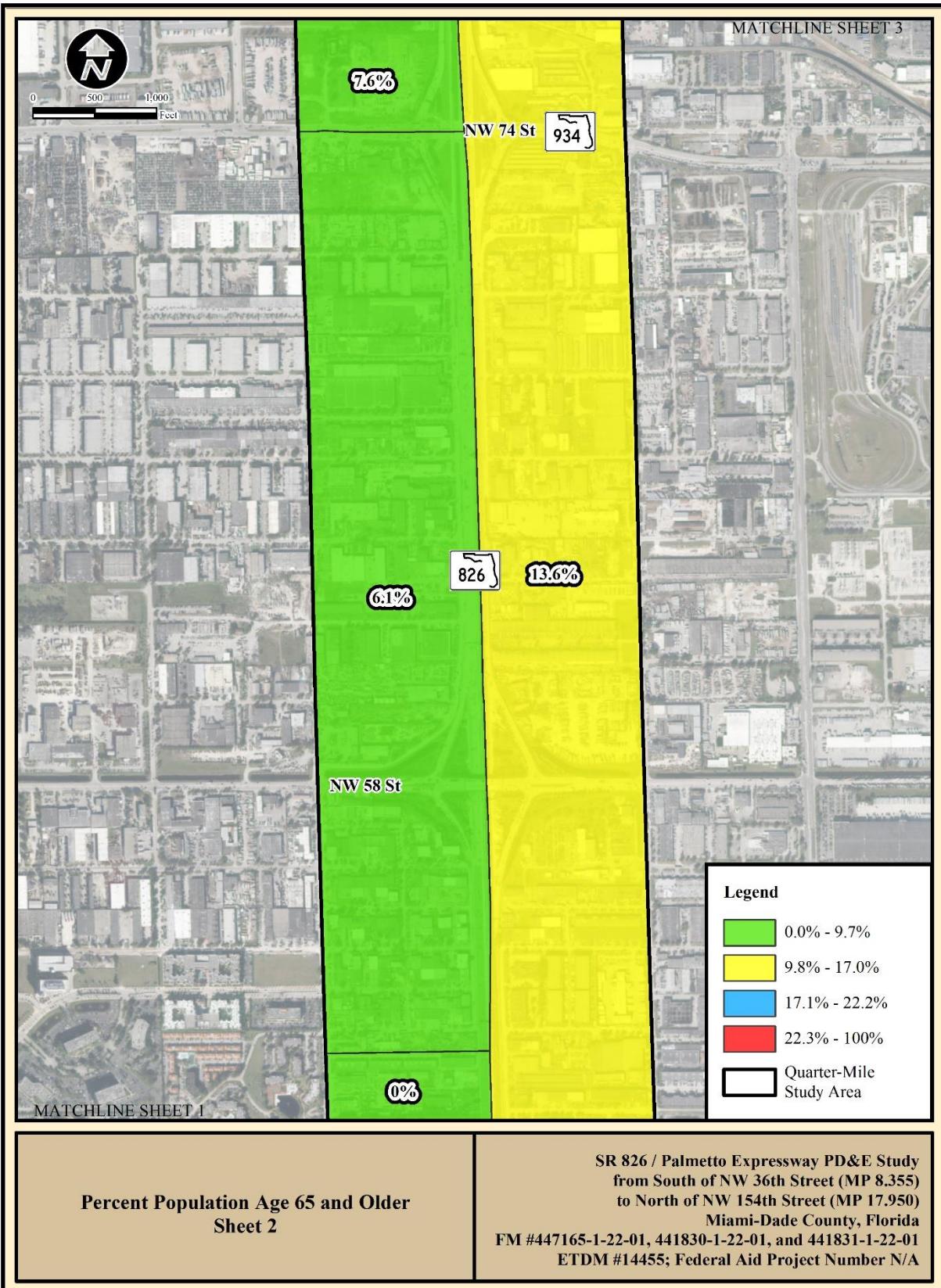


Figure A-2: Percent Age 65 and Older Map 3

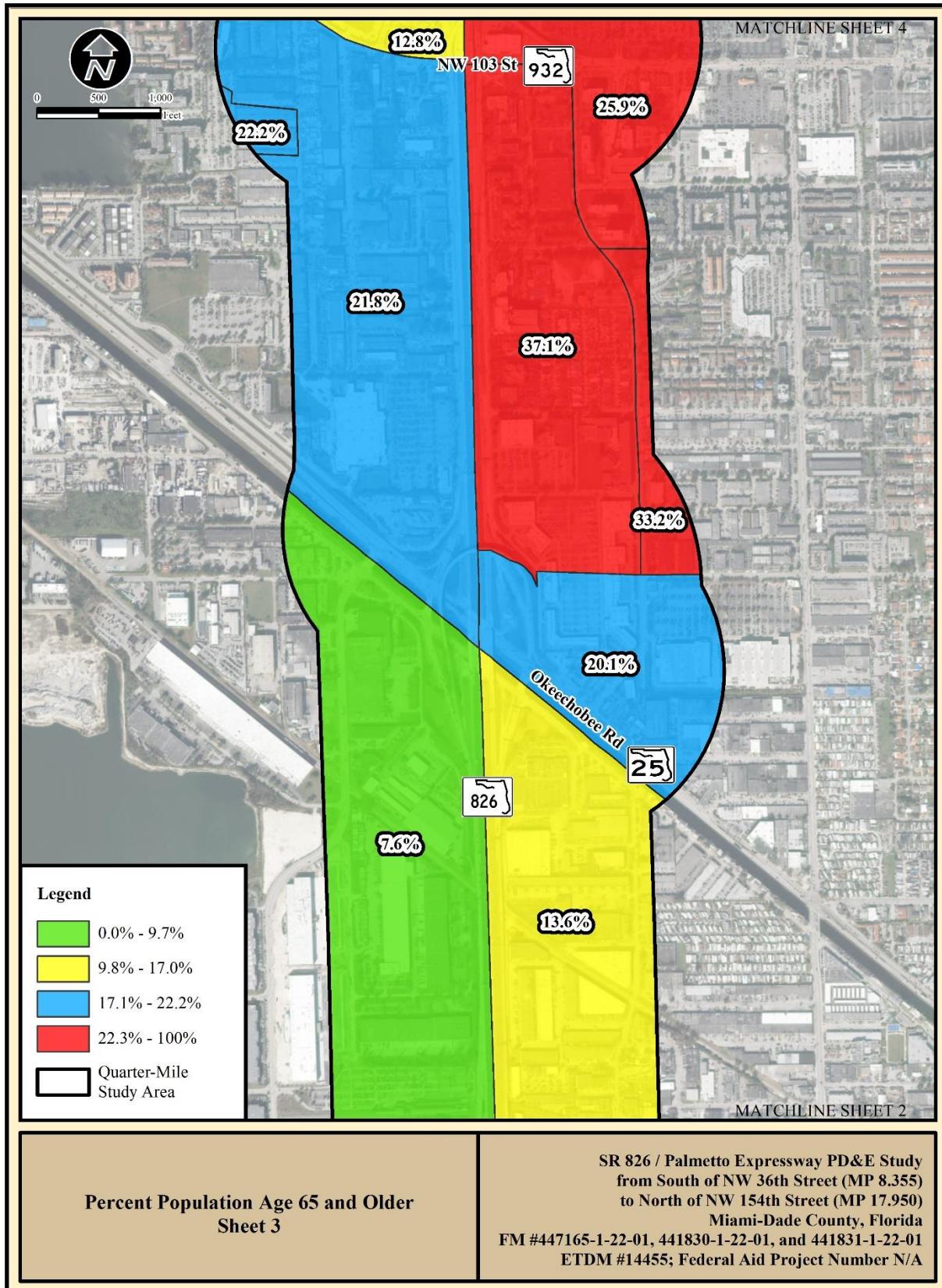


Figure A-2: Percent Age 65 and Older Map 4

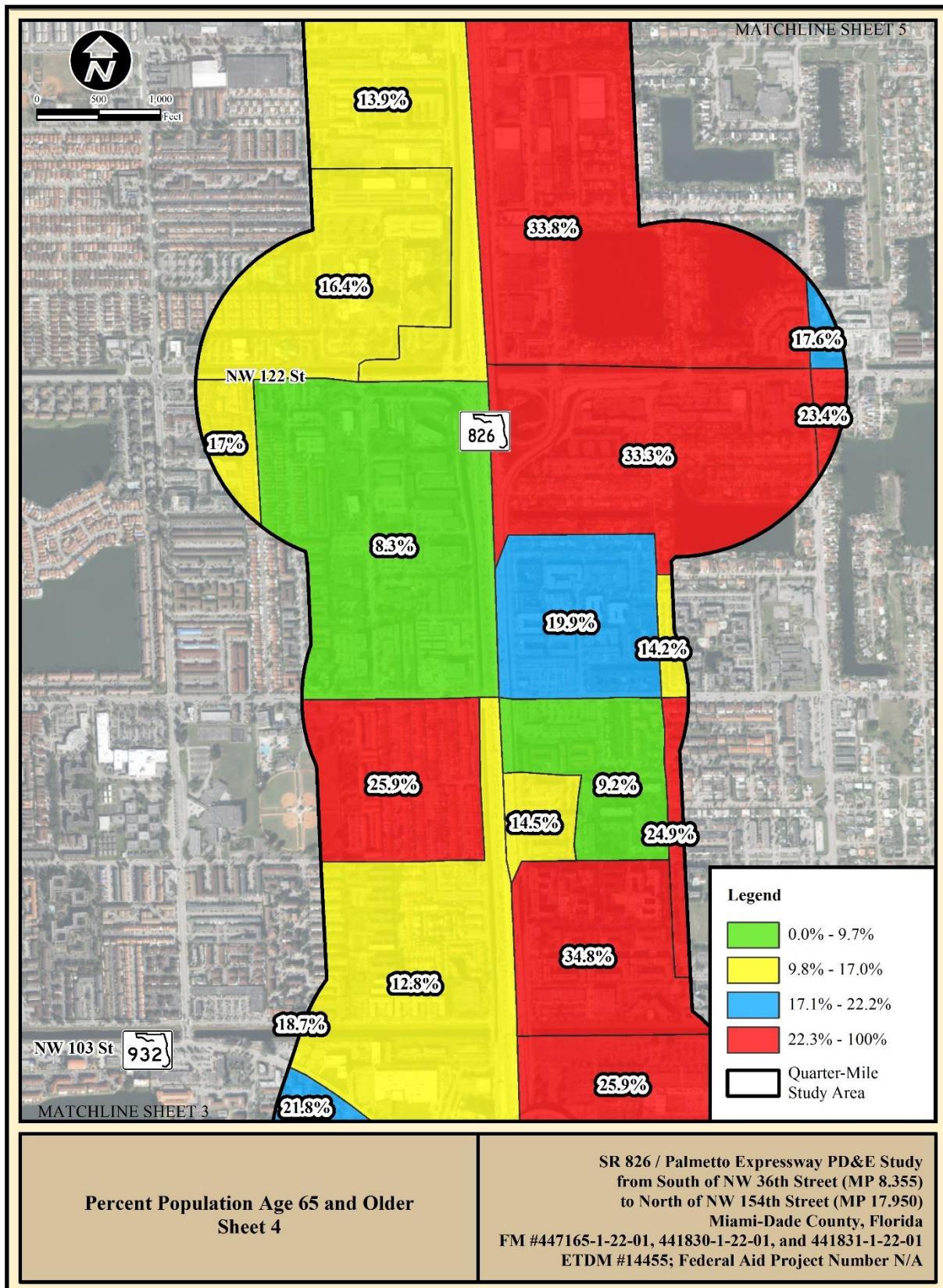
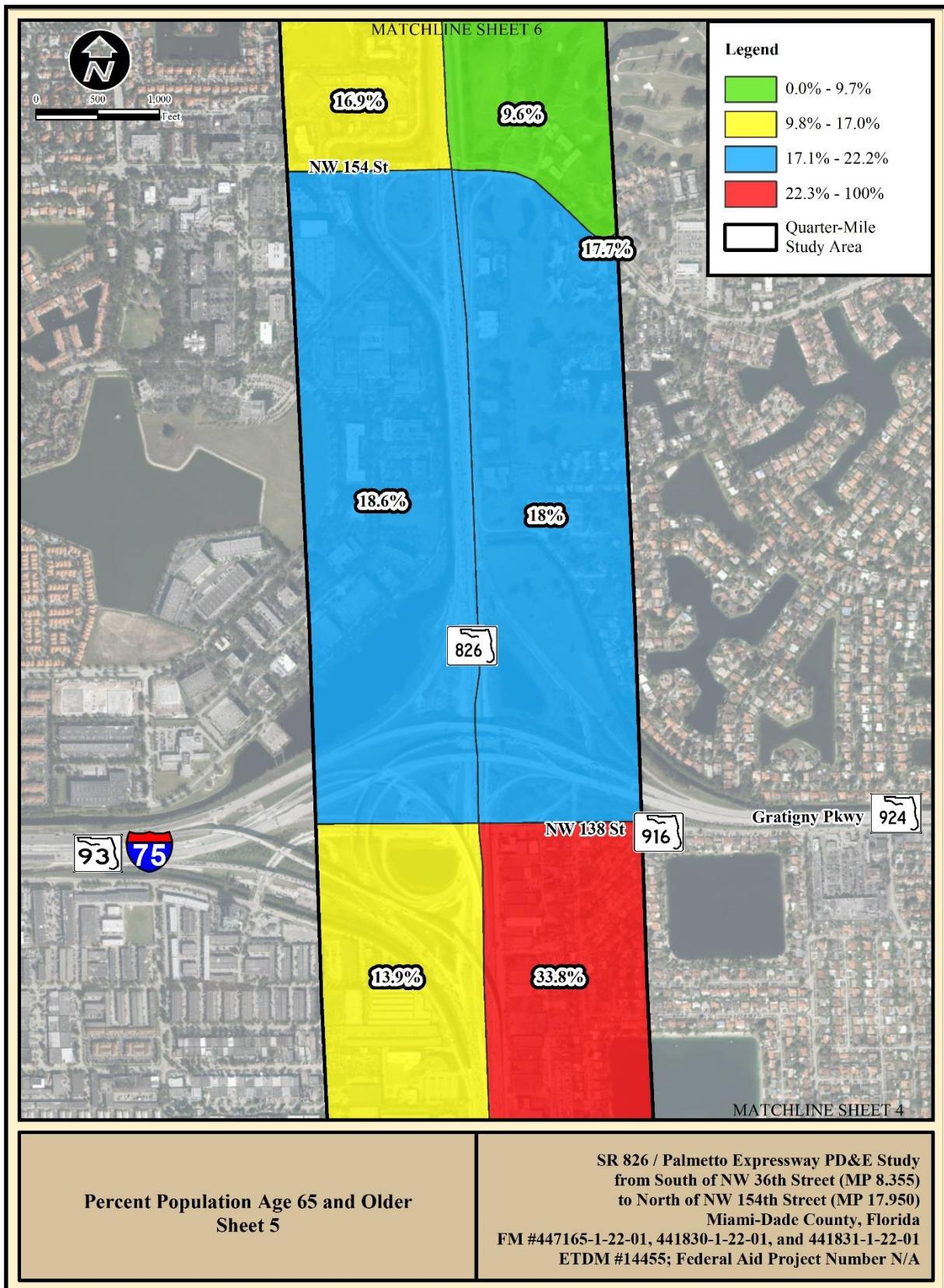


Figure A-2: Percent Age 65 and Older Map 5



**Figure A-2: Percent Age 65 and Older Map 6**

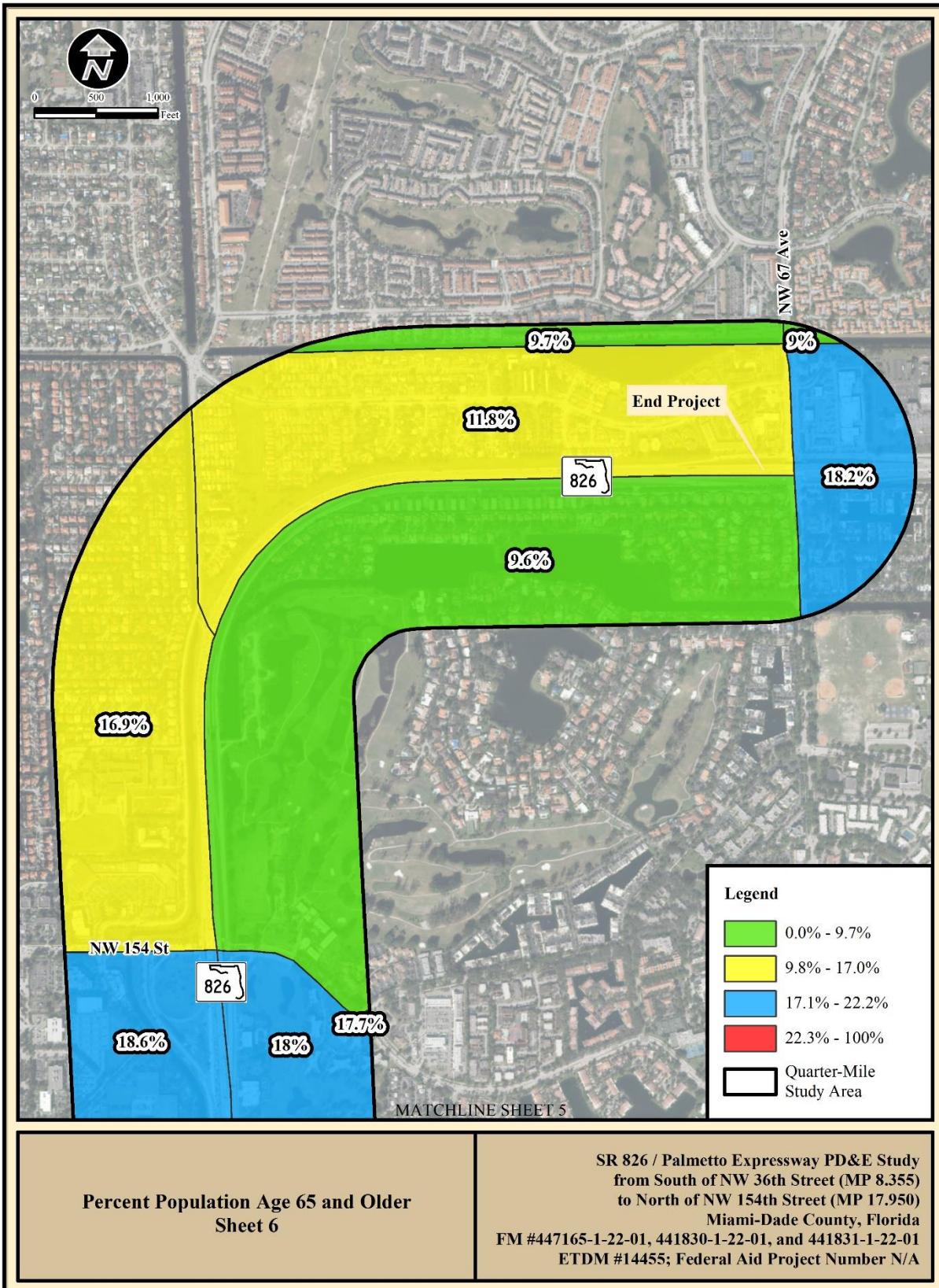


Figure A-3: Percent Below Poverty Key Sheet

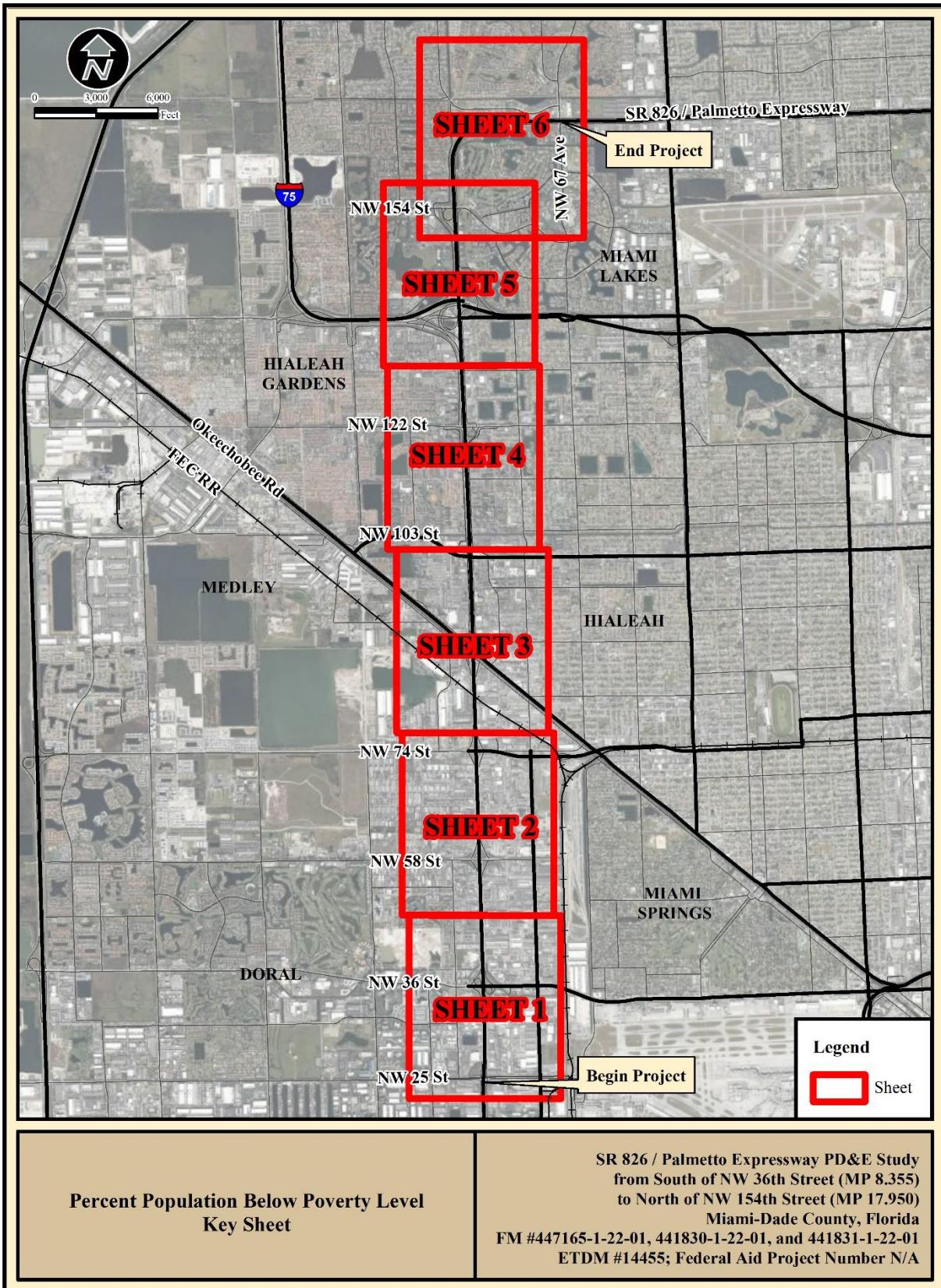


Figure A-4: Percent Below Poverty Map 1

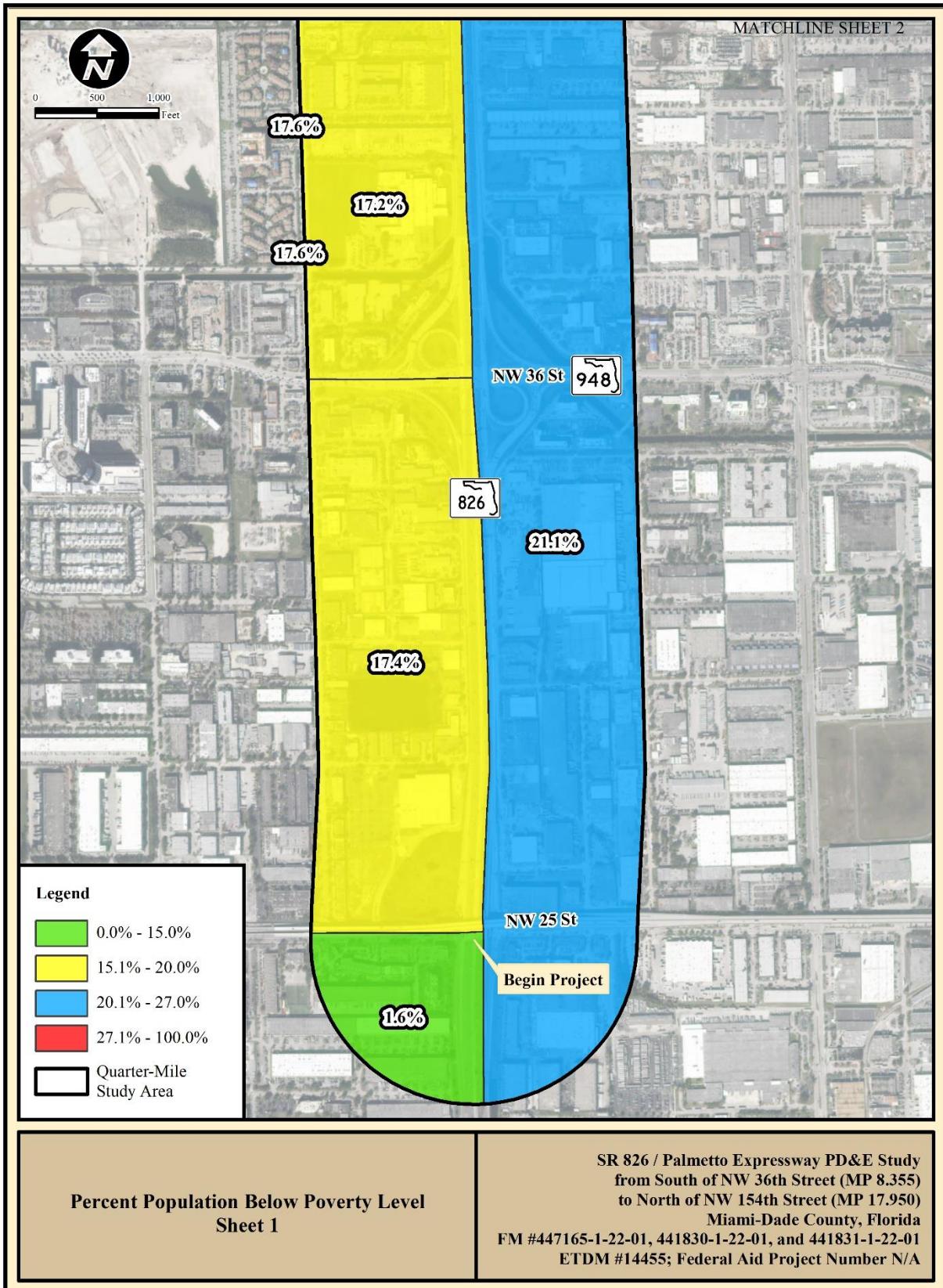


Figure A-4: Percent Below Poverty Map 2

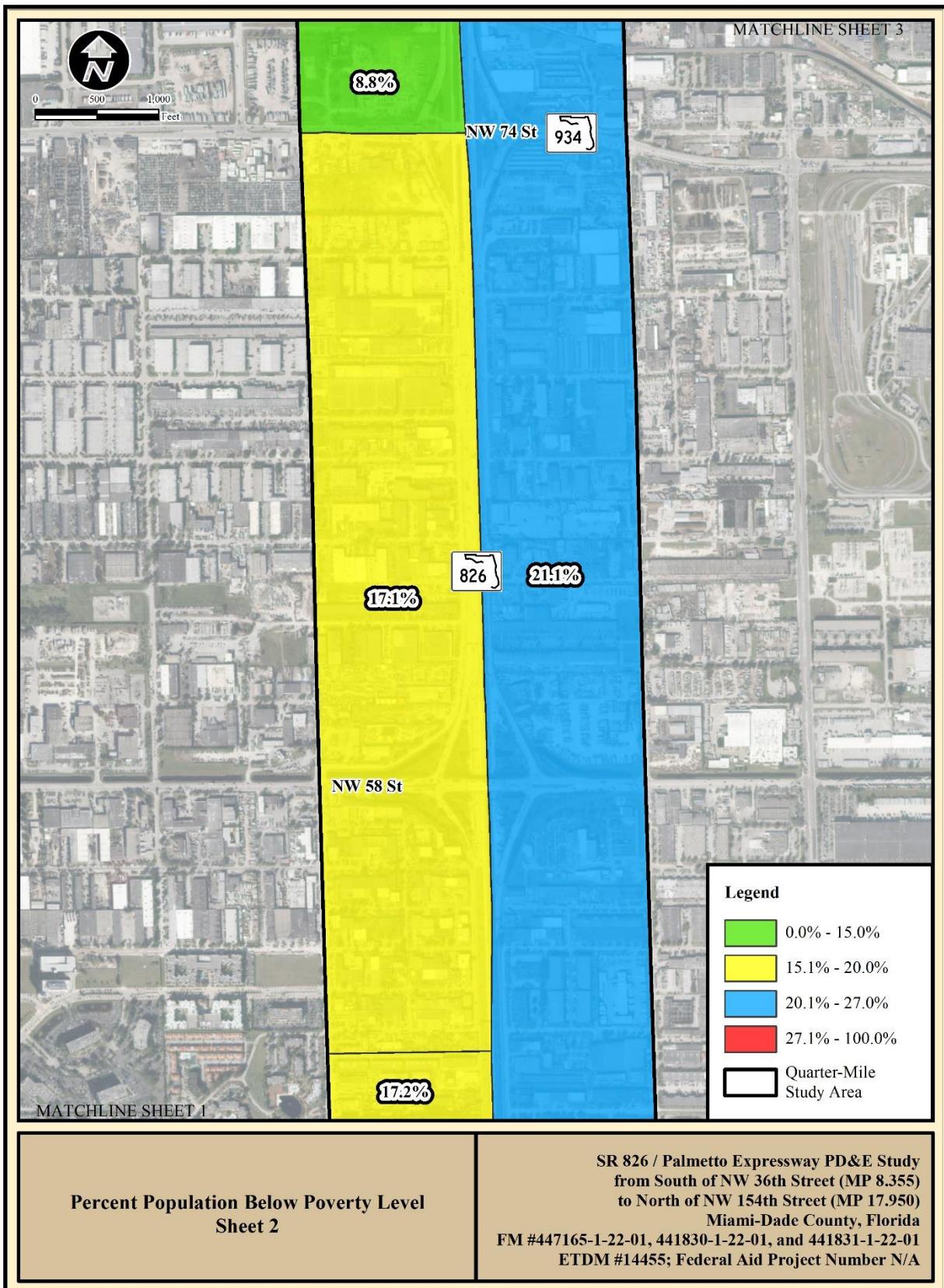


Figure A-4: Percent Below Poverty Map 3

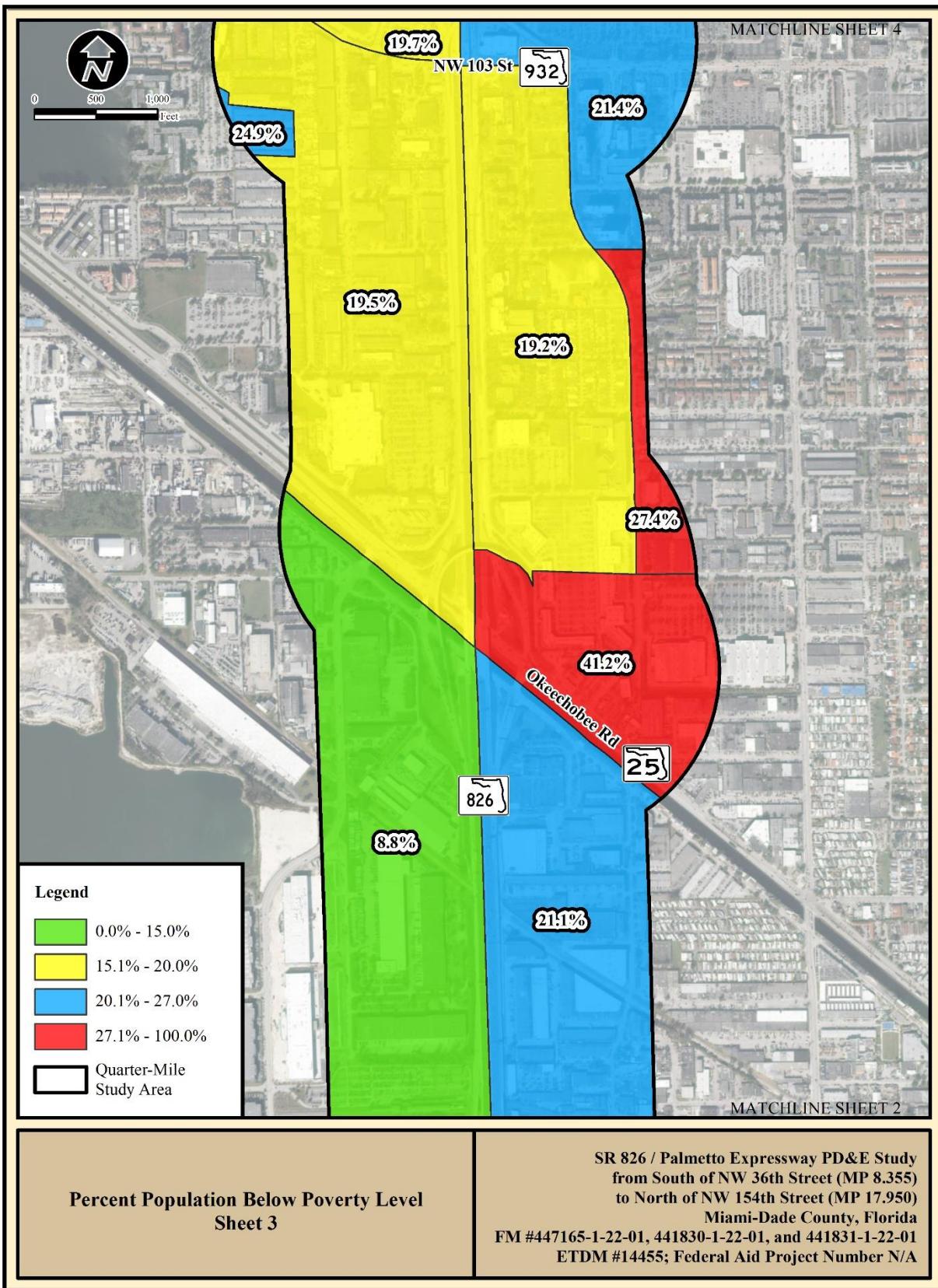


Figure A-4: Percent Below Poverty Map 4

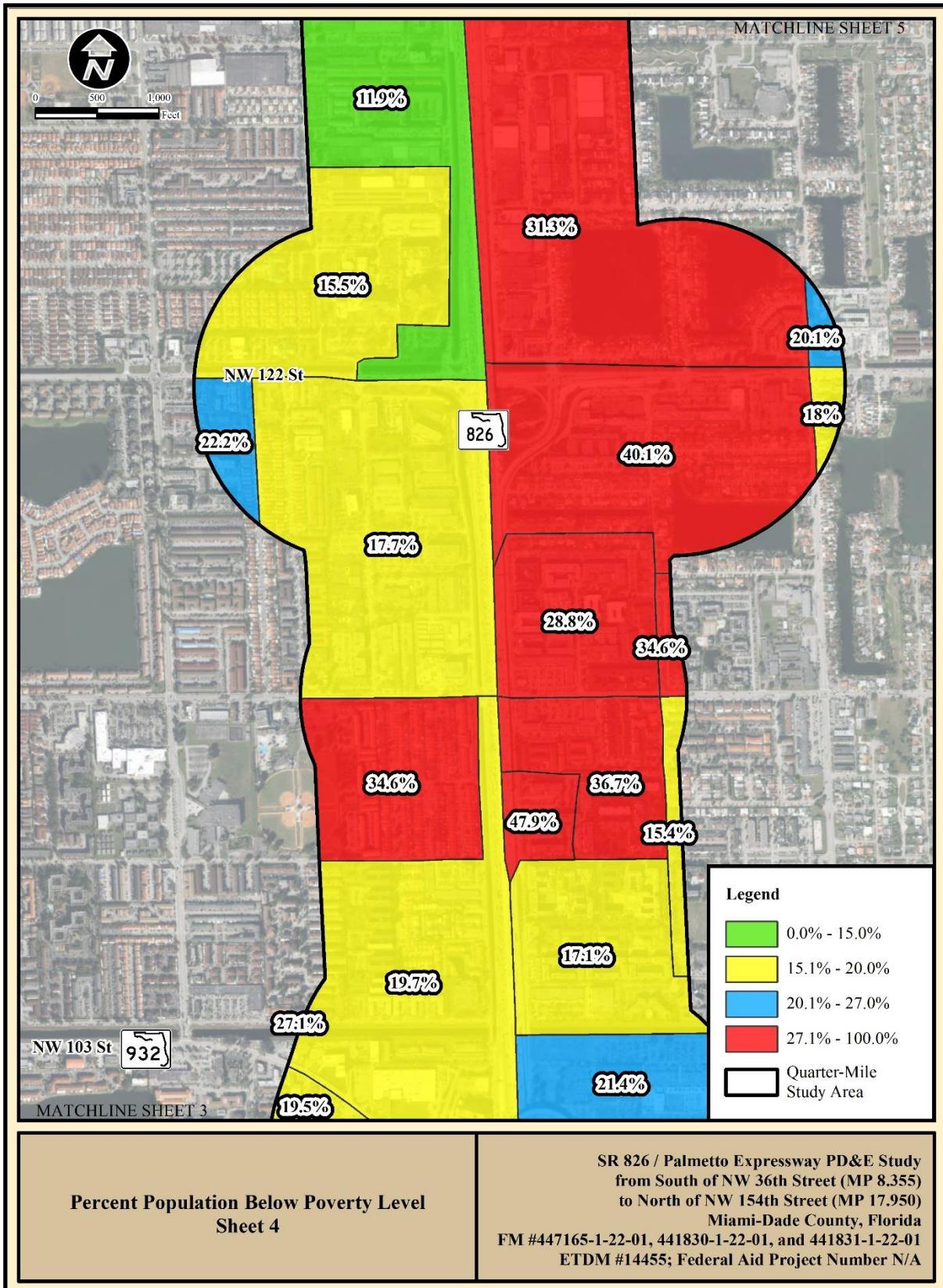


Figure A-4: Percent Below Poverty Map 5

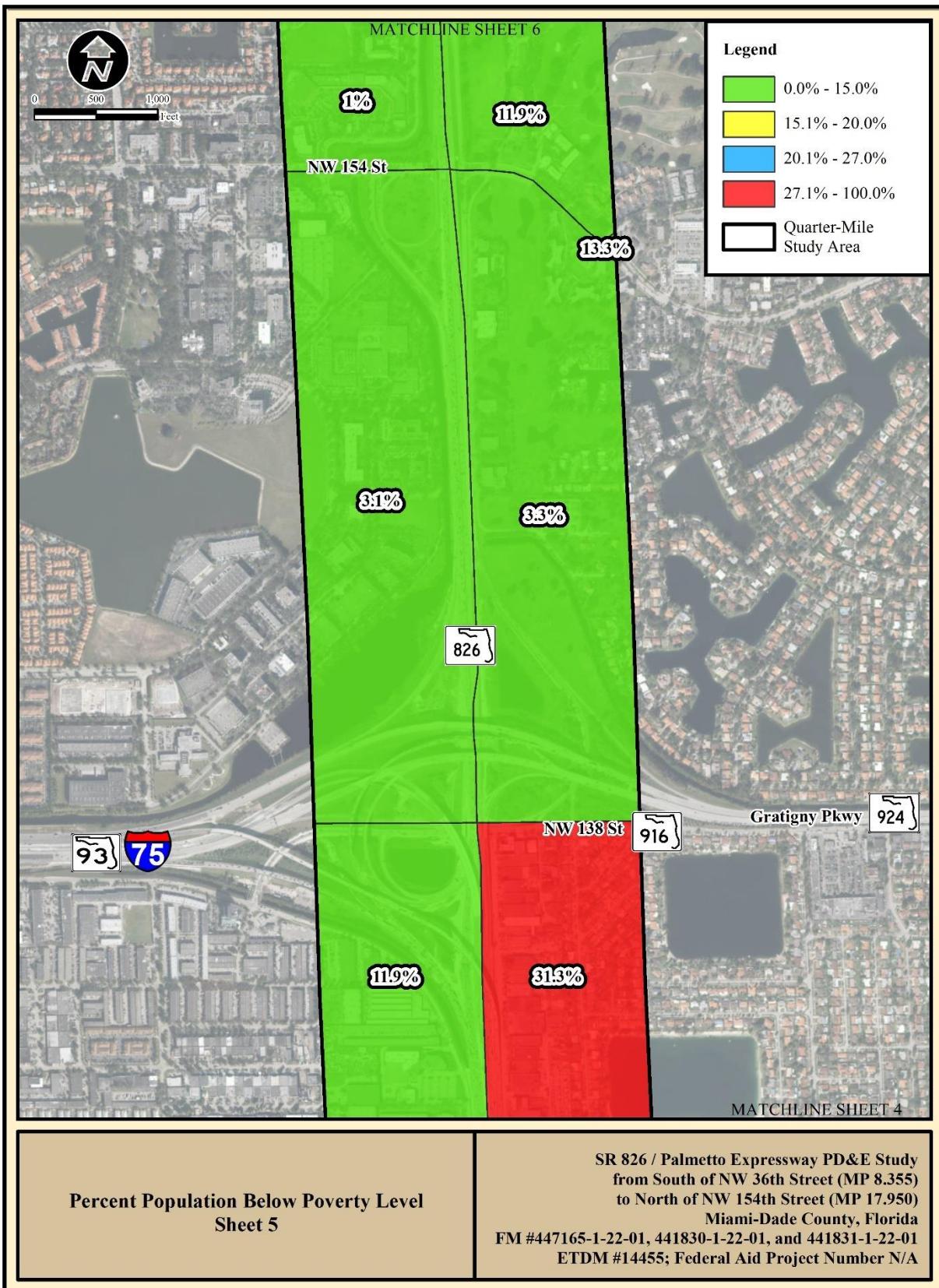


Figure A-4: Percent Below Poverty Map 6

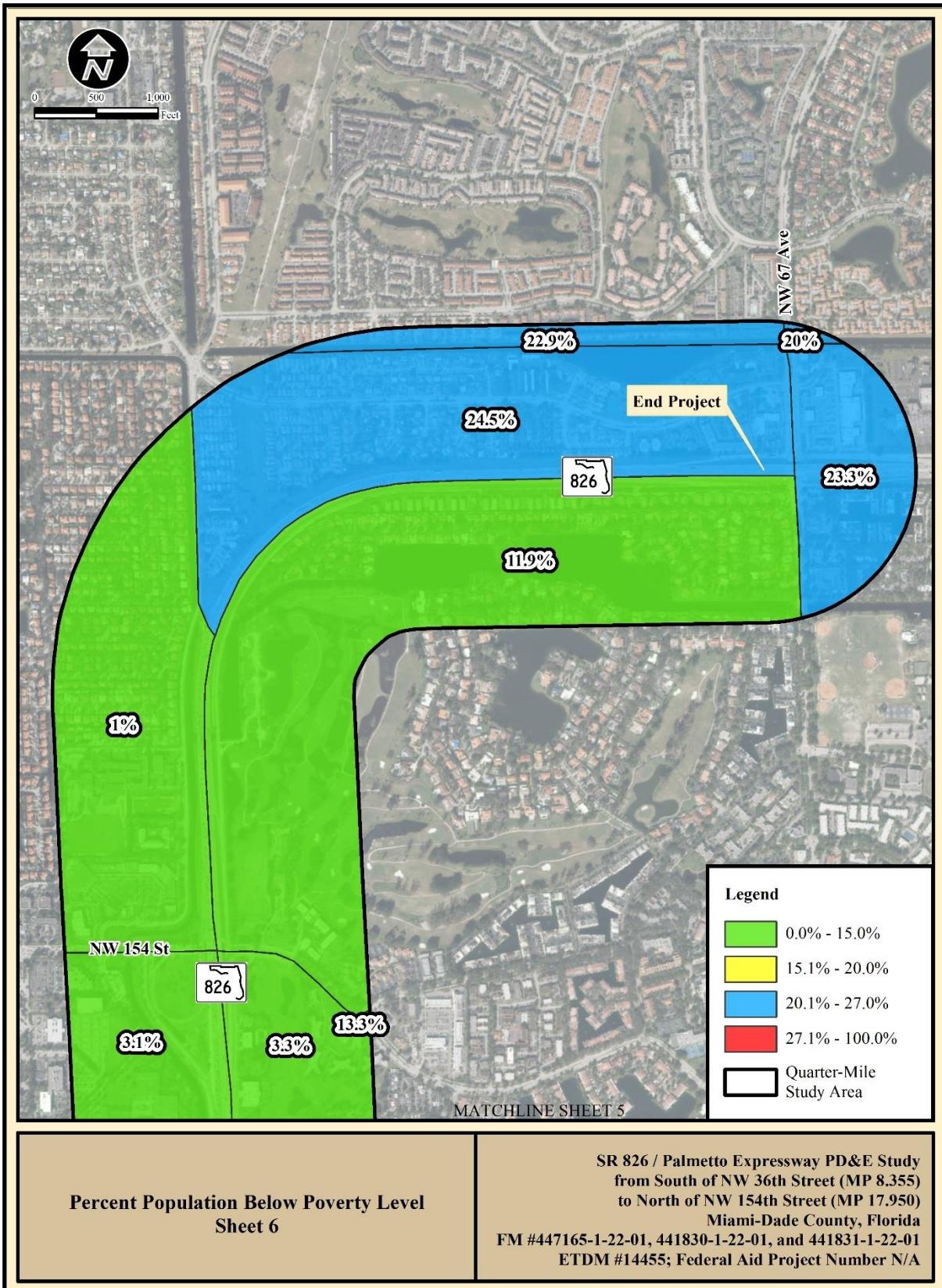


Figure A-5: Percent Hispanic Key Sheet

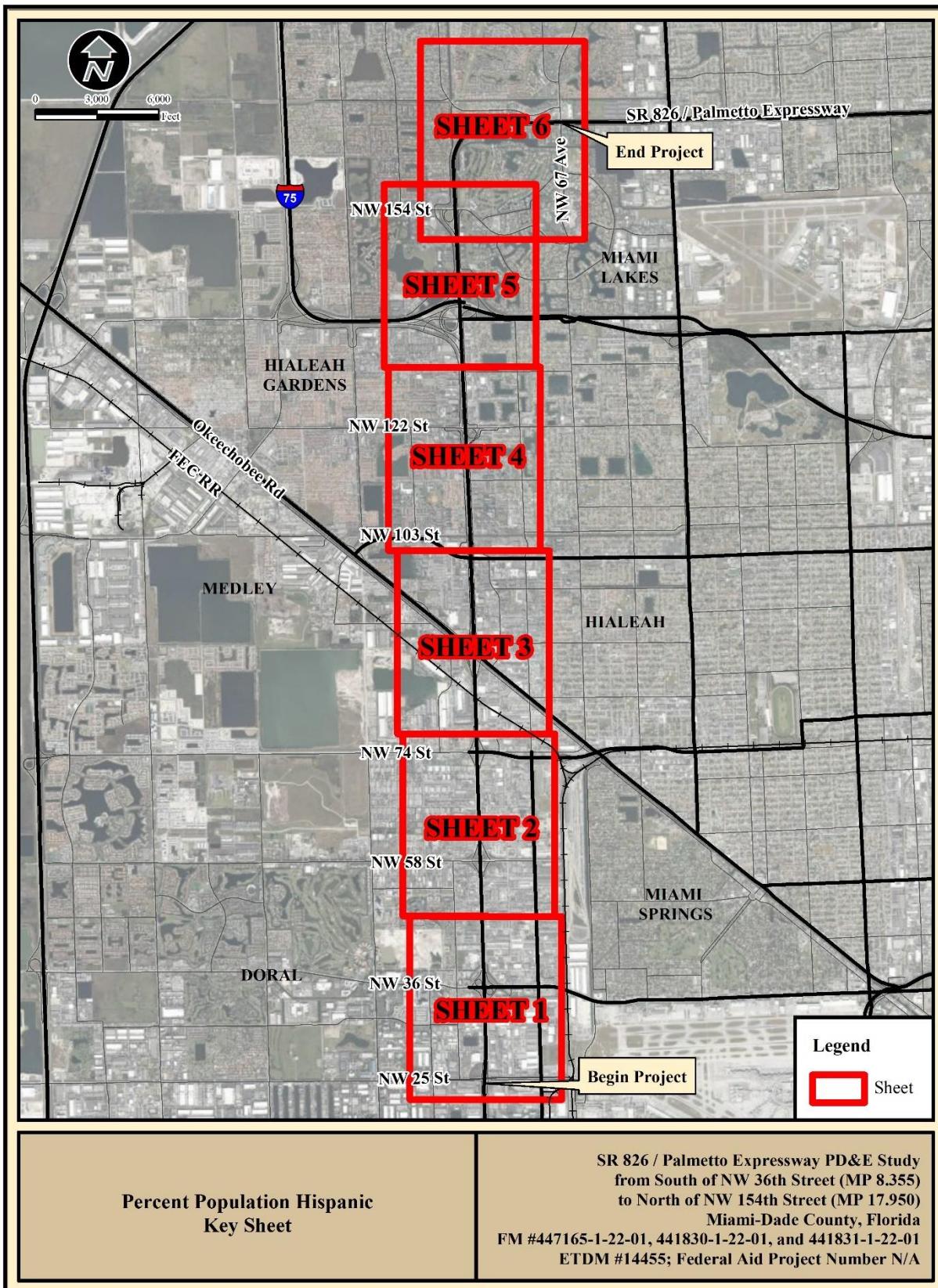
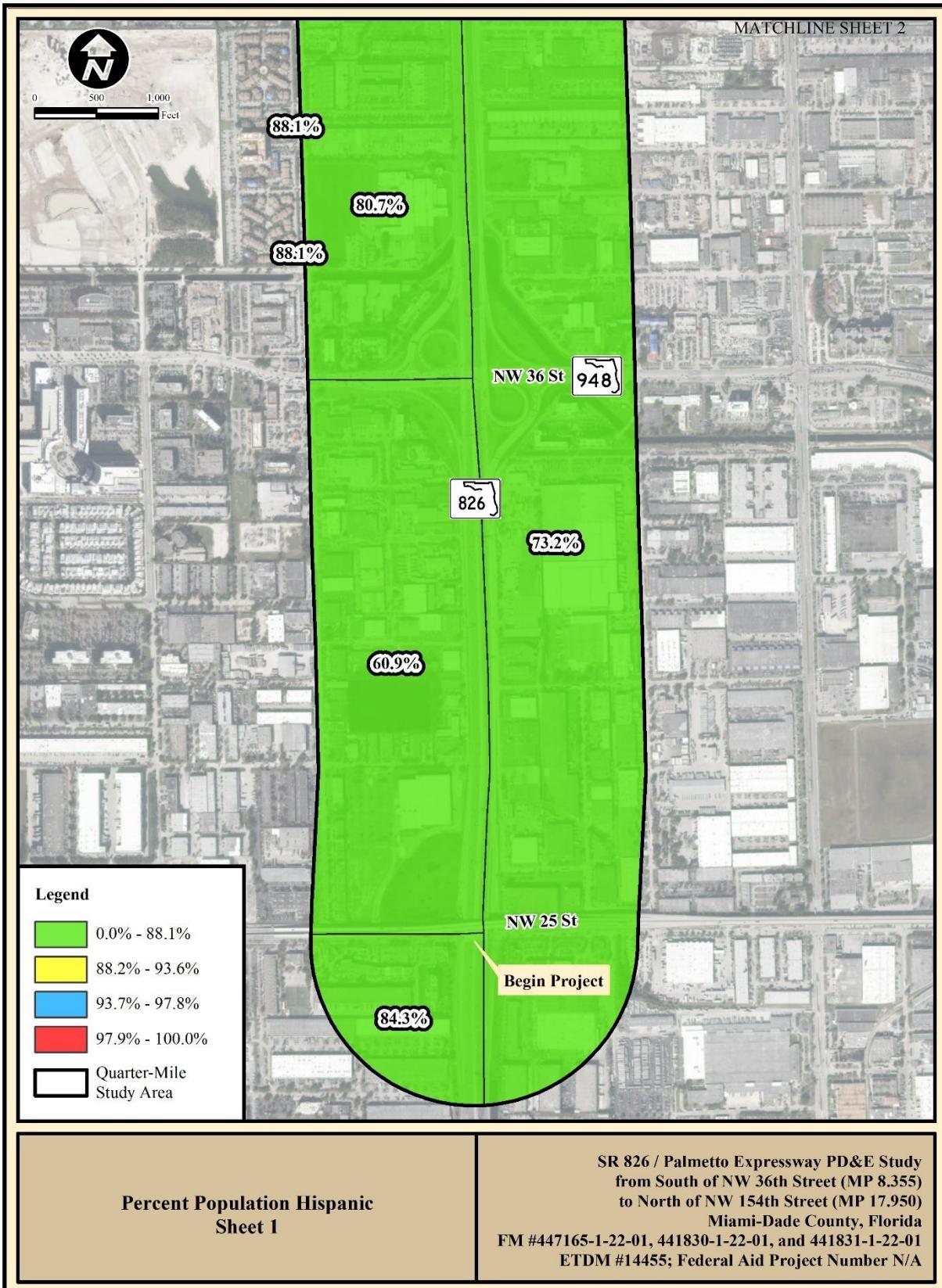


Figure A-6: Percent Hispanic Map 1



**Figure A-6: Percent Hispanic Map 2**

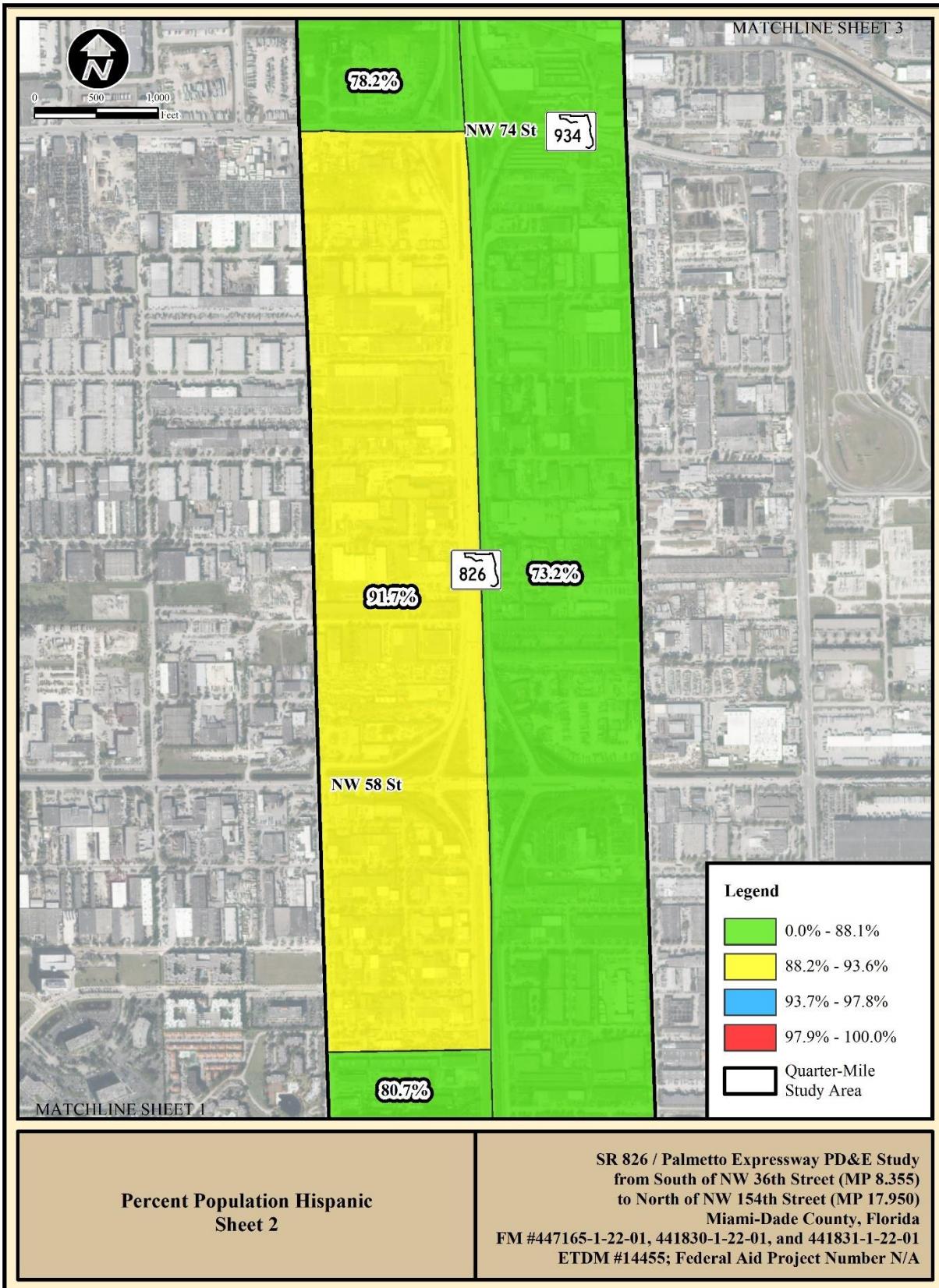


Figure A-6: Percent Hispanic Map 3

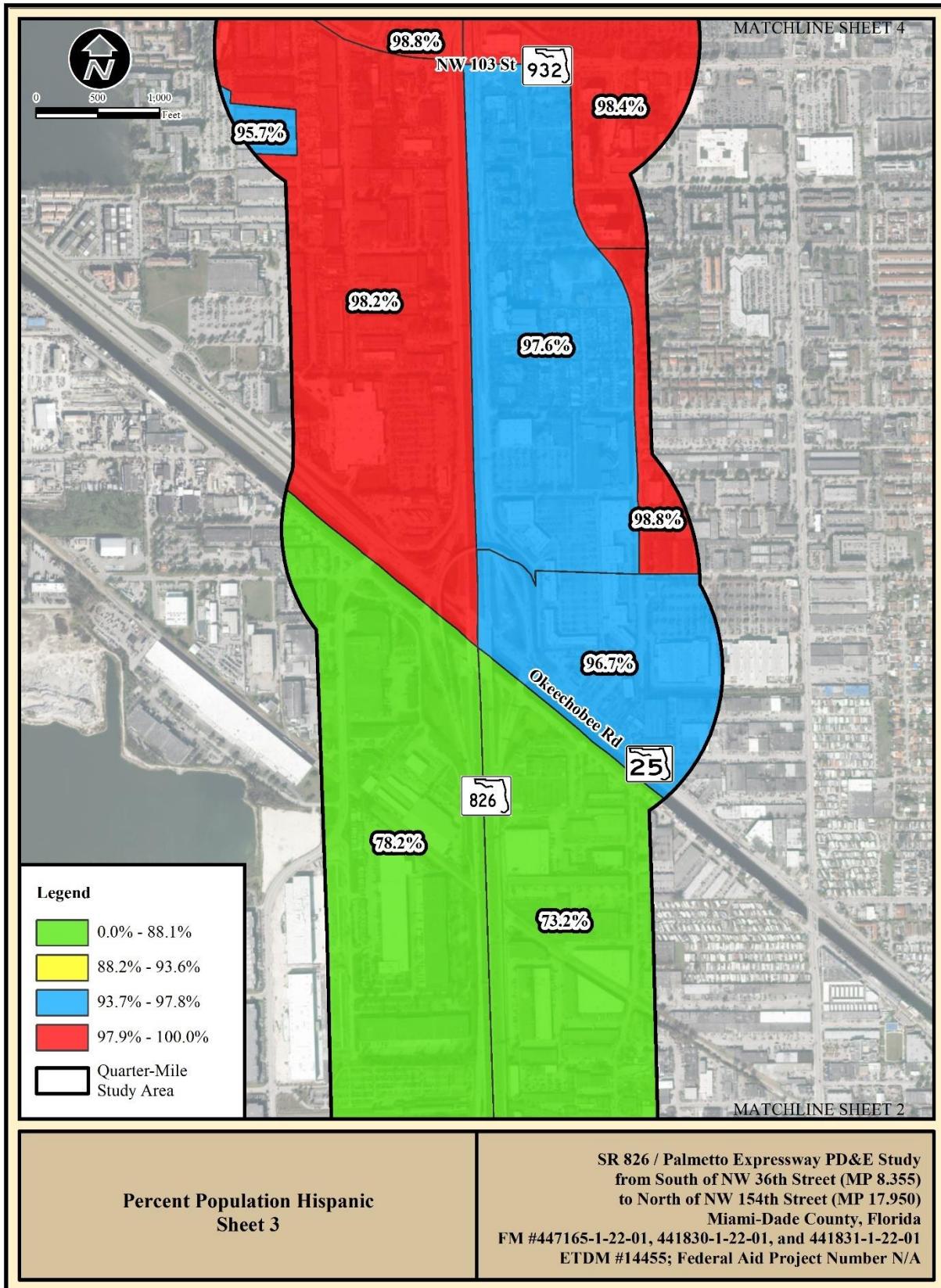
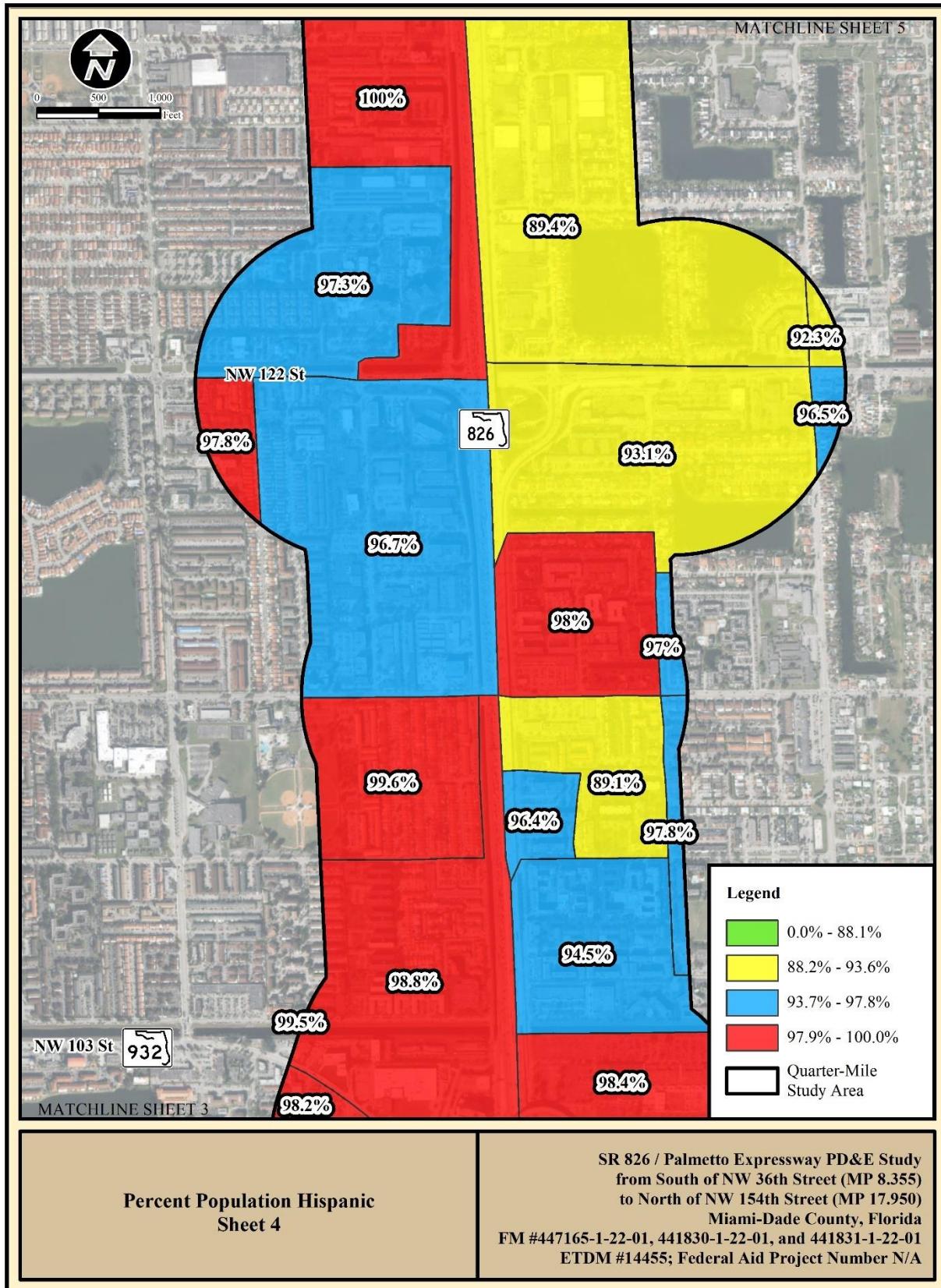


Figure A-6: Percent Hispanic Map 4



**Figure A-6: Percent Hispanic Map 5**

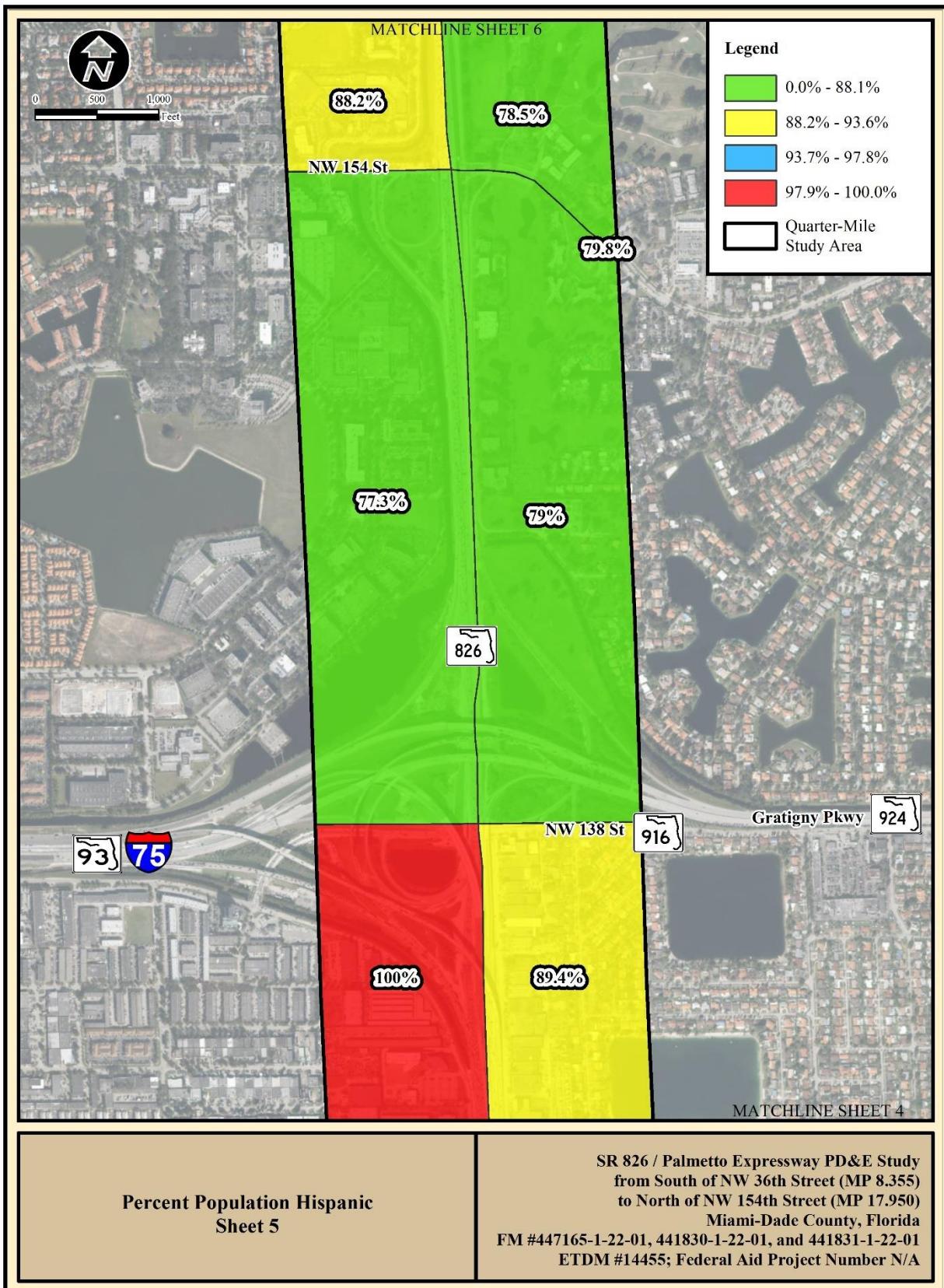


Figure A-6: Percent Hispanic Map 6

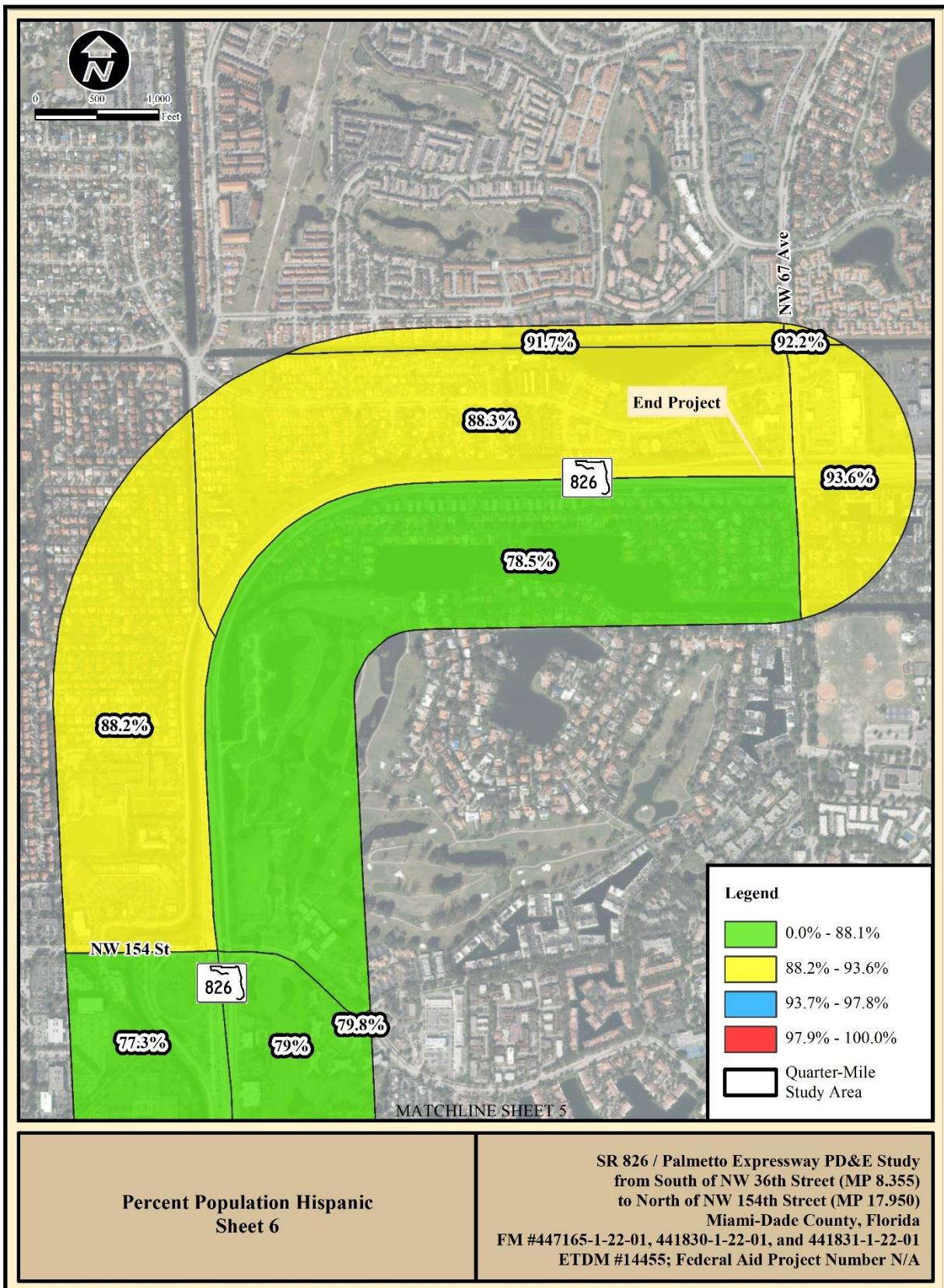
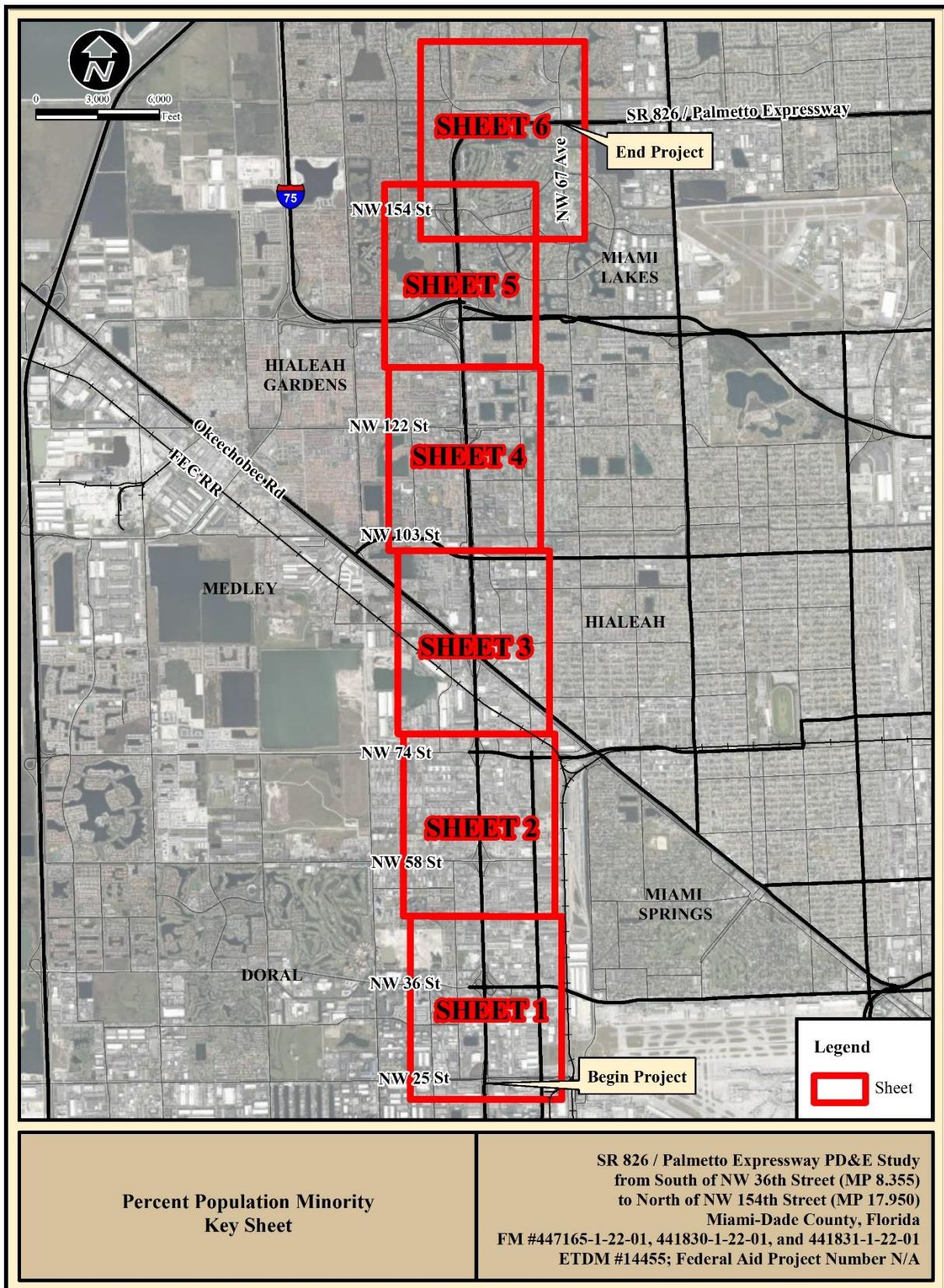
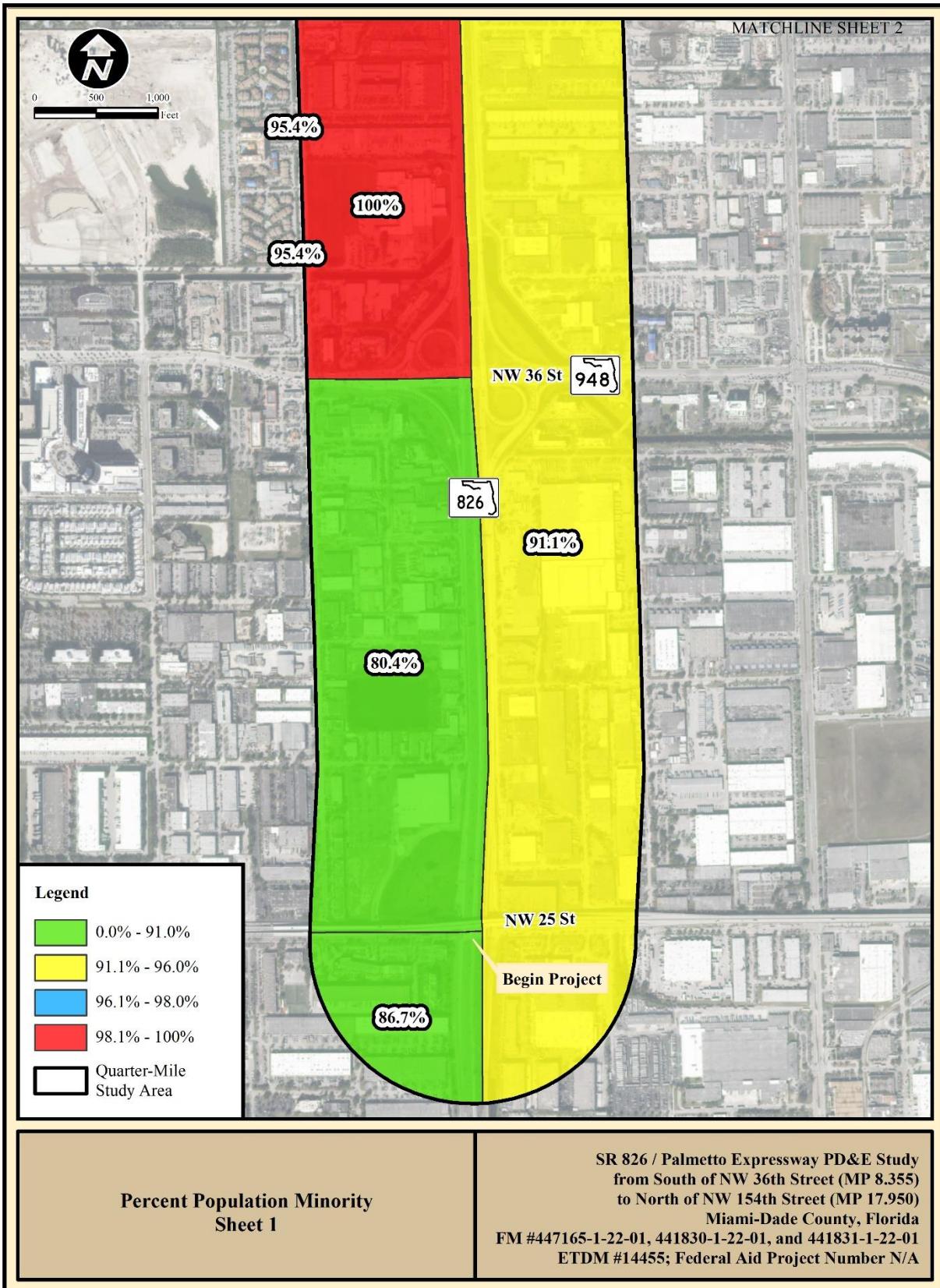


Figure A-7: Percent Minority Key Sheet



**Figure A-8: Percent Minority Map 1**



**Figure A-8: Percent Minority Map 2**

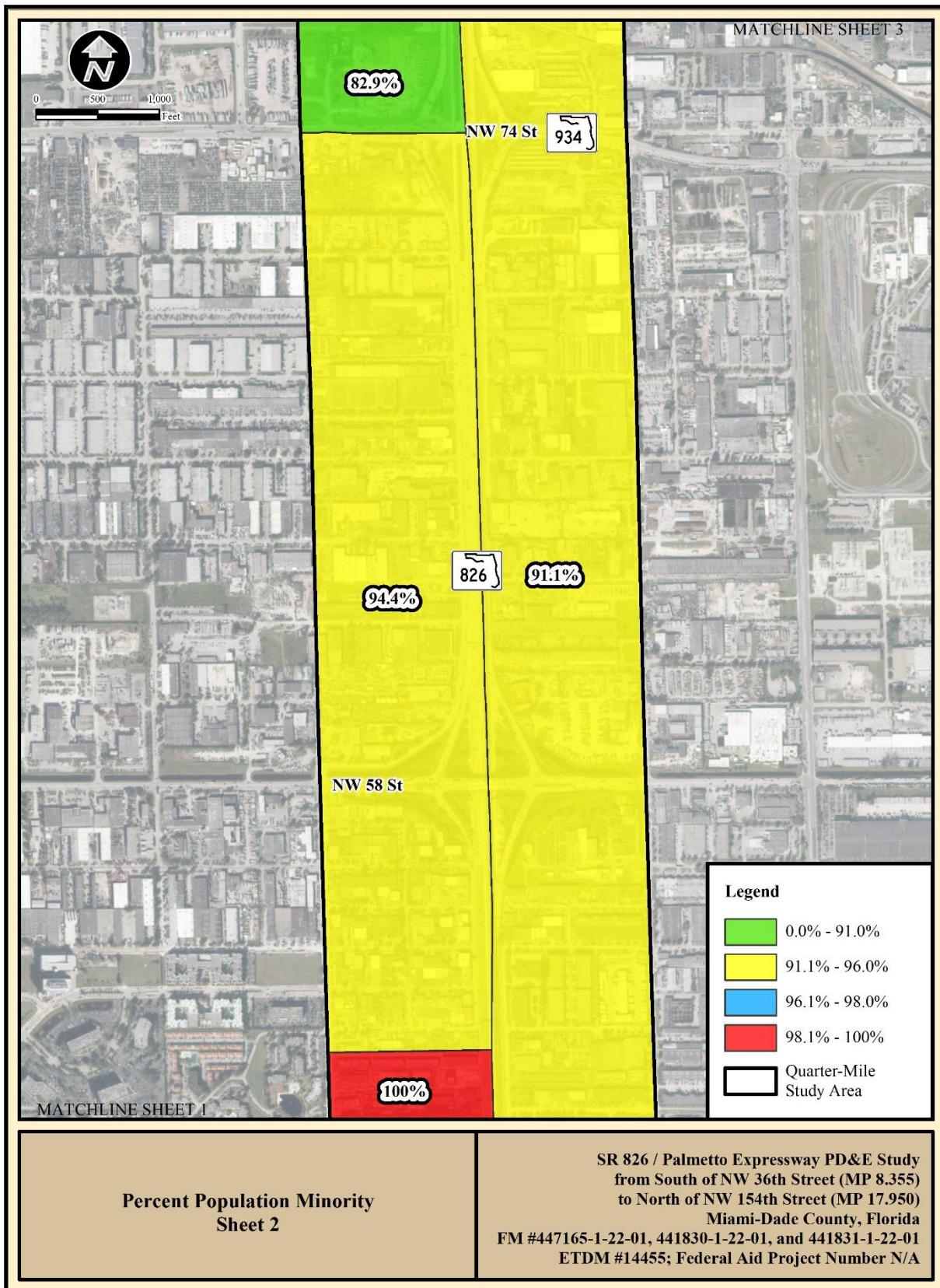
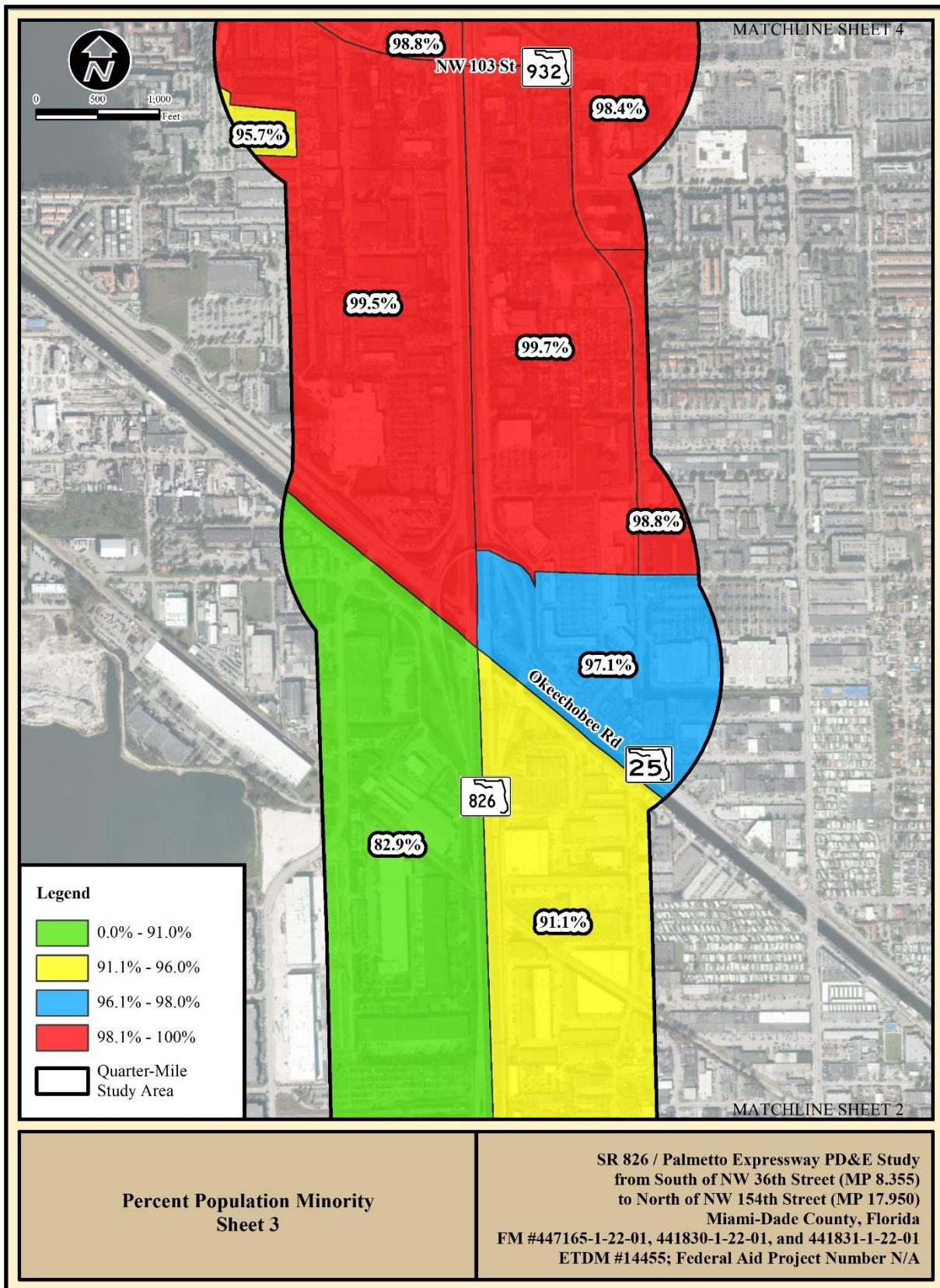
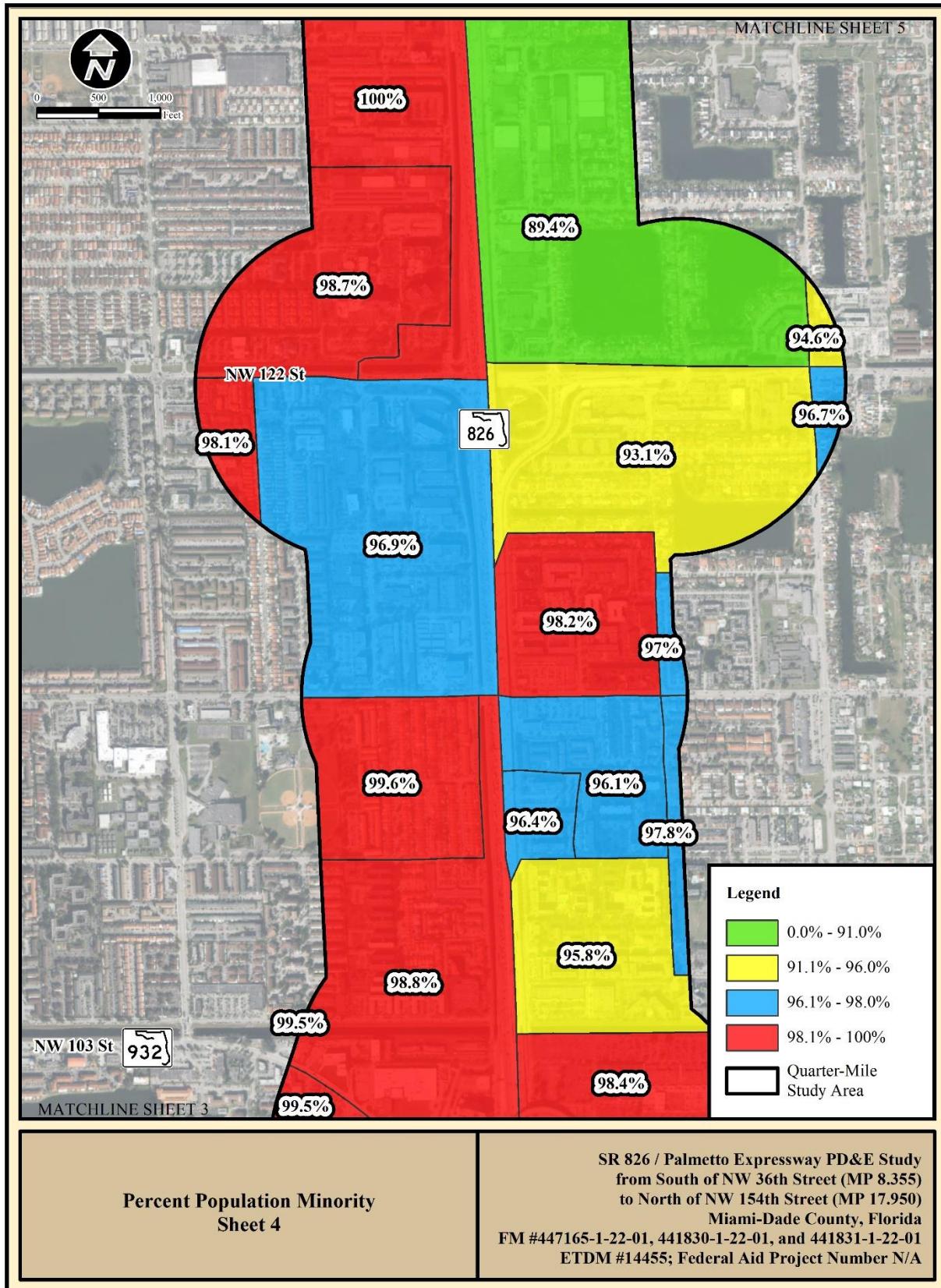


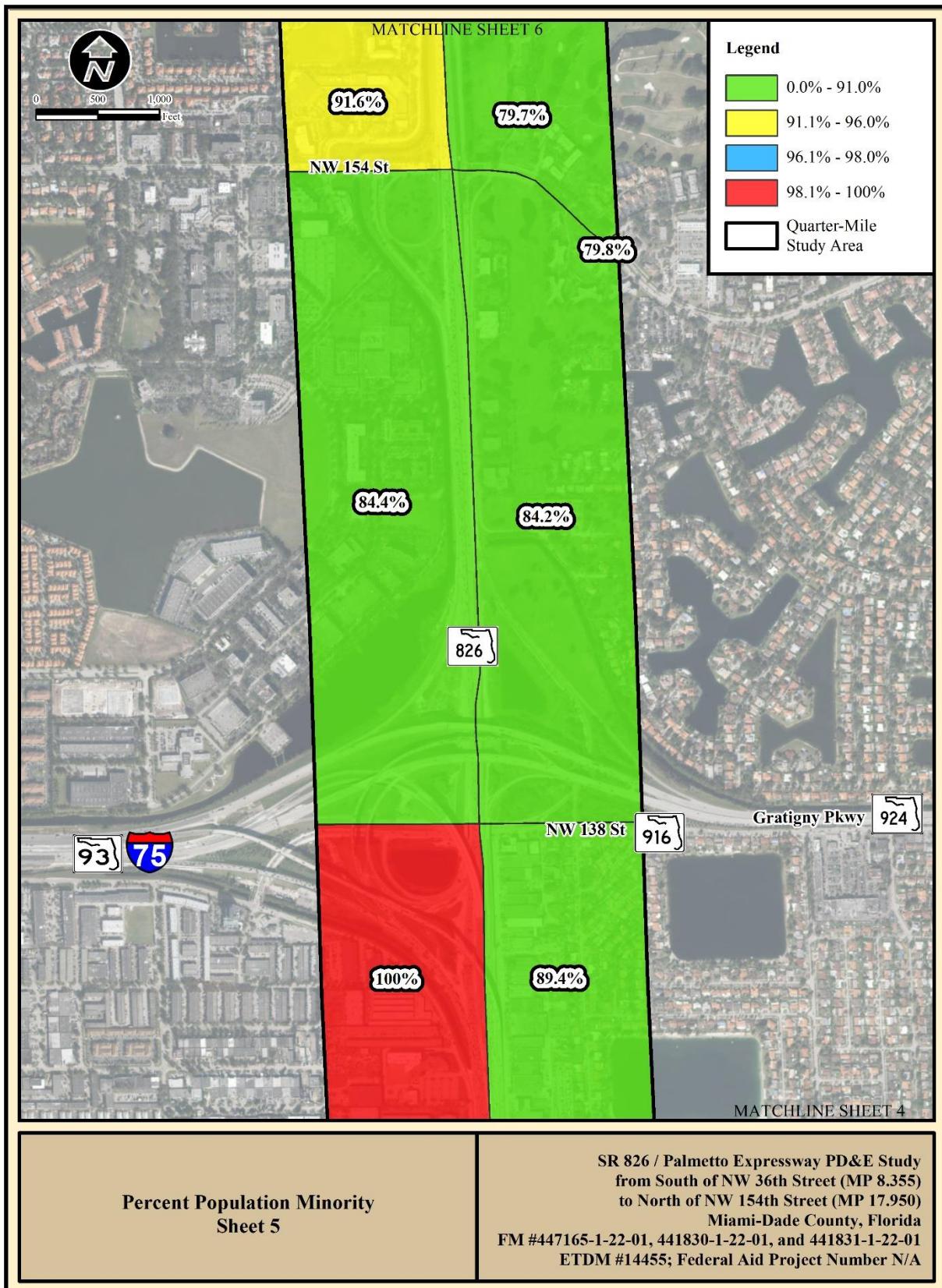
Figure A-8: Percent Minority Map 3



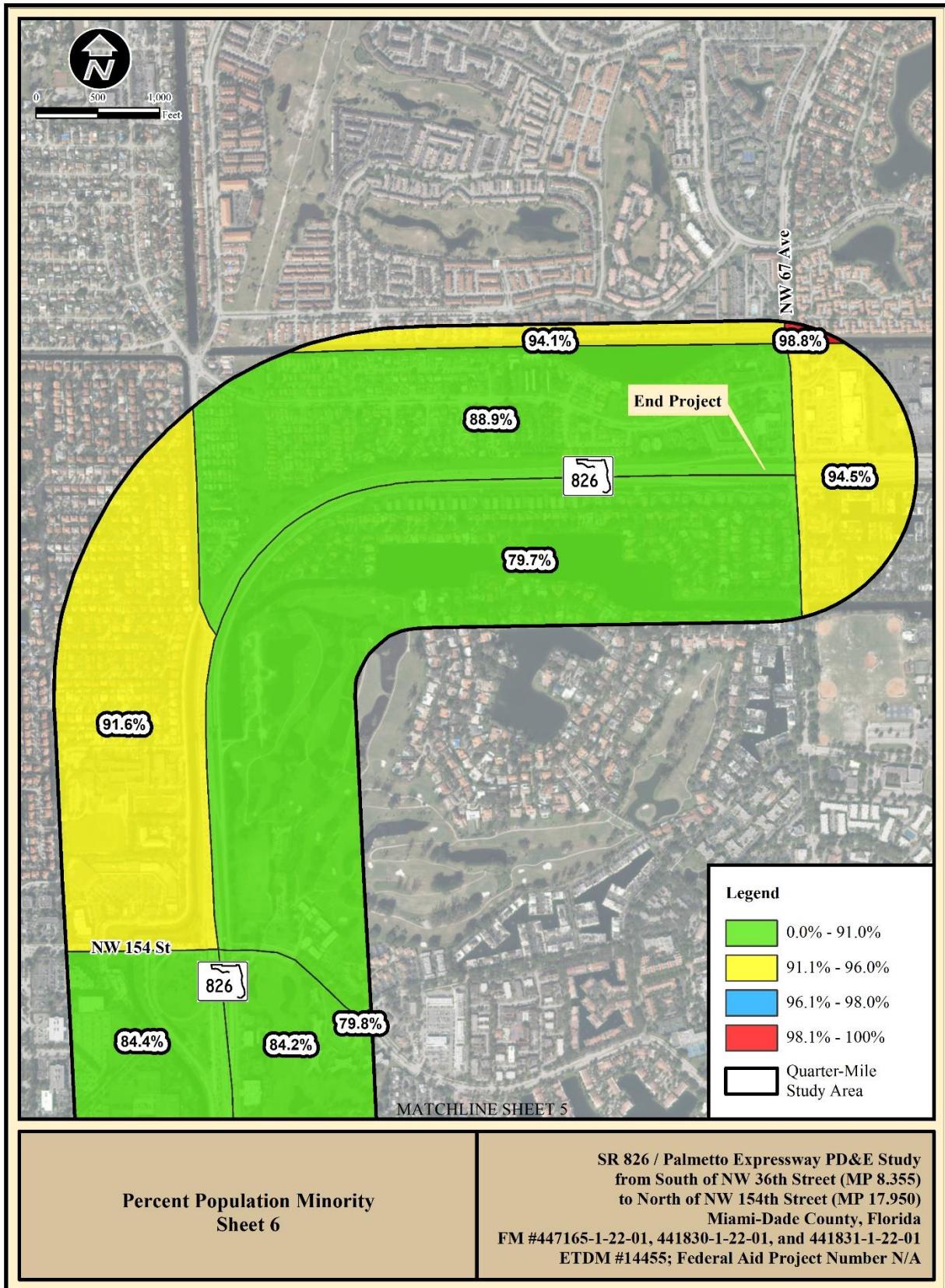
**Figure A-8: Percent Minority Map 4**



**Figure A-8: Percent Minority Map 5**



**Figure A-8: Percent Minority Map 6**



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## **Appendix B**

ETDM Summary Report

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## **Appendix A**

Demographics Maps

**Figure A-1: Percent Age 65 and Older Key Sheet**

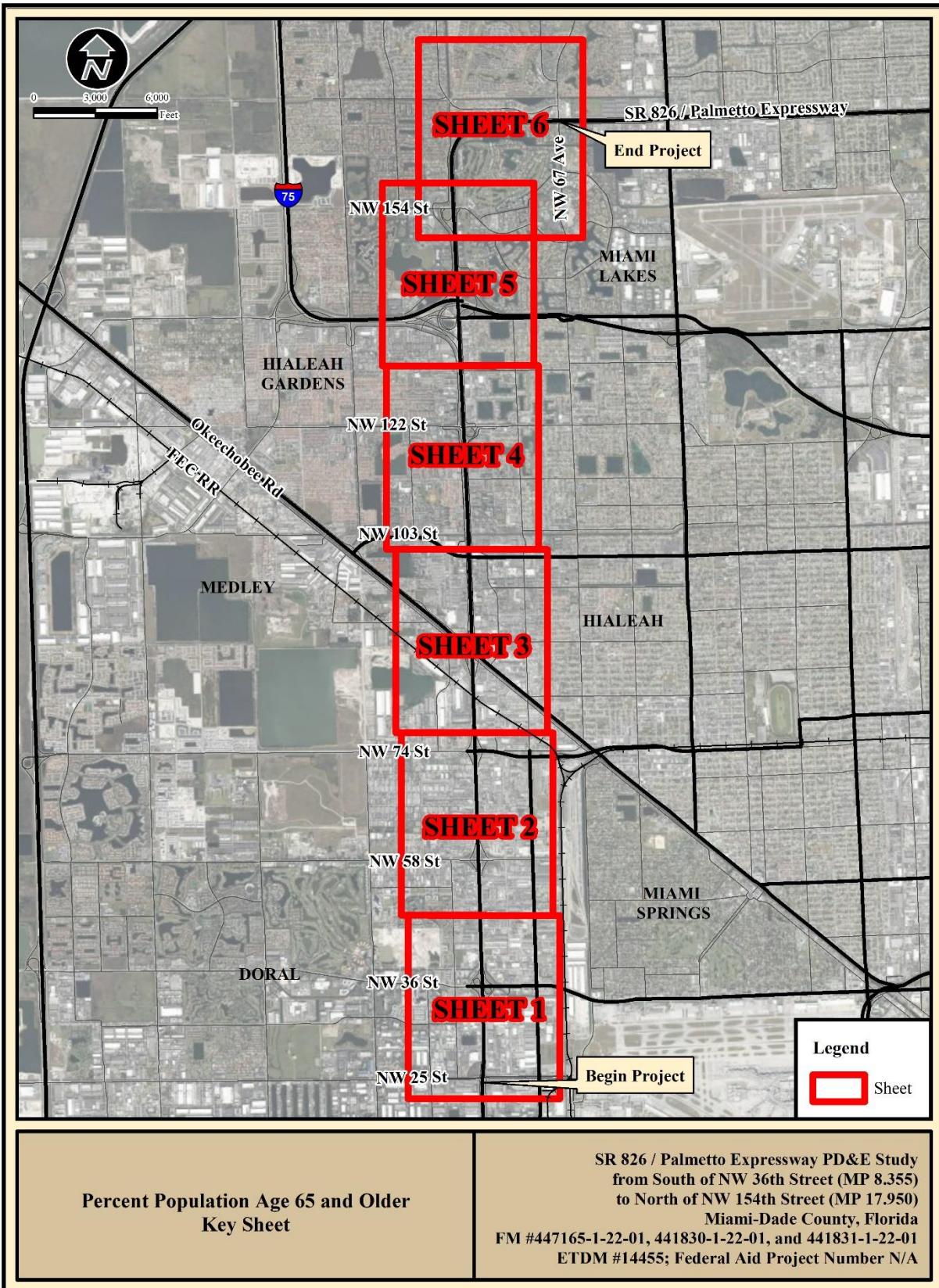
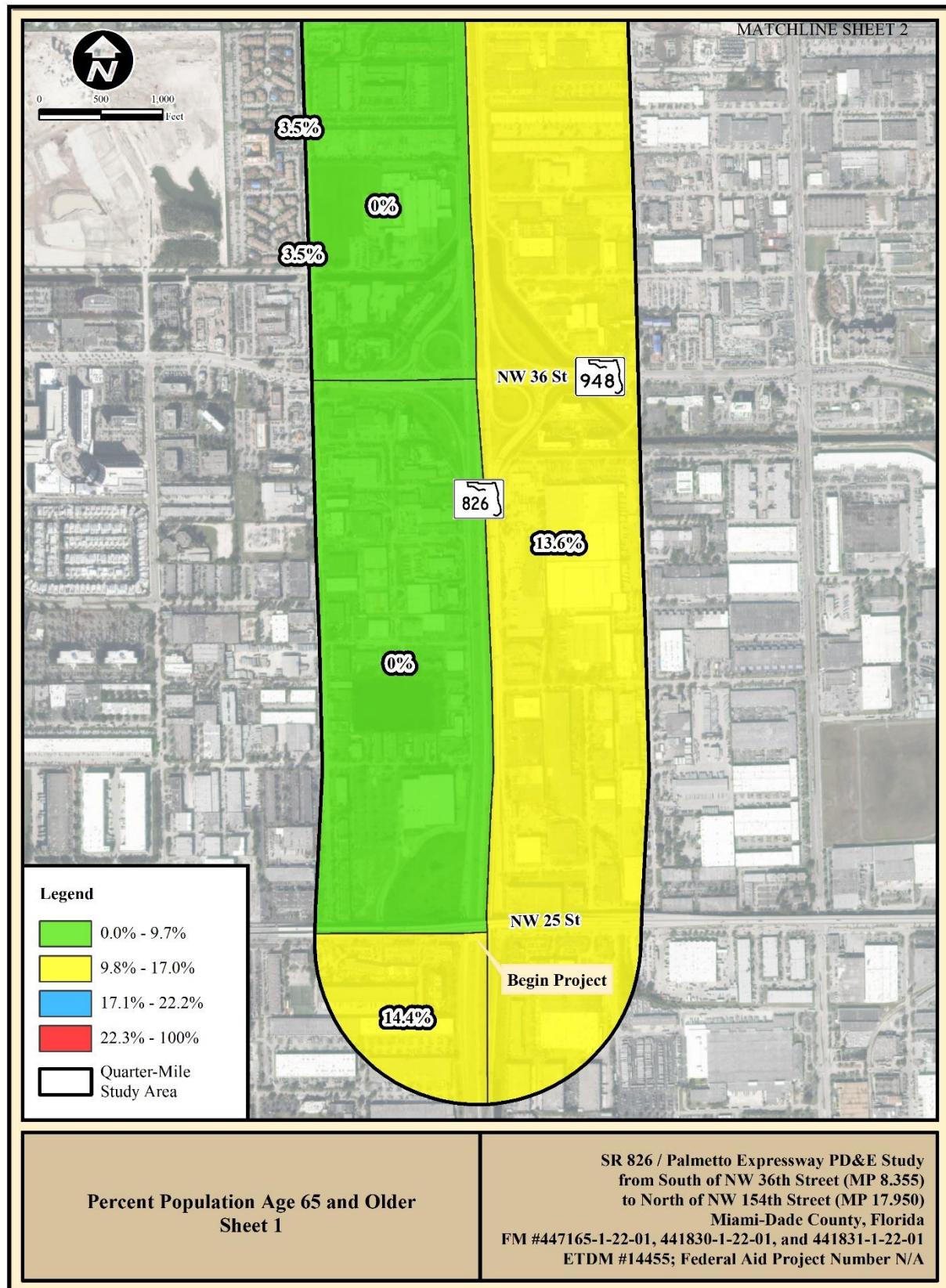


Figure A-2: Percent Age 65 and Older Map 1



**Figure A-2: Percent Age 65 and Older Map 2**

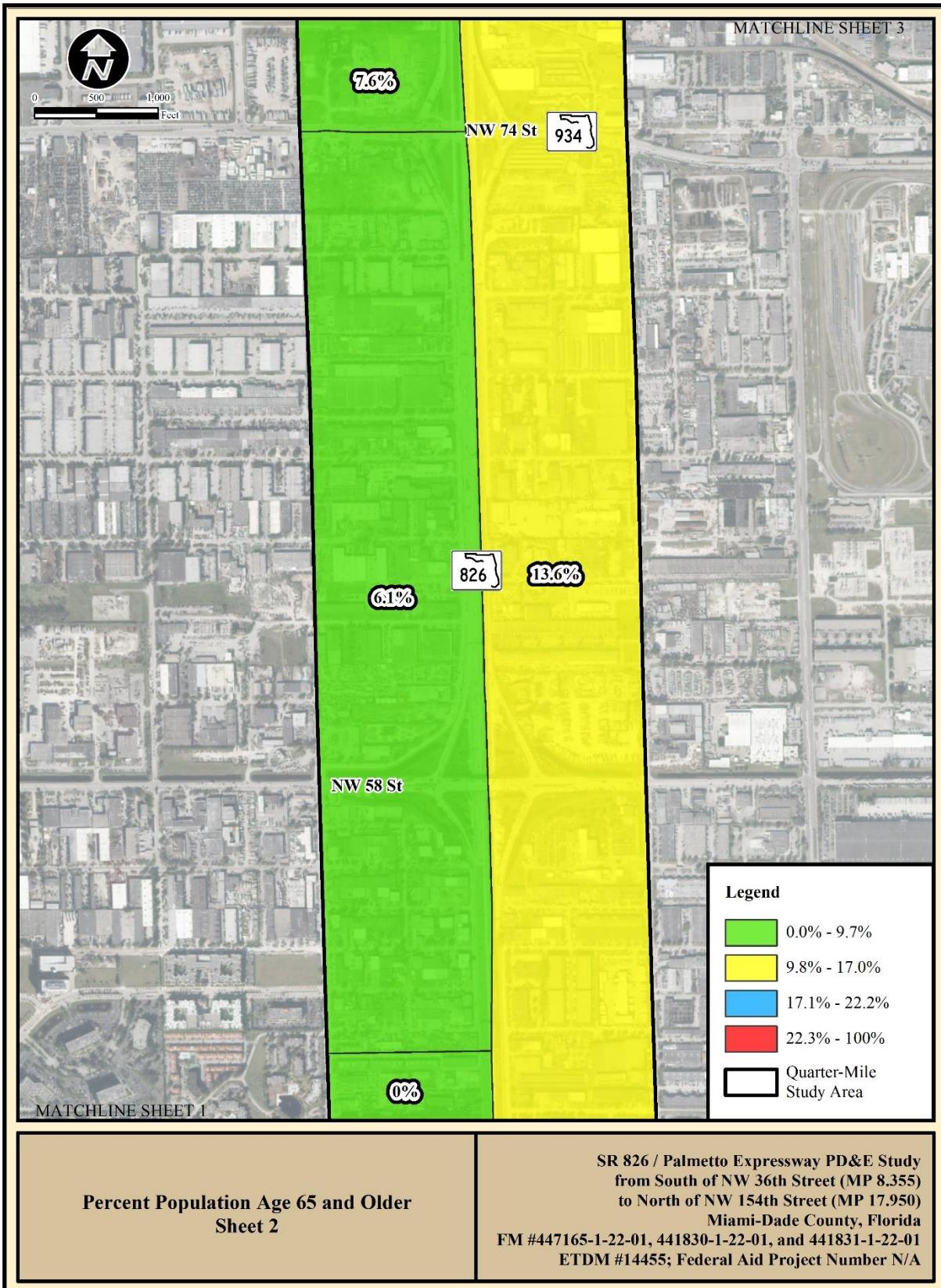


Figure A-2: Percent Age 65 and Older Map 3

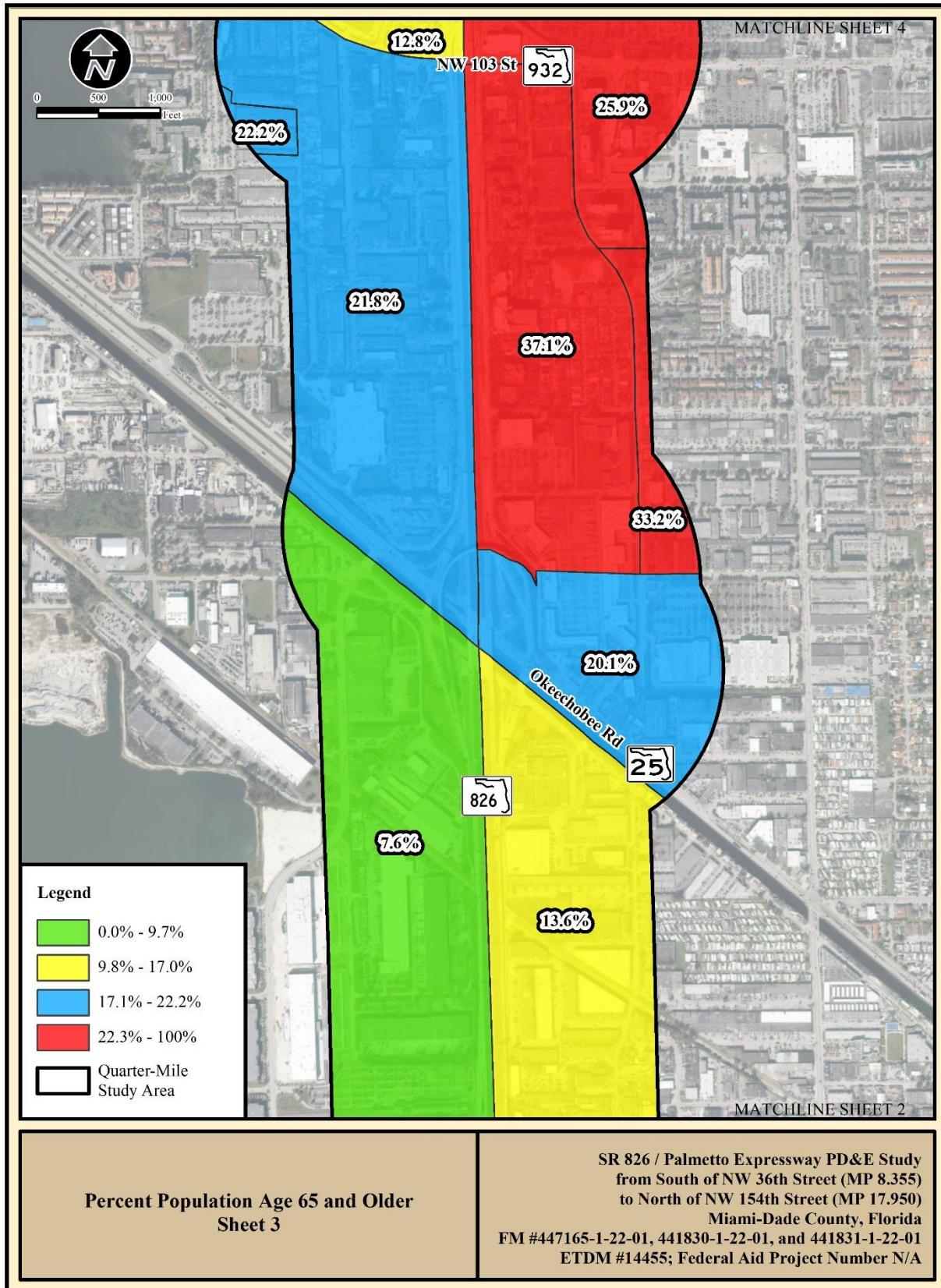


Figure A-2: Percent Age 65 and Older Map 4

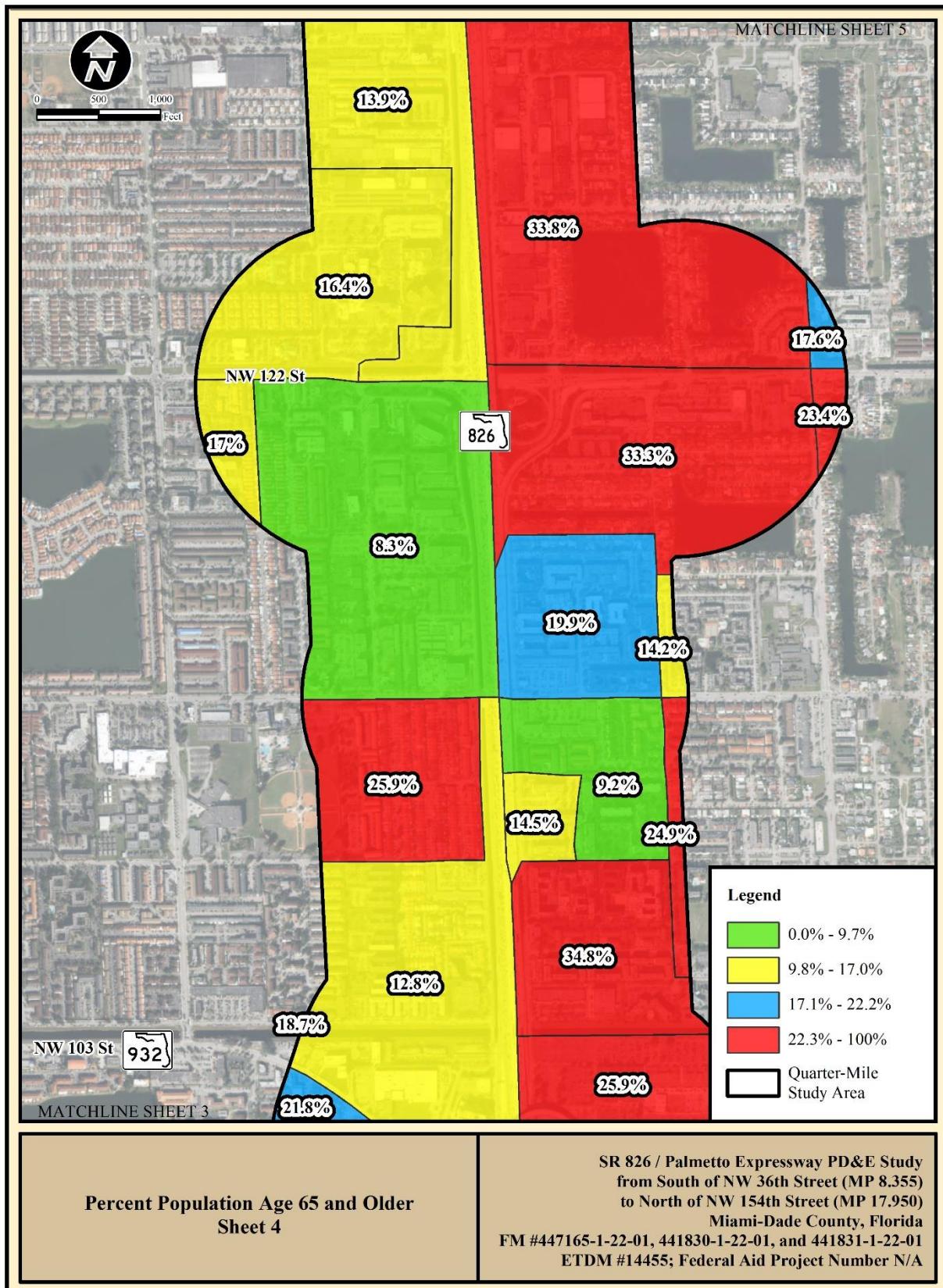
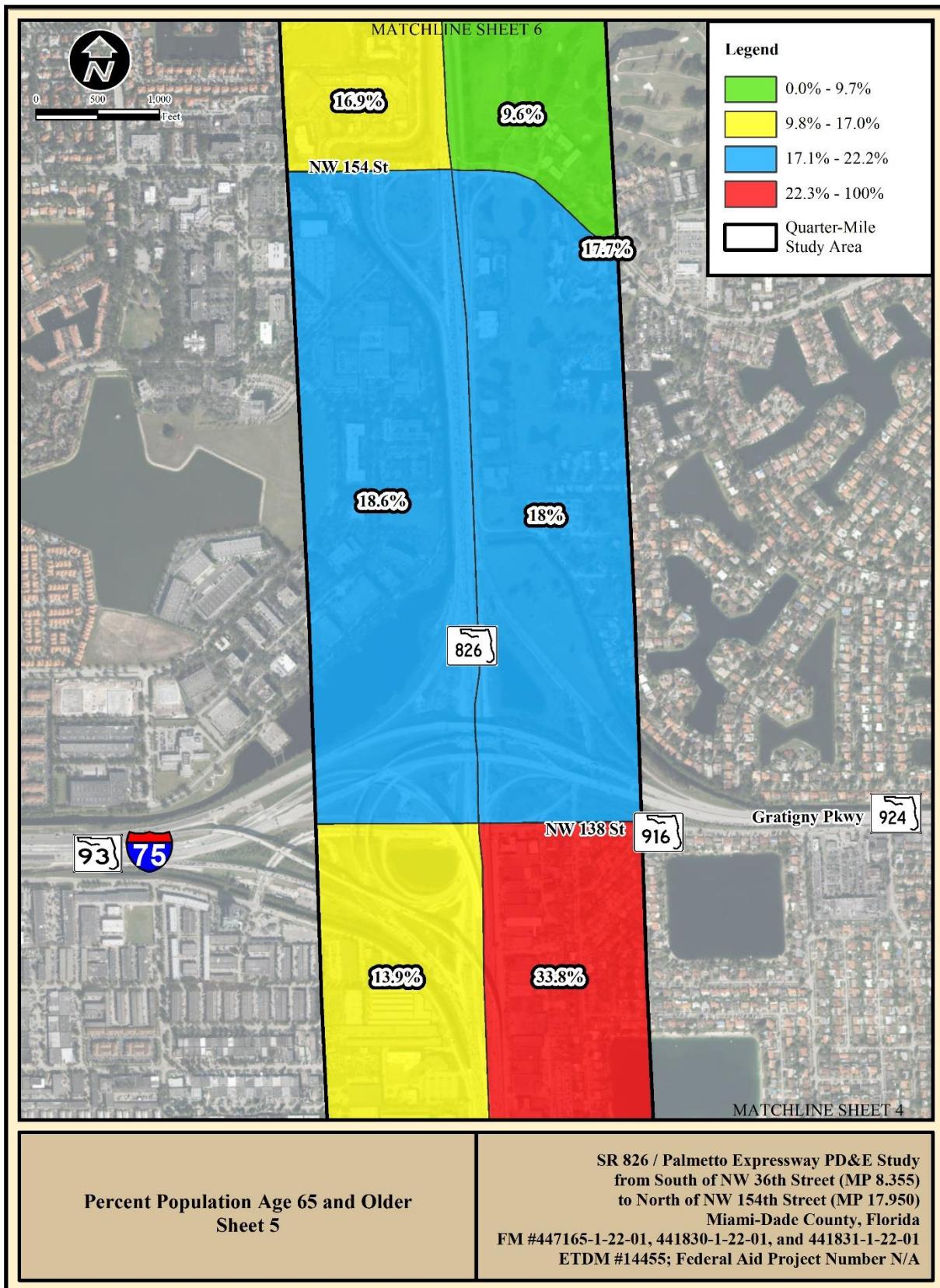


Figure A-2: Percent Age 65 and Older Map 5



**Figure A-2: Percent Age 65 and Older Map 6**

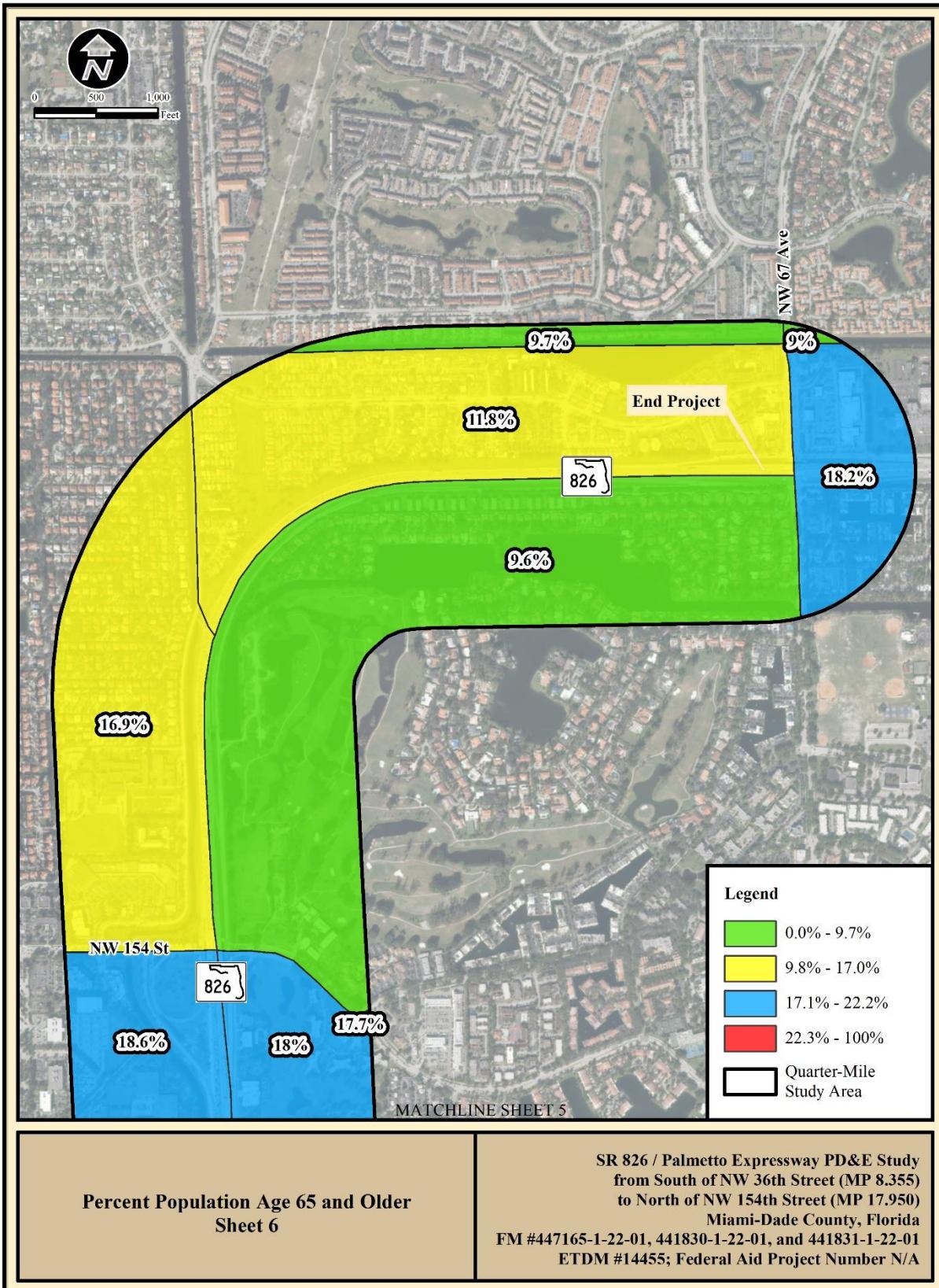


Figure A-3: Percent Below Poverty Key Sheet

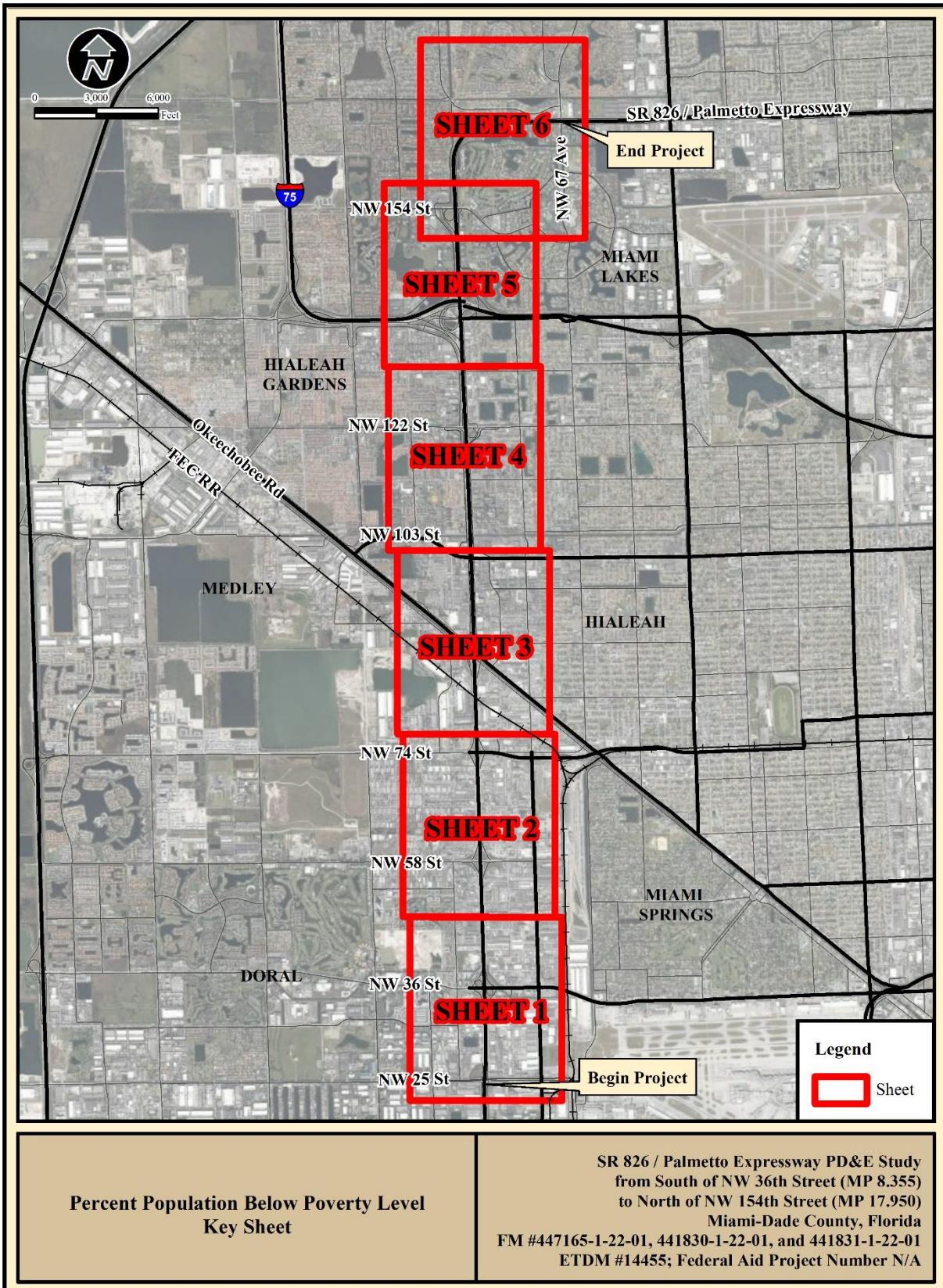


Figure A-4: Percent Below Poverty Map 1

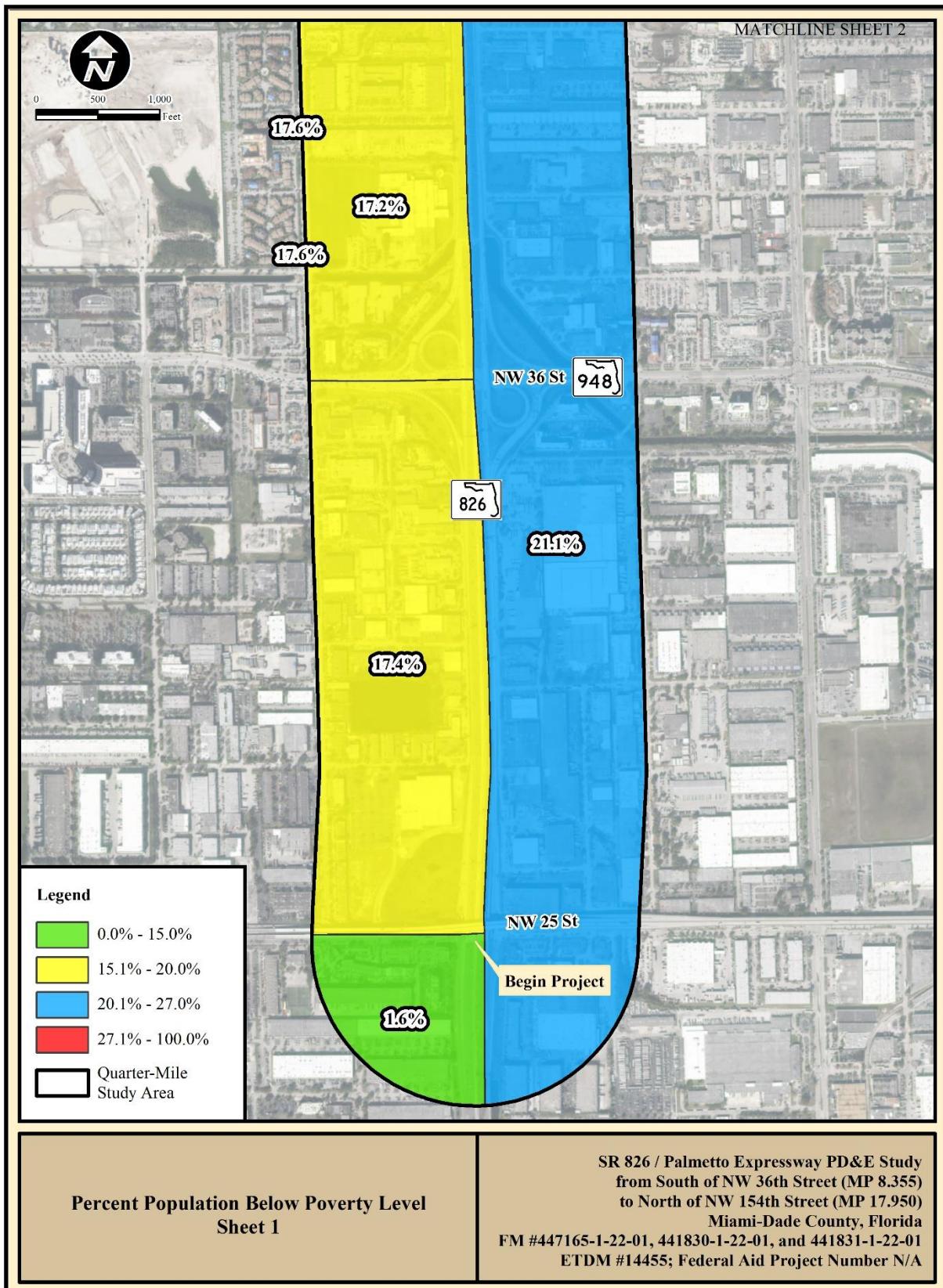


Figure A-4: Percent Below Poverty Map 2

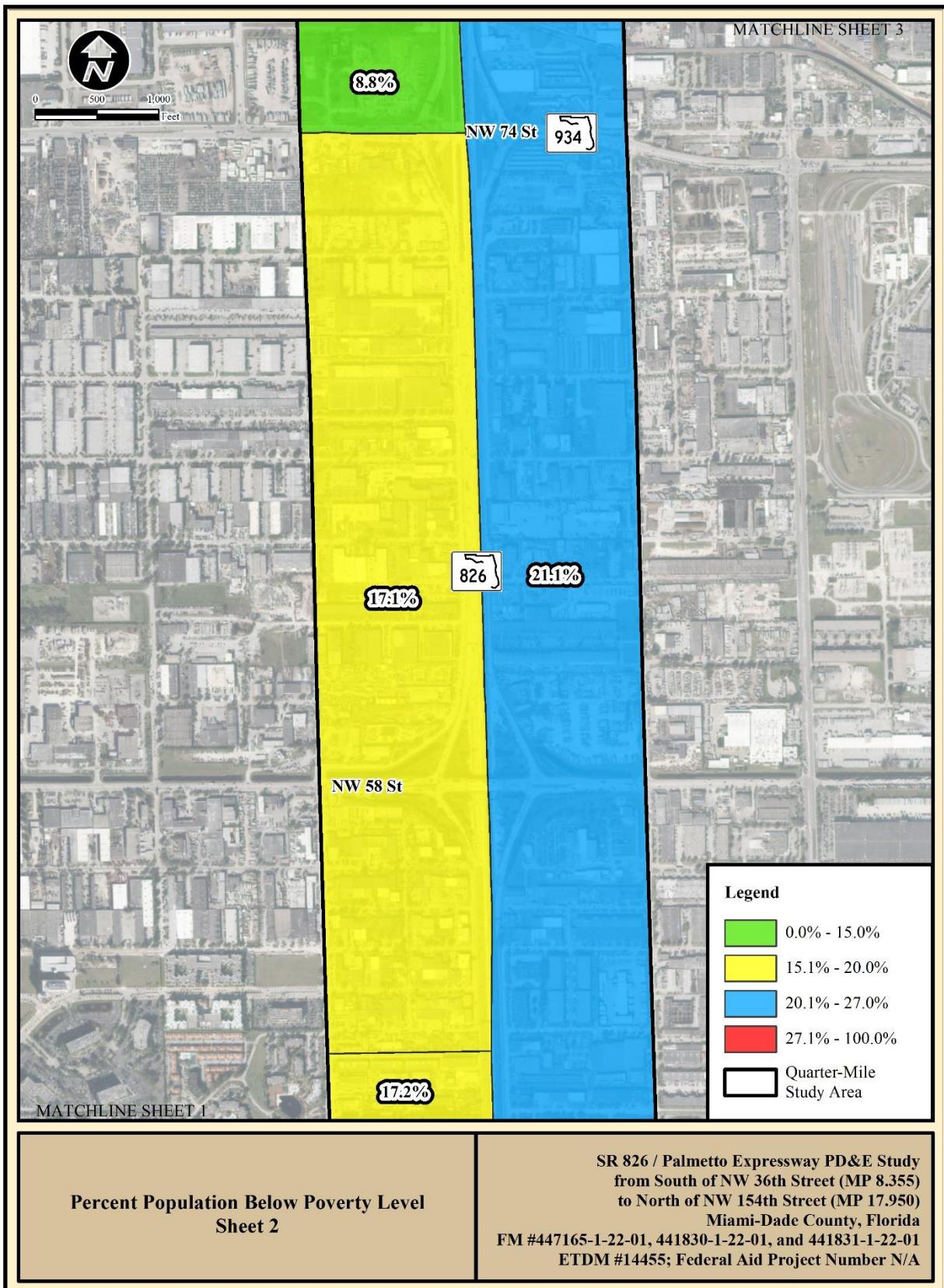


Figure A-4: Percent Below Poverty Map 3

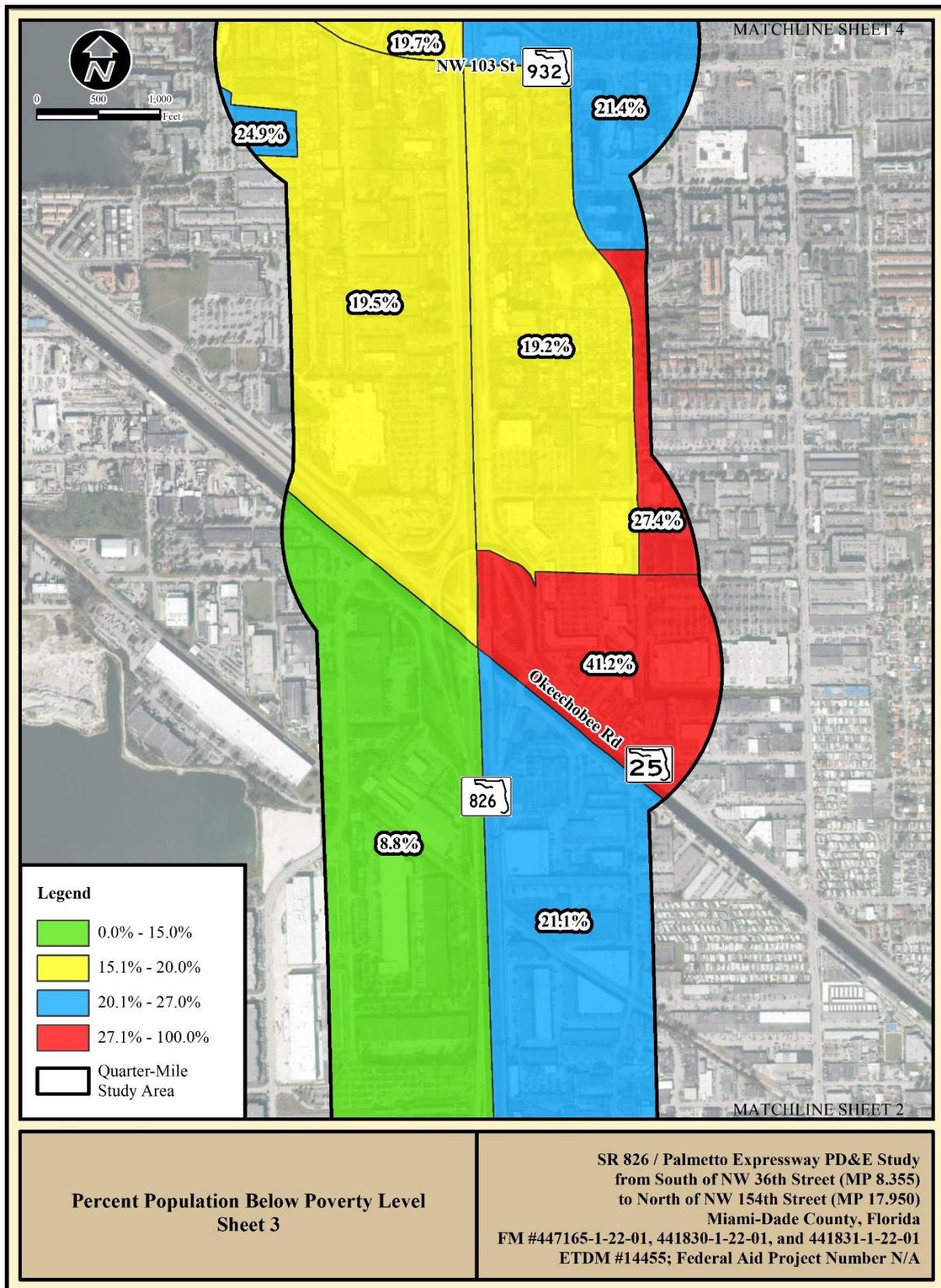


Figure A-4: Percent Below Poverty Map 4

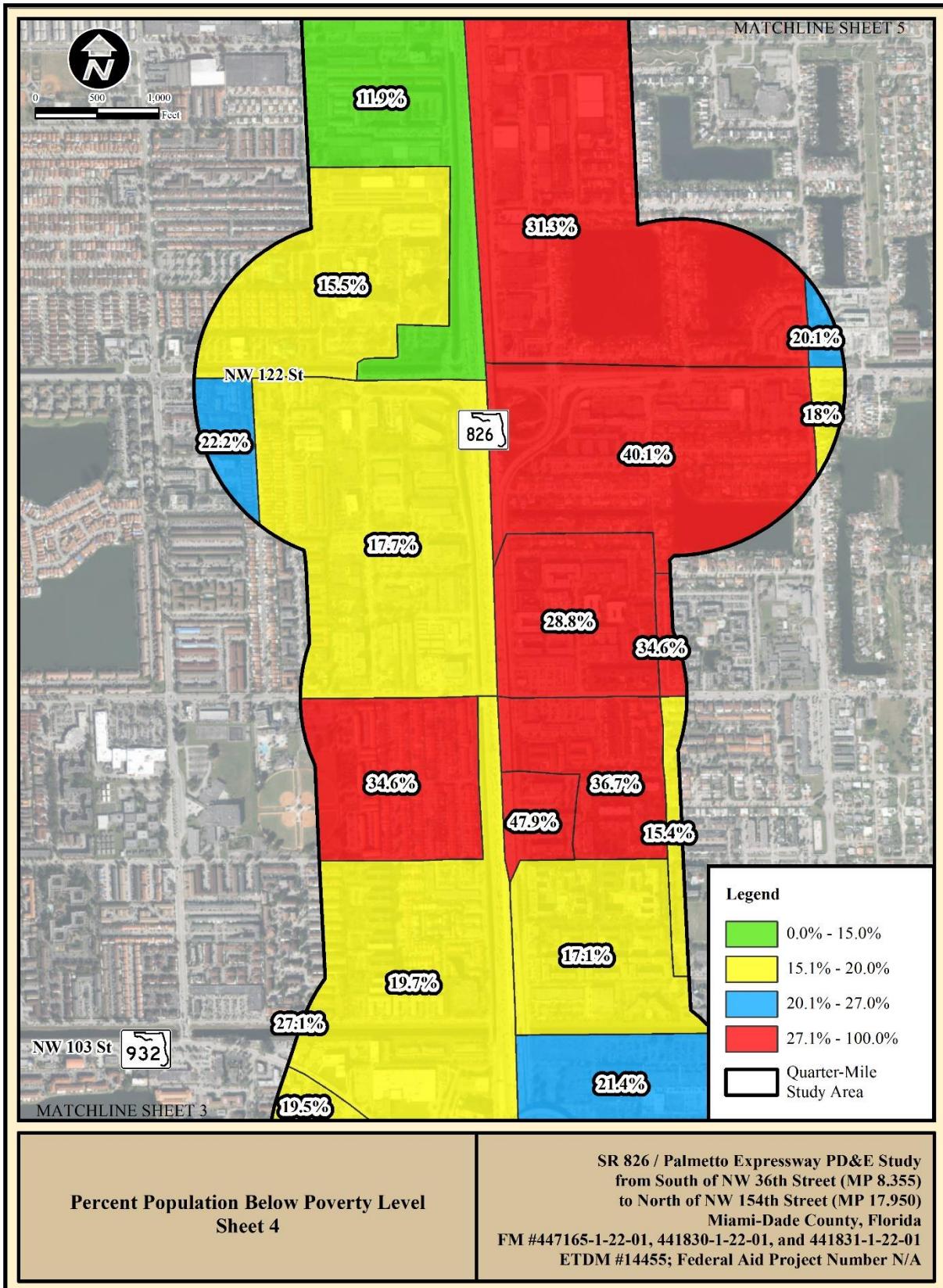


Figure A-4: Percent Below Poverty Map 5

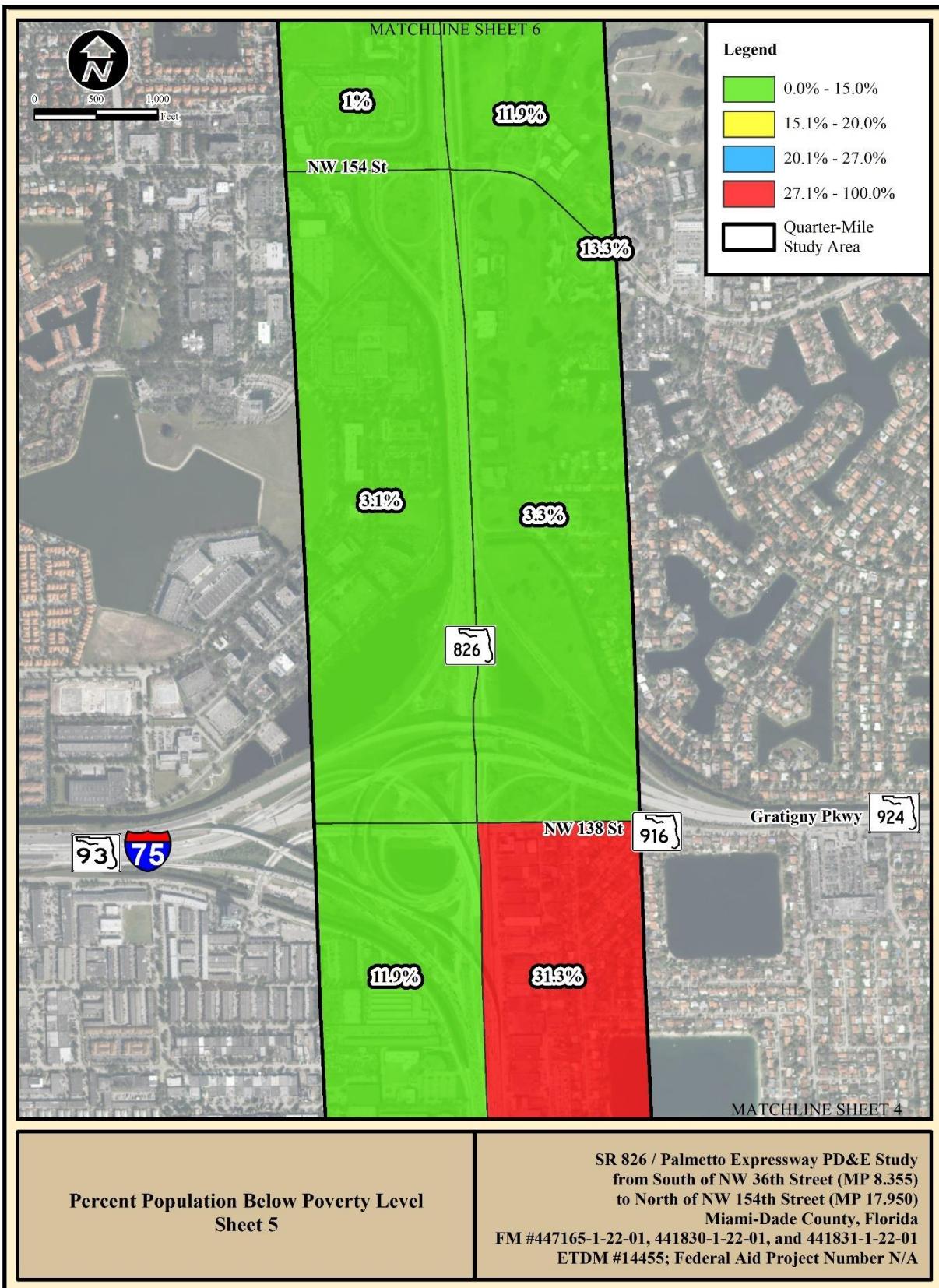


Figure A-4: Percent Below Poverty Map 6

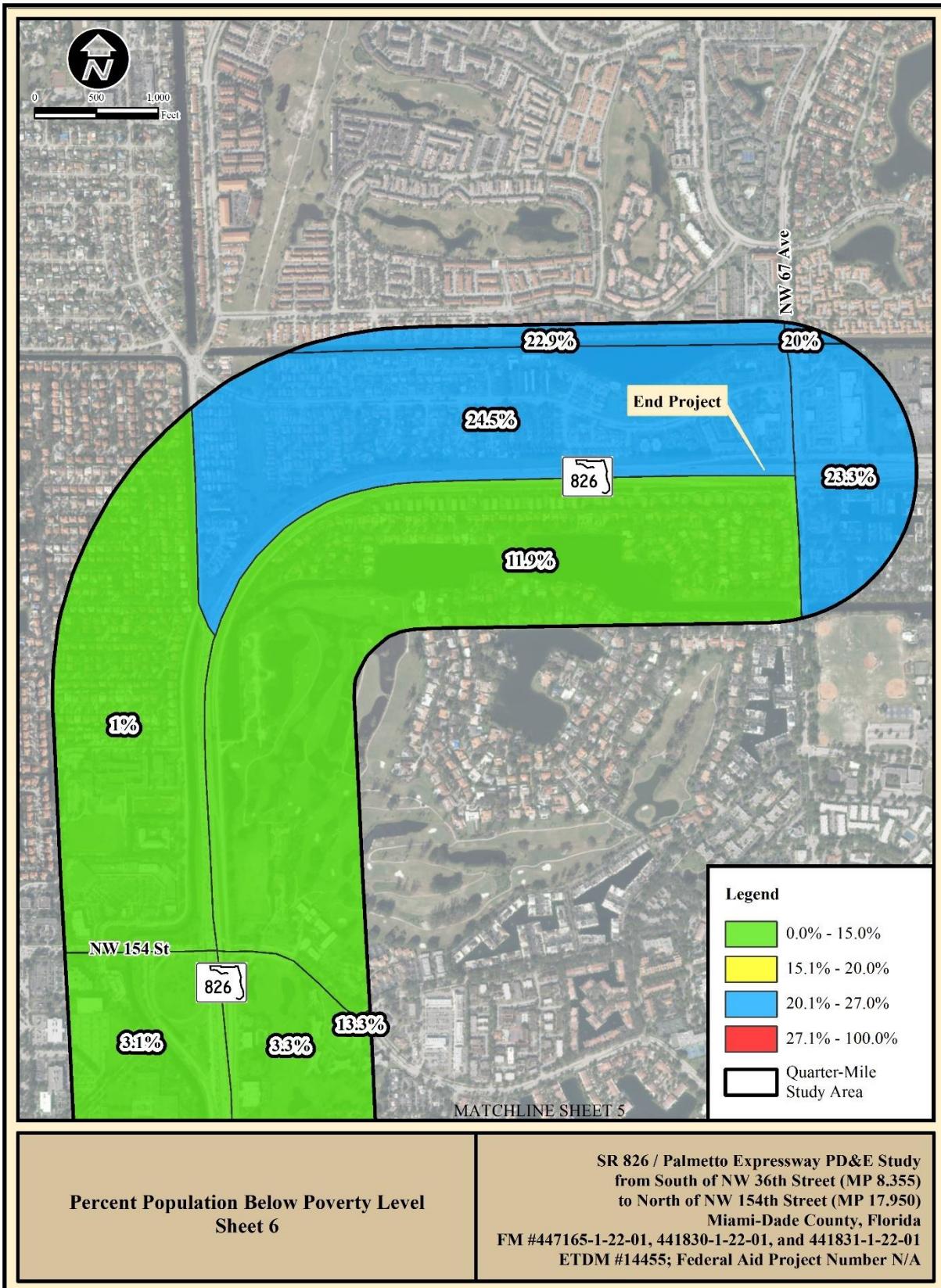


Figure A-5: Percent Hispanic Key Sheet

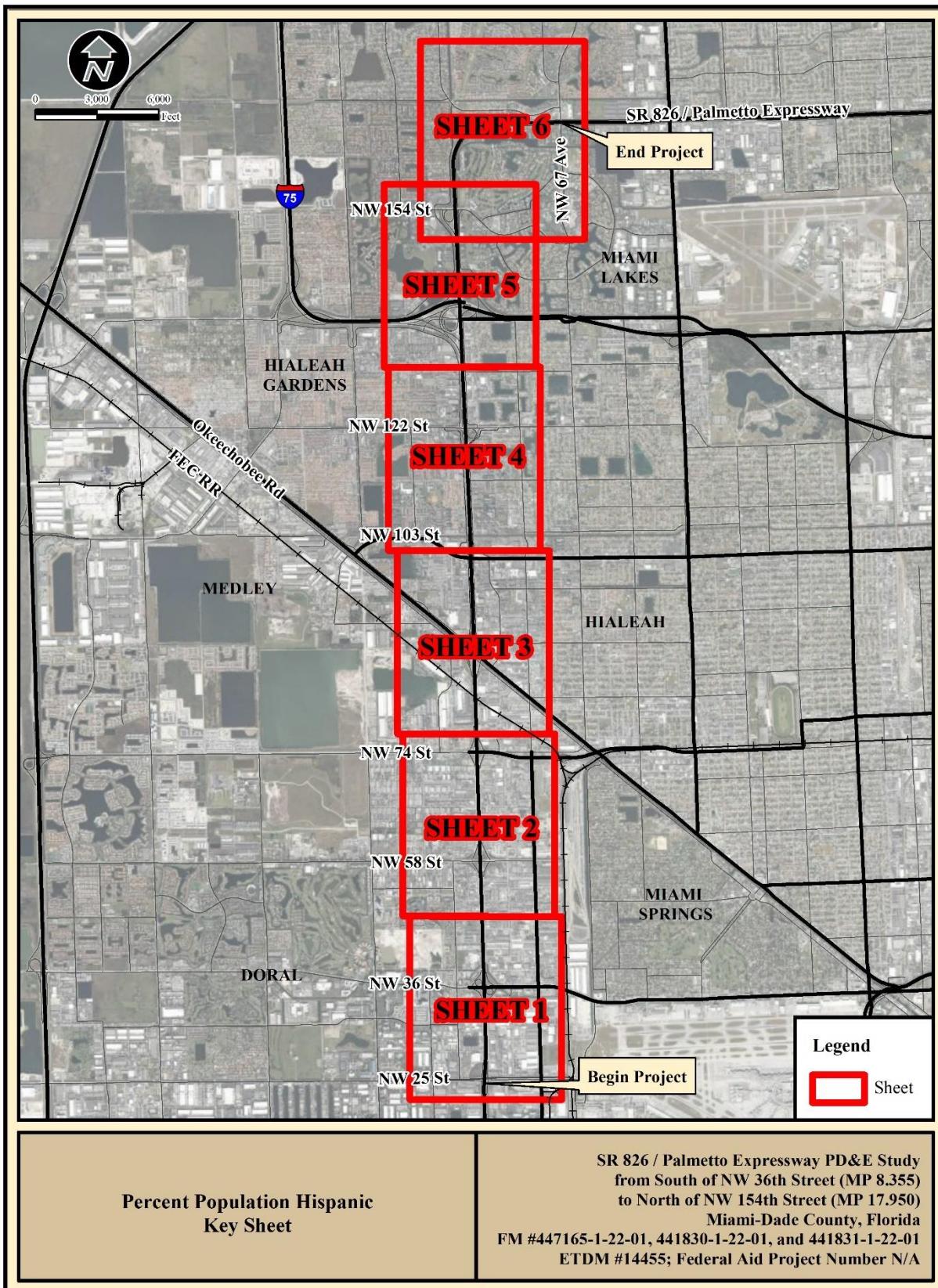
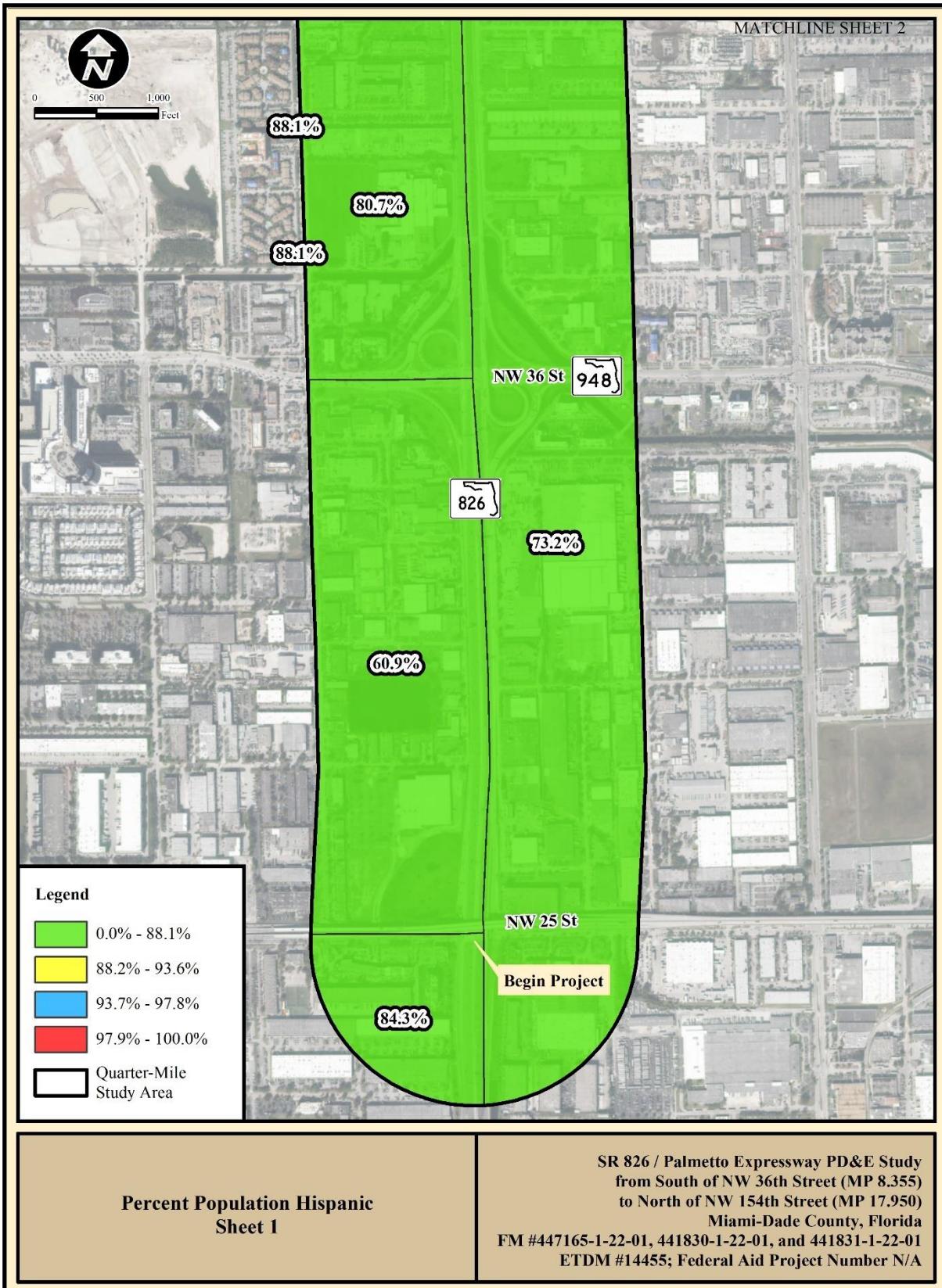


Figure A-6: Percent Hispanic Map 1



**Figure A-6: Percent Hispanic Map 2**

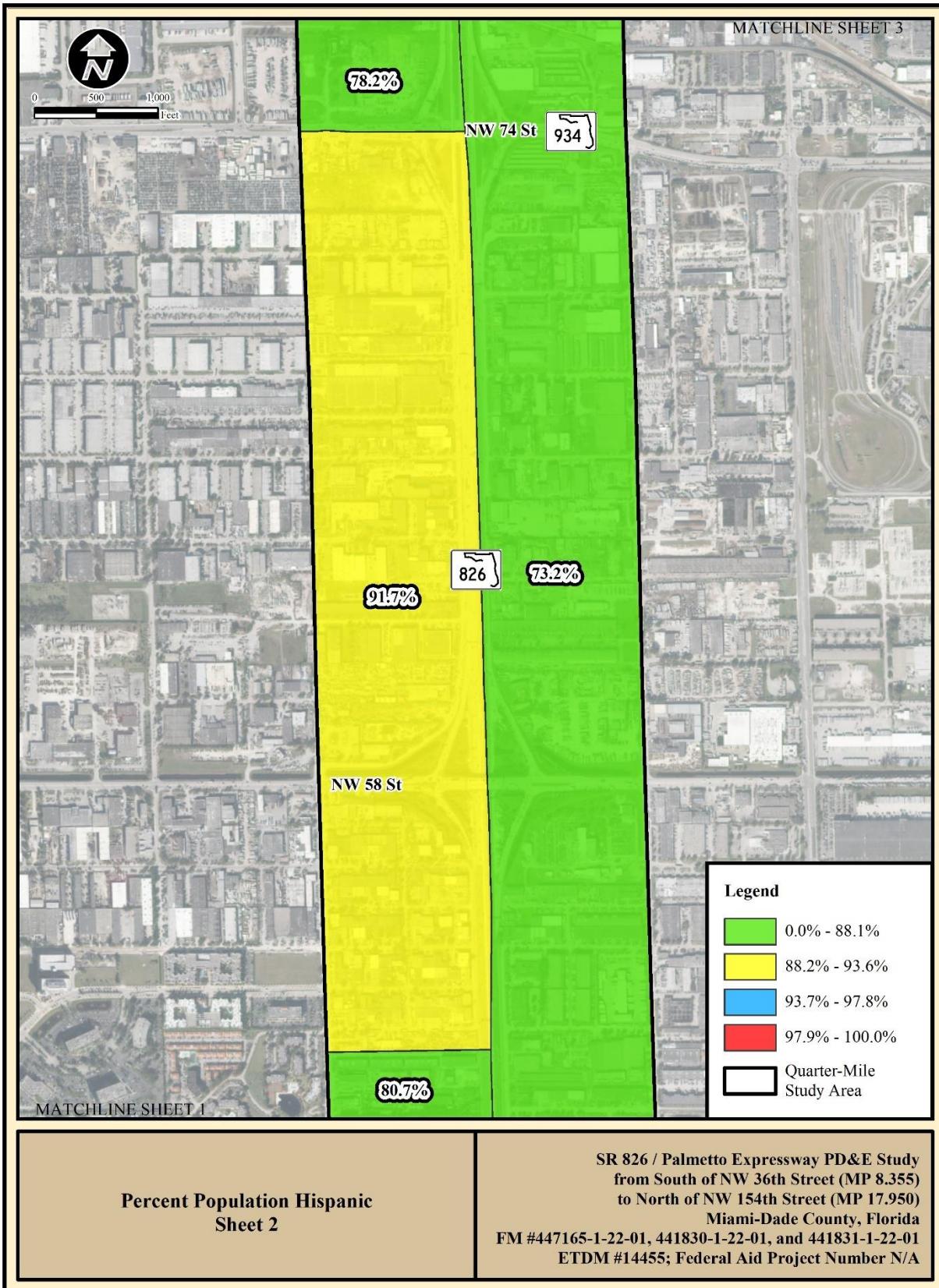


Figure A-6: Percent Hispanic Map 3

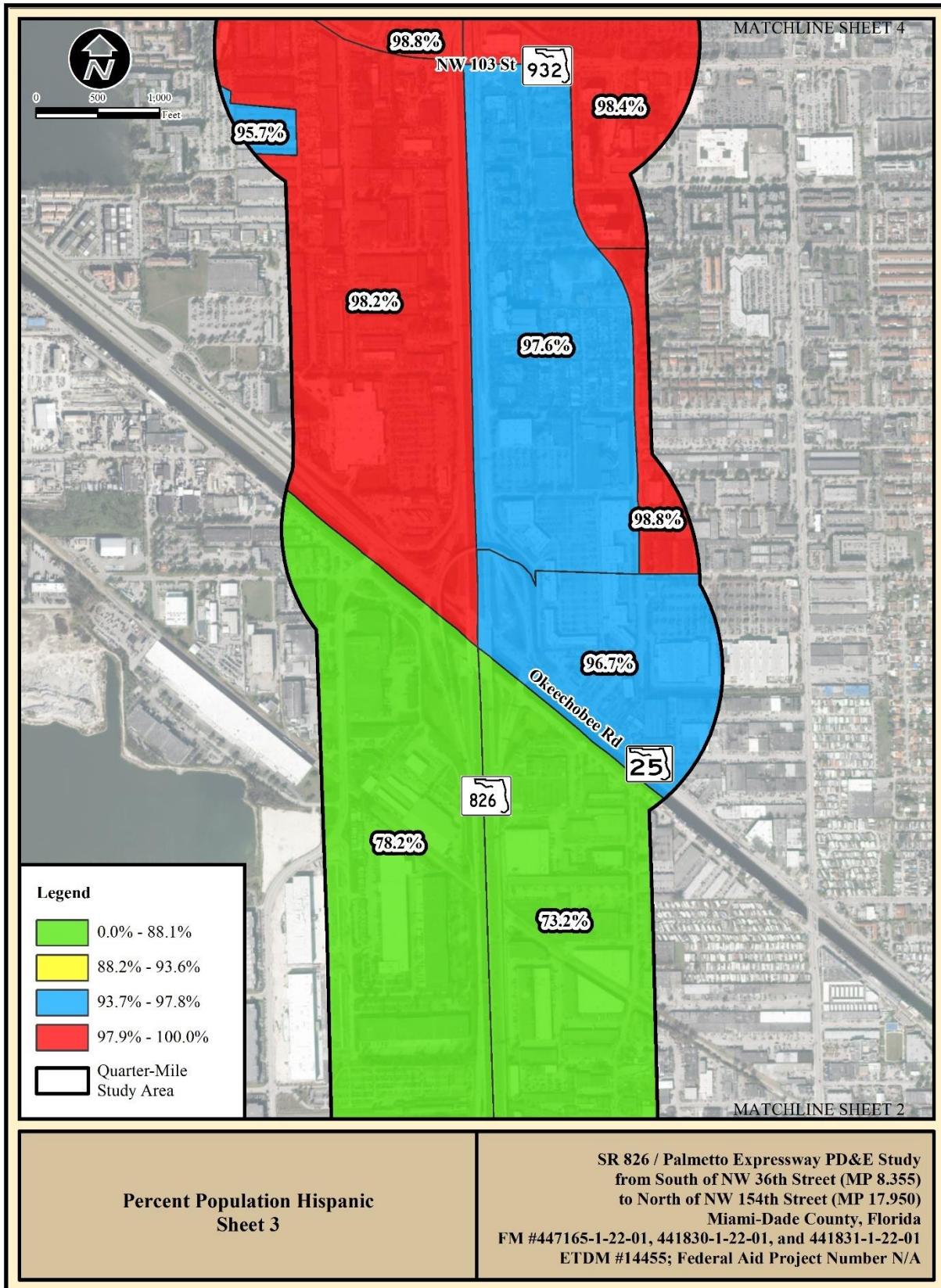
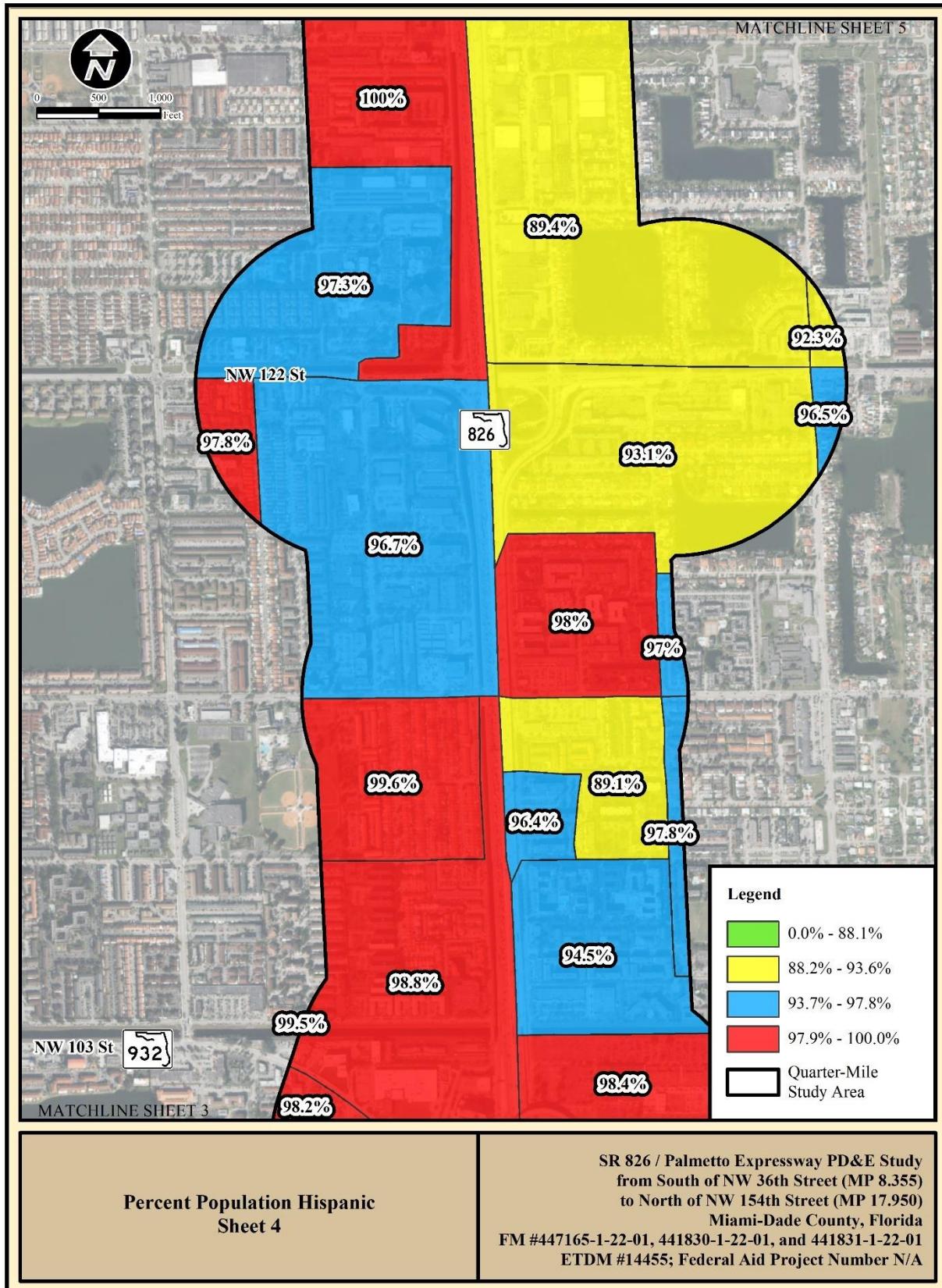


Figure A-6: Percent Hispanic Map 4



**Figure A-6: Percent Hispanic Map 5**

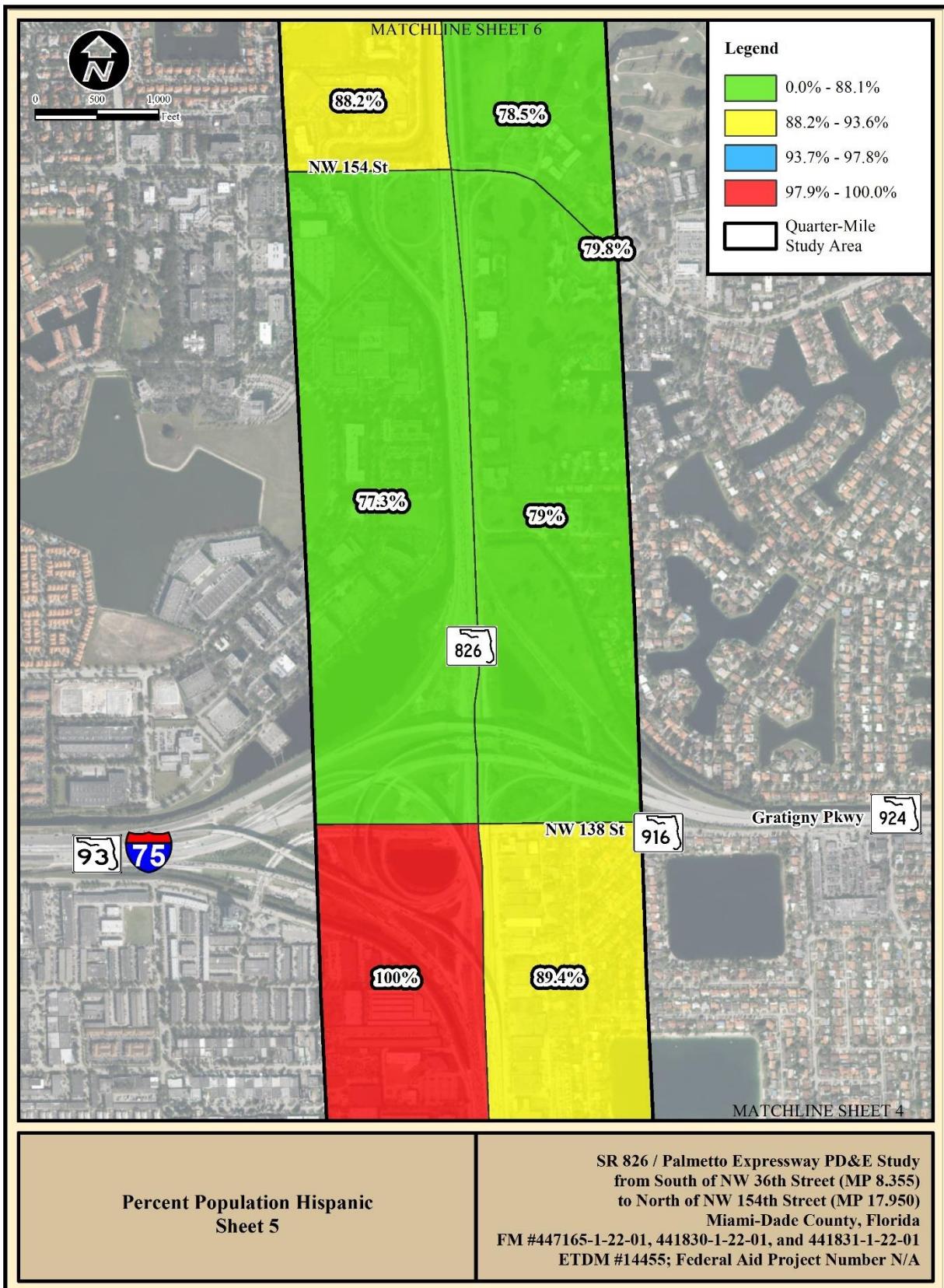


Figure A-6: Percent Hispanic Map 6

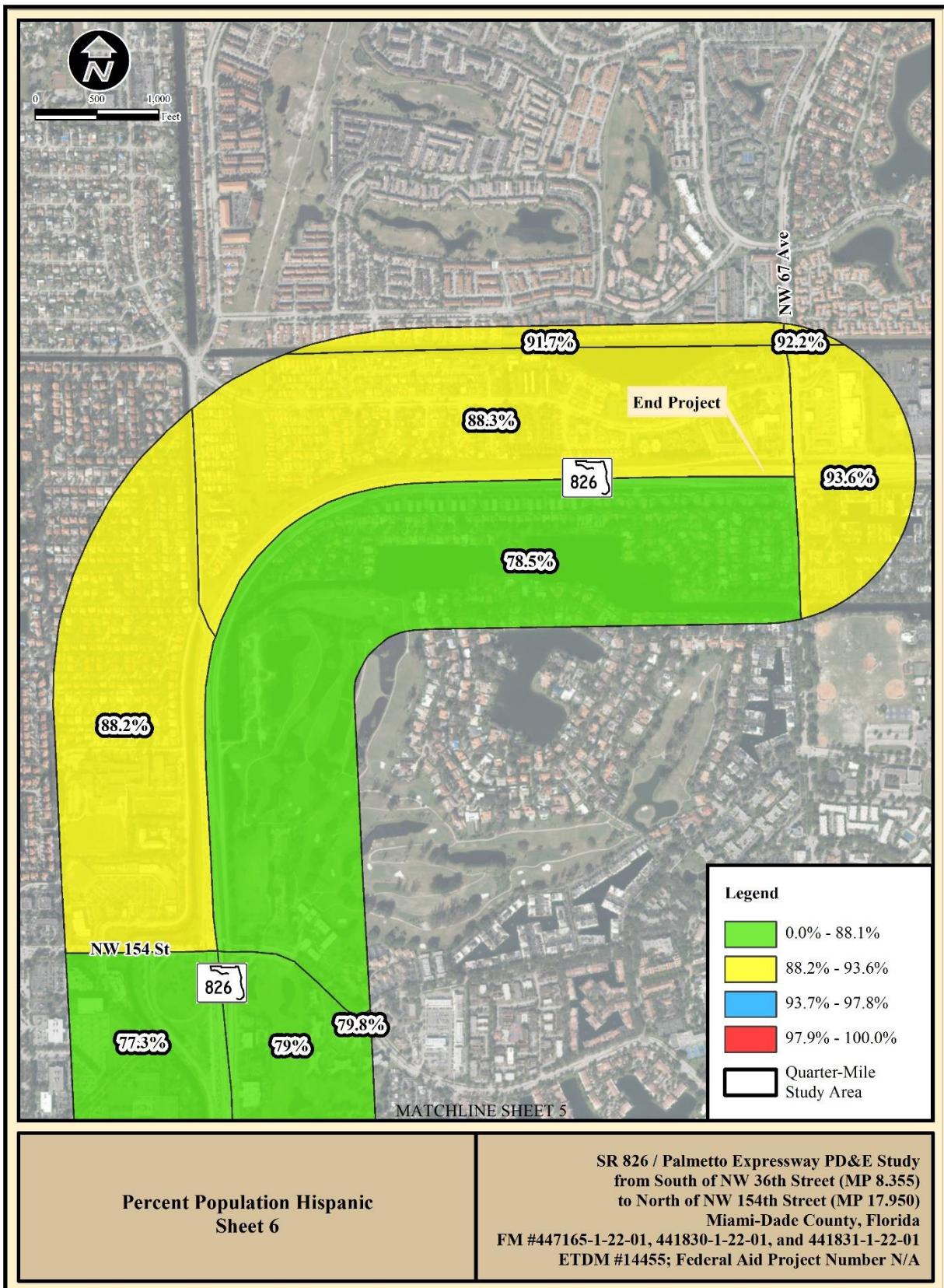
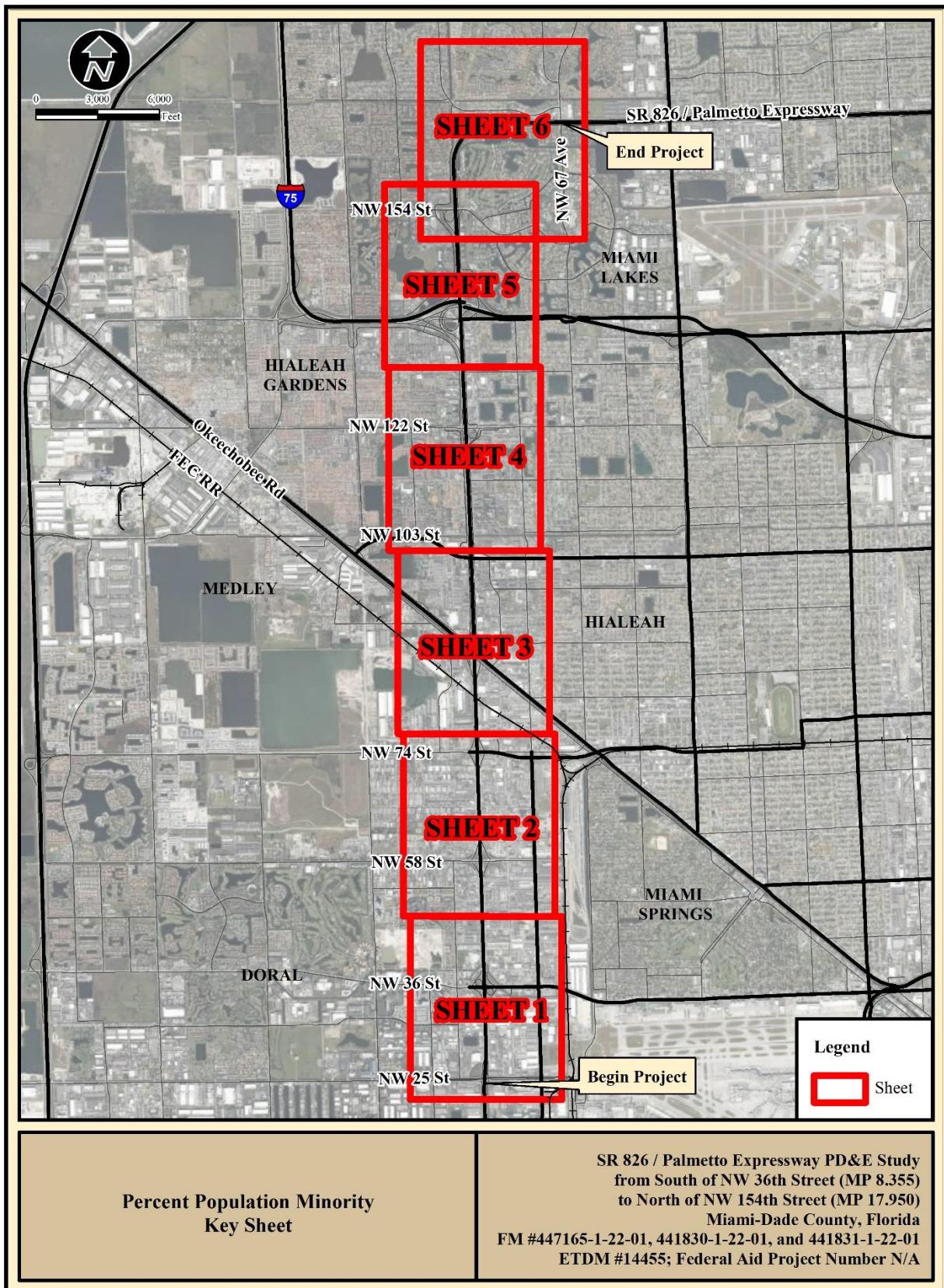
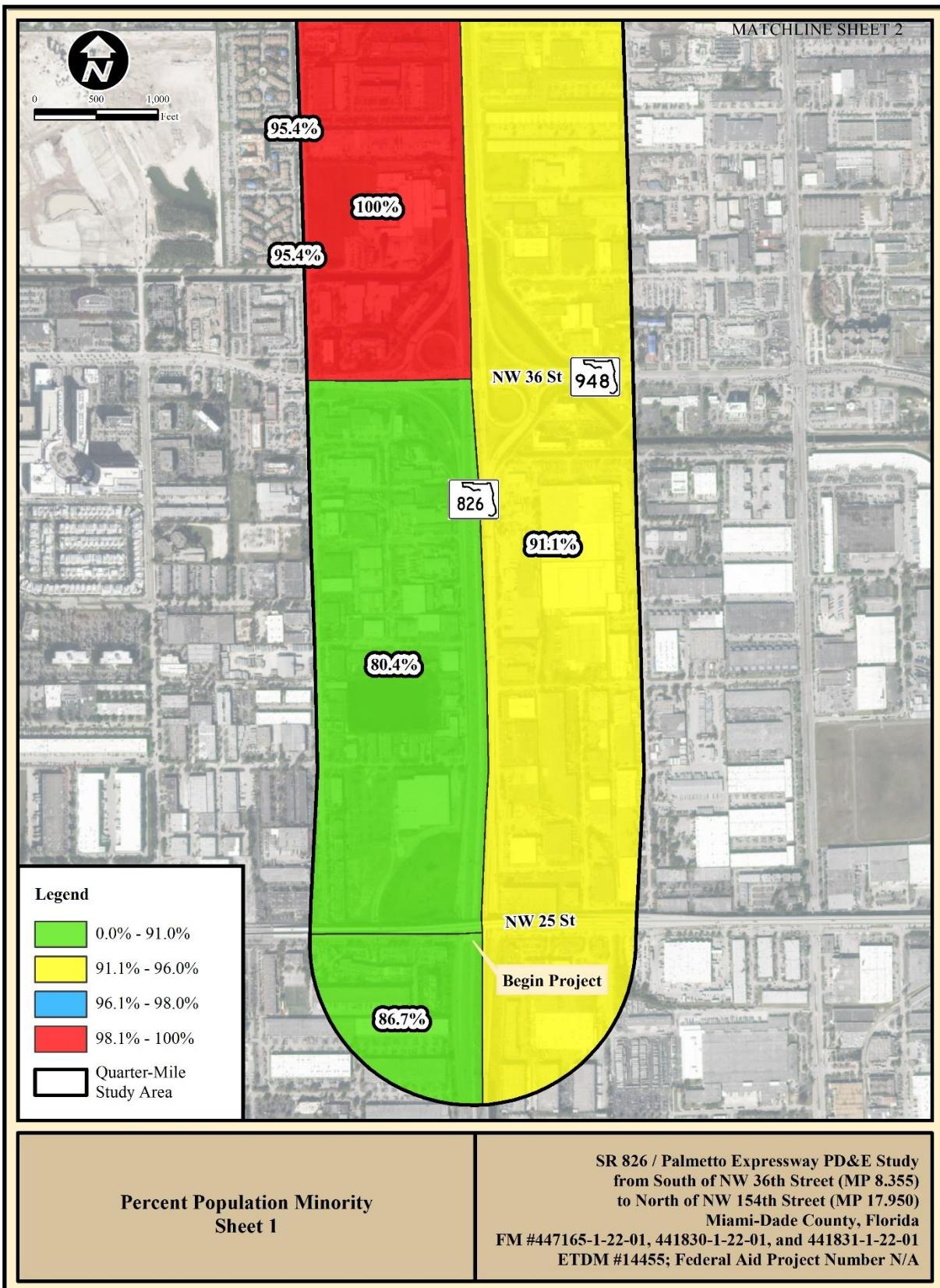


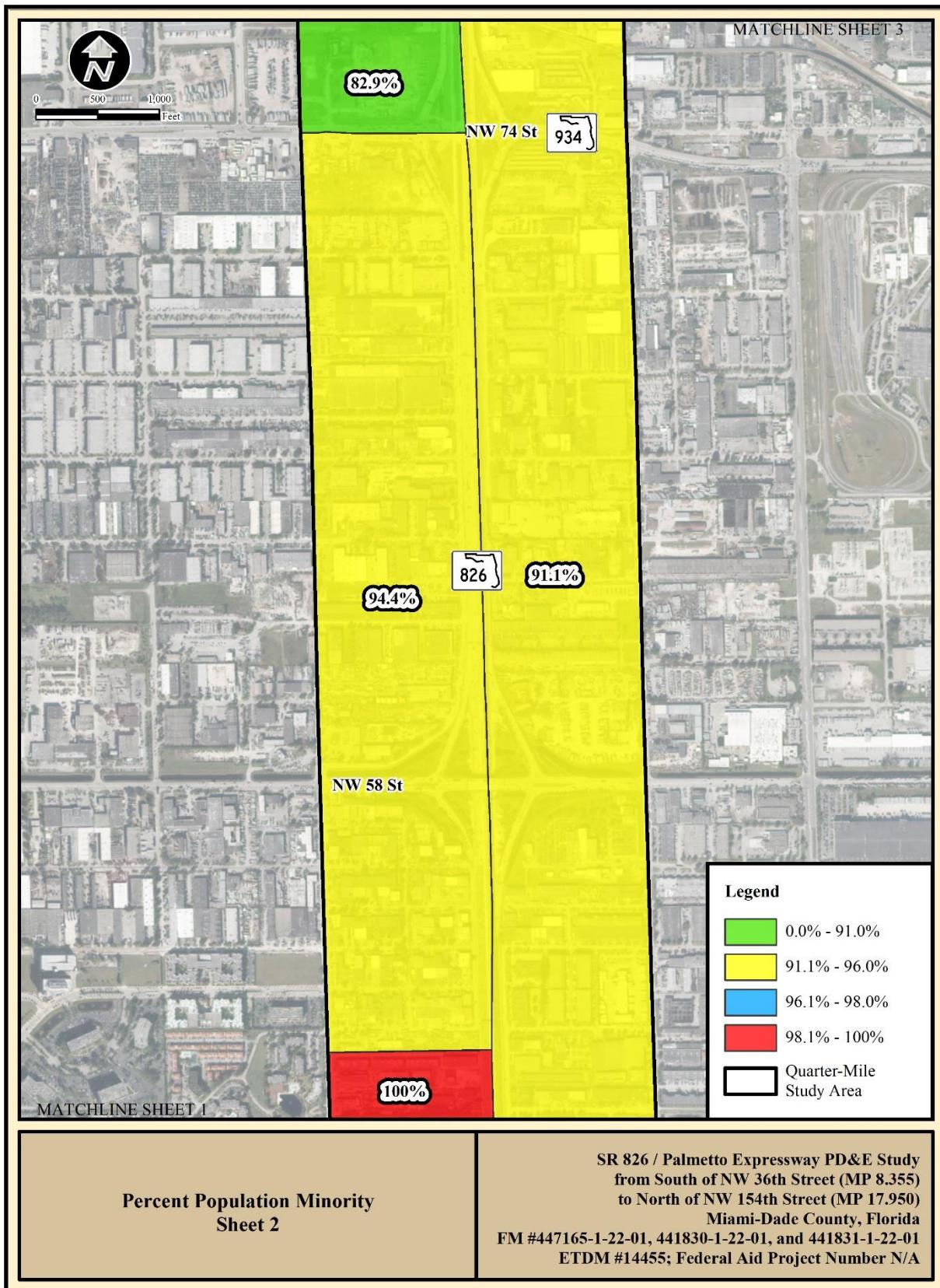
Figure A-7: Percent Minority Key Sheet



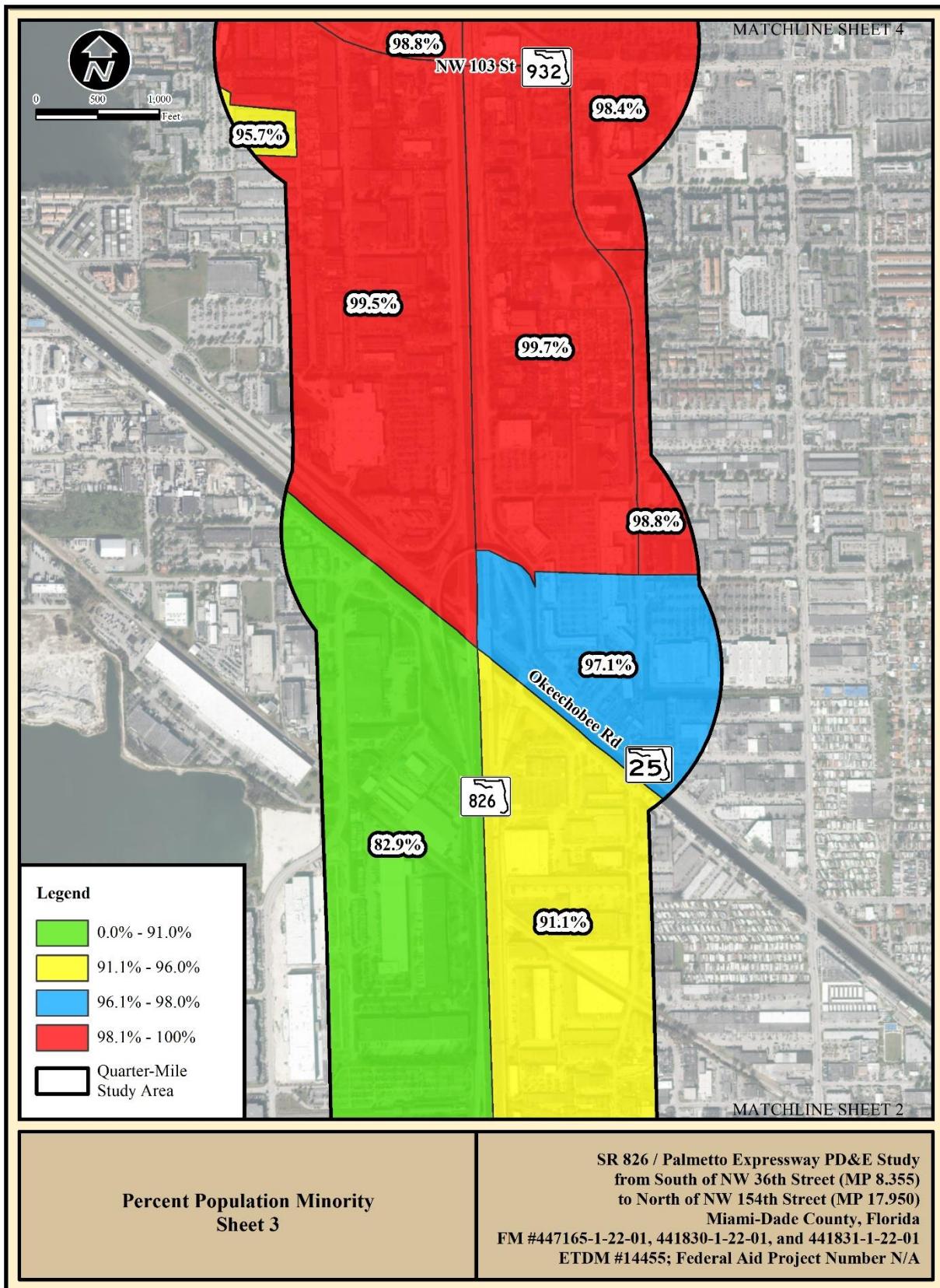
**Figure A-8: Percent Minority Map 1**



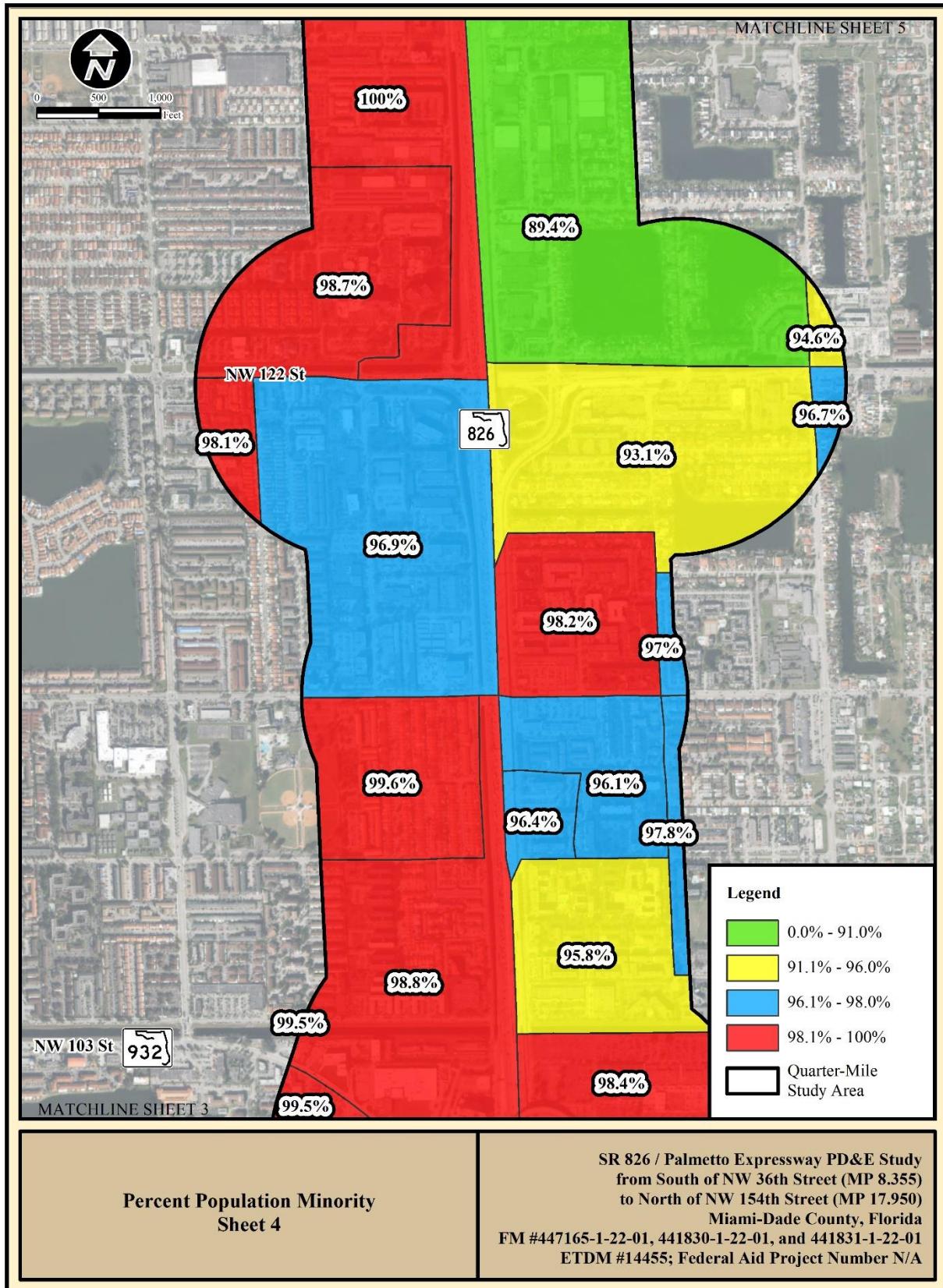
**Figure A-8: Percent Minority Map 2**



**Figure A-8: Percent Minority Map 3**



**Figure A-8: Percent Minority Map 4**



**Figure A-8: Percent Minority Map 5**

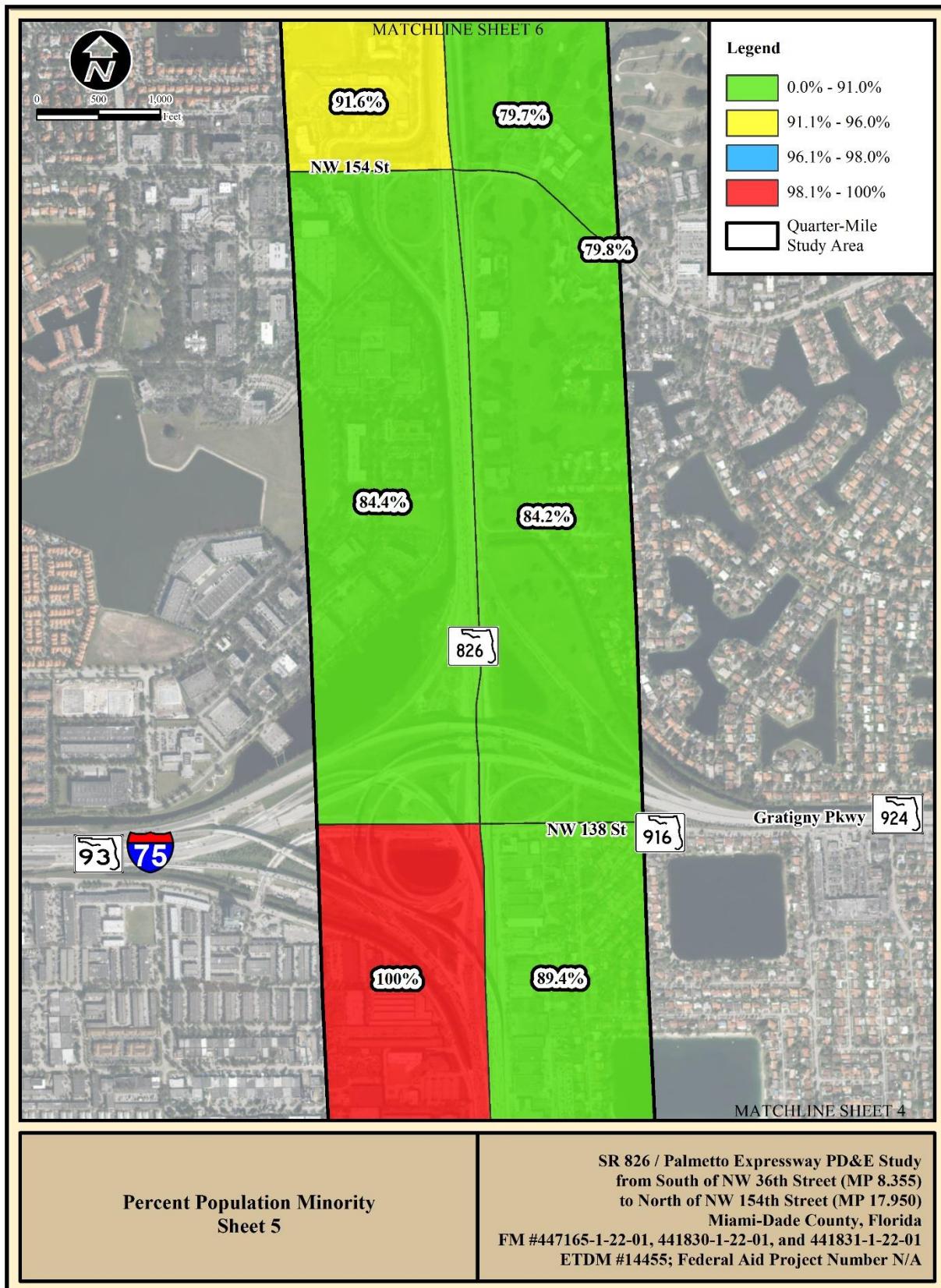
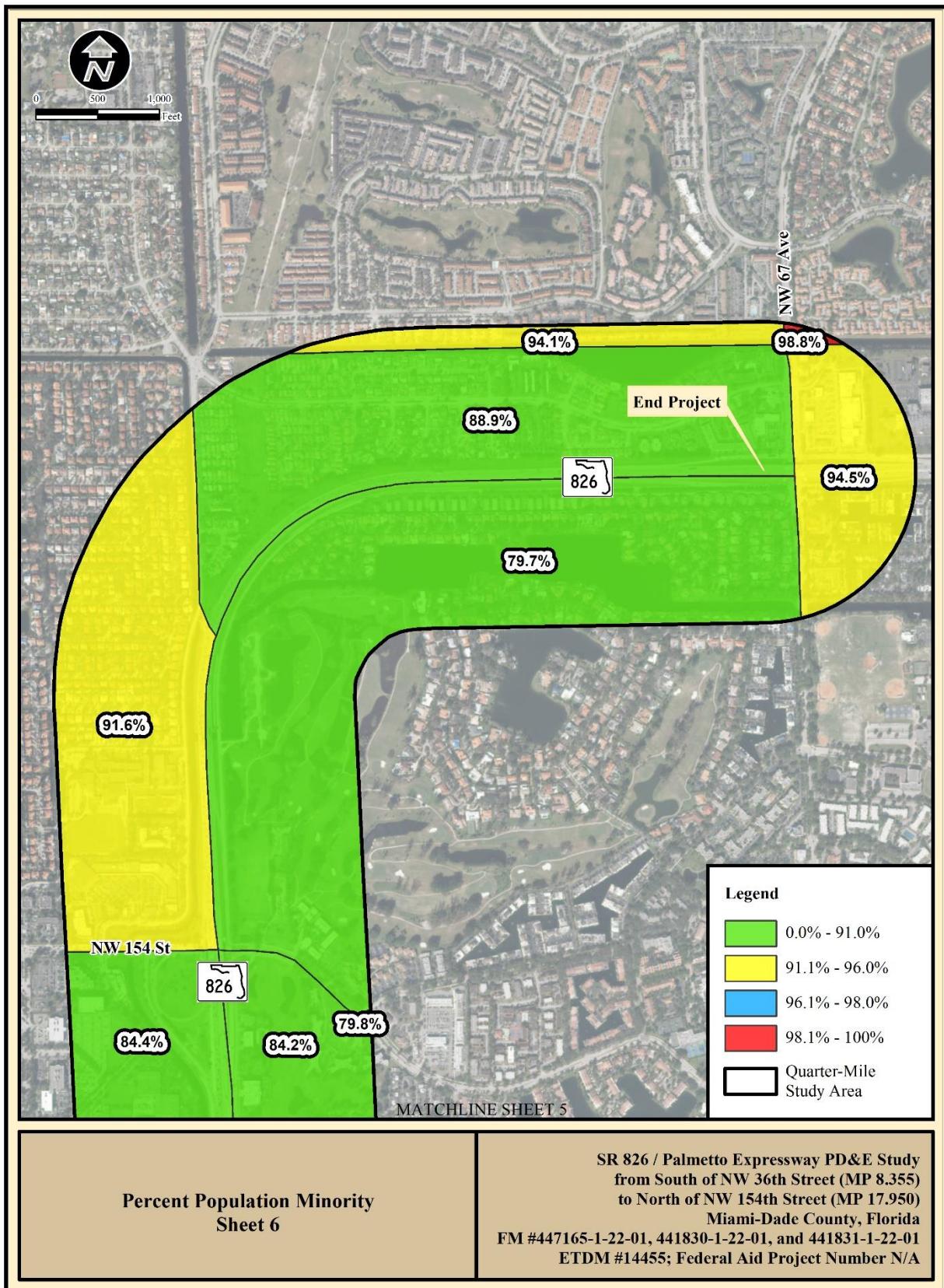


Figure A-8: Percent Minority Map 6



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## **Appendix B**

ETDM Summary Report



## ***Florida Department of Transportation***

**RON DESANTIS**  
**GOVERNOR**

605 Suwannee Street  
Tallahassee, FL 32399-0450

**KEVIN J. THIBAULT**  
**SECRETARY**

# **ETDM Summary Report**

**Project #14455 - SR 826/Palmetto Expressway**

**Preliminary Programming Screen - Published on 11/13/2020**

**Generated by Lauren Brooks (on behalf of FDOT District 6)**

**Printed on: 11/13/2020**

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## Introduction to Programming Screen Summary Report

The Programming Screen Summary Report shown below is a read-only version of information contained in the Programming Screen Summary Report generated by the ETDM Coordinator for the selected project after completion of the ETAT Programming Screen review. The purpose of the Programming Screen Summary Report is to summarize the results of the ETAT Programming Screen review of the project; provide details concerning agency comments about potential effects to natural, cultural, and community resources; and provide additional documentation of activities related to the Programming Phase for the project. Available information for a Programming Screen Summary Report includes:

- Screening Summary Report chart
- Project Description information (including a summary description of the project, a summary of public comments on the project, and community-desired features identified during public involvement activities)
- Purpose and Need information (including the Purpose and Need Statement and the results of agency reviews of the project Purpose and Need)
- Specific information regarding the potential transportation improvement such as alternatives or road segments that were reviewed; an overview of ETAT Programming Screen reviews; and agency comments concerning potential effects and degree of effect, by issue, to natural, cultural, and community resources
- Project Scope information, consisting of general project recommendations resulting from the ETAT Programming Screen review, permits, and technical studies required (if any)
- Class of Action determined for the project
- Dispute Resolution Activity Log (if any)

The legend for the Degree of Effect chart is provided in an appendix to the report.

For complete documentation of the project record, also see the GIS Analysis Results Report published on the same date as the Programming Screen Summary Report.

The Florida Department of Transportation may adopt this planning product into the environmental review process, pursuant to Title 23 Sec. USC 168(4)(d) or the state project development process.

## #14455 SR 826/Palmetto Expressway

**District:** District 6

**County:** Miami-Dade

**Planning Organization:** FDOT District 6

**Plan ID:** Not Available

**Federal Involvement:** Other Federal Permit

**Contact Information:** Dat Huynh (305) 470-5201 Dat.Huynh@dot.state.fl.us

**Snapshot Data From:** Programming Screen Summary Report Published on 11/13/2020 by Lauren Brooks

Topics and Categories are reflective of what was in place at the time of the screening event.

	Social and Economic							Cultural and Tribal	Natural				Physical							
	Social	Economic	Land Use Changes	Mobility	Aesthetic Effects	Relocation Potential	Farmlands		Historic and Archaeological Sites	Recreational and Protected Lands	Wetlands and Surface Waters	Water Resources	Floodplains	Protected Species and Habitat	Coastal and Marine	Noise	Air Quality	Contamination	Infrastructure	Navigation
Alternative #1 From: South of NW 25th Street To: North of NW 154th Street Published: 11/13/2020 Reviewed from 09/14/2020 to 10/29/2020)	2	2	2	1	2	2	N/A	N/A	2	2	2	2	2	2	2	2	3	3	0	2

---

## Purpose and Need

### Purpose and Need

The purpose of this project is to address various roadway deficiencies causing congestion and large speed differentials between general purpose (GP) lanes and express lanes (EL) along the SR 826/Palmetto Expressway corridor. Another goal of the project is to improve access to the EL system by relocating an ingress point in northern Miami-Dade County. The need for the project is based on the following criteria:

#### ROADWAY DEFICIENCIES: Address congestion and speed differentials

Following the opening of the SR 826/Palmetto Expressway ELs to traffic in September 2019, additional congestion and higher than expected speed differentials between EL (higher speeds) and GP lanes (lower speeds) were observed in both the NB and SB directions during peak travel times. Three specific areas were identified for improvement, including the NW 103rd Street Interchange, NW 74th Street to Okeechobee Road (SB direction), and NW 36th Street to NW 122nd Street (NB direction). The issues identified in these three areas are caused by geometric and operational deficiencies such as the lack of auxiliary lanes, insufficient ramp lengths, and substandard shoulders.

The implementation of operational improvements to correct the identified roadway deficiencies on SR 826/Palmetto Expressway would improve congestion and better align speeds between the EL and GP lanes. According to the Palmetto Express Lanes Modification Summary Report (**Attachment 1 in the EST**), an earlier planning study, the proposed improvements would result in a 56% and 42% reduction in travel times for the SB and NB GP lanes, respectively. In addition, throughput would increase a combined 58% and 11% in the SB and NB directions, respectively. While speed in the EL is not projected to increase, speeds in the GP lanes are forecasted to rise by 26 miles per hour in the SB direction and 19 miles per hour in the NB direction. These metrics illustrate how implementation of the potential improvements would address congestion and speed differentials currently caused by roadway deficiencies.

#### SYSTEM LINKAGE: Provide better access to the EL system

Currently the SB EL lanes begin north of NW 154th Street and do not provide SB ingress for motorists in this area of north-central Miami-Dade County (after NW 67th Avenue). To better facilitate the movement of traffic from this area of increased demand into the EL system, the potential project improvements include relocating the existing EL ingress point from north of NW 154th Street to south of NW 103rd Street. This will create access to the EL system for residents in this portion of Miami-Dade County and create an important linkage to the EL system.

### PROJECT STATUS

The proposed improvements on SR 826/Palmetto Expressway from South of NW 25th Street (MP 8.355) to North of NW 154th Street (MP 17.950) are included in the FDOT FY 2020 - FY 2024 Work Program with \$533,000 for Preliminary Engineering and \$643,500 for the Project Development and Environment (PD&E) Study in 2021 and \$6,029,121 for Design Build and Contract Incentives in 2024. The project is also included in the FDOT FY 2020 - FY 2024 State Transportation Improvement Program (STIP) with \$533,000 for Preliminary Engineering in 2021 and \$5,570,110 for Construction in 2023. The project is additionally included in the Miami-Dade Transportation Planning Organization's (TPO) FY 2020/2021 - FY 2024/2025 Transportation Improvement Program (TIP) with \$533,000 for Preliminary Engineering in 2020-21 and \$6,178,000 for Construction in 2023-24. The project is not included in the Miami-Dade TPO's 2045 Long Range Transportation Plan (LRTP) nor in the FDOT's Strategic Intermodal System (SIS) First Five-Year Plan. As the project advances into the PD&E phase, FDOT District Six will coordinate with the Miami-Dade TPO, Miami-Dade County, and FDOT Office of Environmental Management to ensure funding consistency among all plans.

## Project Description

This roadway project involves the potential addition of general use lanes, conversion of express (managed) lanes to general use lanes, as well as traffic operational and geometrical design improvements on SR 826/Palmetto Expressway from South of NW 25th Street (Milepost (MP) 8.355) to North of NW 154th Street (MP 17.950) within Miami-Dade County. The improvements are proposed to address existing congestion and higher than expected speed differentials between the general purpose (GP) lanes and the express lanes (EL) along the project corridor, as well as provide additional access to the EL system. Connecting population and commercial centers in north-central Miami-Dade County, the project traverses the municipalities of Doral, Medley, Hialeah Gardens, Hialeah, and Miami Lakes, as well as unincorporated portions of the county. SR 826/Palmetto Expressway connects to essential east-west facilities within north-central Miami-Dade County, including SR 836/Dolphin Expressway, SR 948/NW 36th Street, SR 934/NW 74th Street, US 27/Okeechobee Road, SR 932/NW 103rd Street, and I-75/SR 924.

The proposed southbound (SB) improvements include modification to the NW 103rd Street westbound (WB) to SB flyover ramp pier, thereby widening the SR 826/Palmetto Expressway mainline bridge and mechanically stabilized earth (MSE) walls, which will allow for an additional GP lane through the interchange and extension of the auxiliary lane between NW 74th Street and Okeechobee Road interchanges. In an effort to improve the existing weaving condition, three SB bridges (over NW 74th Street, Metrorail, and Florida East Coast (FEC) Rail) are expected to be widened. One SB EL will be converted to provide an additional GP lane between NW 103rd Street and NW 154th Street by milling and resurfacing, restriping, and moving the delineators. In addition, the existing SB EL ingress currently located at north of NW 154th Street is expected to be relocated to NW 103rd Street in order to provide better access to residents in northern Miami-Dade County. Proposed northbound (NB) improvements include widening the mainline within the NW 103rd Street interchange to provide the minimum 10-foot recommended shoulder width for both inside and outside shoulders. In addition, the project includes improvements to the frontage road system on both sides of SR 826/Palmetto Expressway including milling, resurfacing, and construction between Okeechobee Road and NW 122nd Street.

Within the project limits, SR 826/Palmetto Expressway is classified as a 'Principal Arterial-Other Freeways and Expressways' and consists of six to eight general use lanes and zero to four express (managed) lanes; the typical section varies throughout the project segment. In addition, SR 826/Palmetto Expressway is part of the state's emergency evacuation network and is a designated Strategic Intermodal System (SIS) highway corridor, providing access via SR 934/NW 74th Street (also known as Hialeah Expressway) and surface streets to the Miami Hialeah FEC Intermodal Terminal, an SIS Freight Rail Terminal. Existing right-of-way along the corridor ranges from approximately 235 feet to 455 feet or more in width near the existing interchanges. Right-of-way is not expected to be required to accommodate the proposed improvements. Specific right-of-way requirements will be determined during the PD&E Study, which is anticipated to be a State Environmental Impact Report.

## Summary of Public Comments

Summary of Public Comments is not available at this time.

## Planning Consistency Status

### Federal Consistency Determination

No federal consistency determination found.

## US Coast Guard Review

The USCG District Bridge Office for USCG District 7 has reviewed this project and provides the following determination(s):

### No bridges were found requiring permits.

These determinations remain valid unless the proposed project changes to include additional unassessed bridges or the project scope changes.

No further involvement from the USCG is required unless the proposed project changes to include additional unassessed

bridges or the project scope changes.

## Out of Jurisdiction, No Permit Required

Analysis Area	Water Crossing	Latitude	Longitude	Comments
Alternative #1	CANAL/DITCH AT PALMETTO EXPWY	25.812429	-80.321279	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT NW 77 CT	25.921385	-80.325385	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT NW58 ST TO NB SR-826	25.826991	-80.32073	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT SB SR826 TO NW 58 ST	25.827072	-80.32259	Not in USCG Jurisdiction.
Alternative #1	ARTIFICIALPATH AT NB SR 826 TO EB SR25 MIAMI CANAL	25.852888	-80.321526	Not in USCG Jurisdiction.
Alternative #1	ARTIFICIALPATH AT PALMETTO EXPWY MIAMI CANAL	25.853571	-80.322445	Not in USCG Jurisdiction.
Alternative #1	ARTIFICIALPATH AT SR 826/SR 25 FLYOVER MIAMI CANAL	25.854343	-80.323484	Not in USCG Jurisdiction.
Alternative #1	ARTIFICIALPATH AT SB SR826 FROM E SR25 MIAMI CANAL	25.854567	-80.323786	Not in USCG Jurisdiction.
Alternative #1	ARTIFICIALPATH AT NW 79 PL MIAMI CANAL	25.855612	-80.325191	Not in USCG Jurisdiction.
Alternative #1	ARTIFICIALPATH AT W 20TH AVENUE	25.869577	-80.322602	Not in USCG Jurisdiction.
Alternative #1	ARTIFICIALPATH AT PALMETTO EXPWY	25.869577	-80.322907	Not in USCG Jurisdiction.
Alternative #1	ARTIFICIALPATH AT W 20 AV / NW 77 CT	25.869577	-80.323518	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT PALMETTO EXPWY	25.881078	-80.323418	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT W 20TH AVENUE	25.881081	-80.323117	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT W 68 ST/NW 122 ST	25.884035	-80.324857	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT PALMETTO EXPWY	25.884312	-80.323571	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT W 19 CT	25.884319	-80.321901	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT W 20TH AVENUE	25.884323	-80.32451	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT PALMETTO EXPWY	25.904999	-80.324509	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT NB SR826 TO NW 154ST	25.905007	-80.324321	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT PALMETTO EXPWY	25.918473	-80.325121	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT NW 77 AVE	25.918484	-80.324816	Not in USCG Jurisdiction.
Alternative #1	CANAL/DITCH AT PALMETTO EXPWY	25.826965	-80.321688	Not in USCG Jurisdiction.

## Potential Lead Agencies

- FL Department of Transportation

## Exempted Agencies

Agency Name	Justification	Date
Federal Transit Administration	FTA has requested to be exempt from reviewing any non-transit projects.	06/16/2020

## Community Desired Features

No desired features have been entered into the database. This does not necessarily imply that none have been identified.

## User Defined Communities Within 500 Feet

No user defined communities were found within a 500 ft. buffer distance for this project.

## Census Places Within 500 Feet

- Country Club (CDP)
- Doral (city)
- Hialeah (city)
- Hialeah Gardens (city)
- Medley (town)
- Miami Lakes (town)

## Purpose and Need Reviews

### FDOT Office of Environmental Management

Acknowledgment	Date Reviewed	Reviewer	Comments
Understood	09/15/2020	Mike McDaniel (Michael.McDaniel@do	No Purpose and Need comments found.

**FL Department of Agriculture and Consumer Services**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/05/2020	Mark Kiser (Mark.Kiser@fdacs.gov)	No additional comments.

**FL Department of Economic Opportunity**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/29/2020	Matt Preston (matt.preston@deo.myflorida.com)	No Purpose and Need comments found.

**FL Department of Environmental Protection**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/27/2020	Chris Stahl (Chris.Stahl@FloridaDEP.gov)	No Purpose and Need comments found.

**FL Department of State**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	09/28/2020	Adrienne Daggett (Adrienne.Daggett@dos.myflorida.com)	No comments

**FL Fish and Wildlife Conservation Commission**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/19/2020	Jason Hight (Jason.Hight@MyFWC.com)	No Purpose and Need comments found.

**National Marine Fisheries Service**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/19/2020	Jennifer Schull (Jennifer.Schull@noaa.gov)	No Purpose and Need comments found.

**National Park Service**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/12/2020	Anita Barnett (anita_barnett@nps.gov)	No Purpose and Need comments found.

**South Florida Water Management District**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/19/2020	Trisha Stone (tstone@sfwmd.gov)	No Purpose and Need comments found.

**US Army Corps of Engineers**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/24/2020	Mark Tamblyn (Mark.M.Tamblyn@usace.army.mil)	No Purpose and Need comments found.

**US Coast Guard**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/13/2020	Marty Bridges	No Purpose and Need comments found.

(Martin.A.Bridges@usc  
g.mil)

### **US Environmental Protection Agency**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	10/21/2020	Amanetta Somerville (somerville.amanetta @epa.gov)	No Purpose and Need comments found.

### **US Fish and Wildlife Service**

<b>Acknowledgment</b>	<b>Date Reviewed</b>	<b>Reviewer</b>	<b>Comments</b>
Understood	09/16/2020	John Wrublik (john_wrublik@fws.go v)	none provided

The following organizations were notified but did not submit a review of the Purpose and Need:

- Miami-Dade Expressway Authority
- Natural Resources Conservation Service
- Seminole Tribe of Florida

## Alternative #1

### Description

Name	From	To	Type	Status	Total Length	Cost	Modes	SIS
Unnamed	South of NW 25th Street	North of NW 154th Street	Traffic Operation Enhancement	ETAT Review Complete	? mi.		Roadway	Y

### Project Effects Overview for Alternative #1

Topic	Degree of Effect	Organization	Date Reviewed
<b>Social and Economic</b>			
Social	2 Minimal	FDOT District 6	10/29/2020
Social	3 Moderate	US Environmental Protection Agency	10/23/2020
Mobility	1 Enhanced	FDOT District 6	10/29/2020
Land Use Changes	0 None	FL Department of Economic Opportunity	10/29/2020
Land Use Changes	2 Minimal	FDOT District 6	10/29/2020
Relocation Potential	2 Minimal	FDOT District 6	10/29/2020
Economic	0 None	FL Department of Economic Opportunity	10/29/2020
Economic	2 Minimal	FDOT District 6	10/29/2020
Aesthetic Effects	2 Minimal	FDOT District 6	10/29/2020
<b>Cultural and Tribal</b>			
Historic and Archaeological Sites	2 Minimal	FL Department of State	09/28/2020
Recreational and Protected Lands	0 None	FL Department of Environmental Protection	10/27/2020
Recreational and Protected Lands	N/A N/A / No Involvement	National Park Service	10/20/2020
Recreational and Protected Lands	0 None	South Florida Water Management District	10/19/2020
<b>Natural</b>			
Water Resources	2 Minimal	FL Department of Environmental Protection	10/27/2020
Water Resources	3 Moderate	US Environmental Protection Agency	10/27/2020
Water Resources	2 Minimal	South Florida Water Management District	10/26/2020
Wetlands and Surface Waters	2 Minimal	FL Department of Environmental Protection	10/27/2020
Wetlands and Surface Waters	3 Moderate	US Environmental Protection Agency	10/27/2020
Wetlands and Surface Waters	3 Moderate	US Army Corps of Engineers	10/24/2020
Wetlands and Surface Waters	2 Minimal	South Florida Water Management District	10/19/2020
Wetlands and Surface Waters	2 Minimal	National Marine Fisheries Service	10/19/2020
Wetlands and Surface Waters	2 Minimal	US Fish and Wildlife Service	09/16/2020
Protected Species and Habitat	2 Minimal	FL Fish and Wildlife Conservation Commission	10/19/2020

Protected Species and Habitat	2 Minimal	South Florida Water Management District	10/19/2020
Protected Species and Habitat	N/A N/A / No Involvement	FL Department of Agriculture and Consumer Services	10/05/2020
Protected Species and Habitat	2 Minimal	US Fish and Wildlife Service	09/16/2020
Coastal and Marine	0 None	South Florida Water Management District	10/19/2020
Coastal and Marine	2 Minimal	National Marine Fisheries Service	10/19/2020
Floodplains	2 Minimal	South Florida Water Management District	10/26/2020
<b>Physical</b>			
Contamination	2 Minimal	FL Department of Environmental Protection	10/27/2020
Contamination	3 Moderate	US Environmental Protection Agency	10/27/2020
Contamination	2 Minimal	South Florida Water Management District	10/19/2020
Air Quality	2 Minimal	US Environmental Protection Agency	10/23/2020
Navigation	2 Minimal	US Army Corps of Engineers	10/24/2020
Navigation	N/A N/A / No Involvement	US Coast Guard	10/13/2020
<b>Special Designations</b>			
Special Designations	3 Moderate	US Environmental Protection Agency	10/23/2020
Special Designations	2 Minimal	South Florida Water Management District	10/19/2020

## ETAT Reviews and Coordinator Summary: Social and Economic

### Social

#### Project Effects

**Coordinator Summary Degree of Effect:** 2 Minimal assigned 11/13/2020 by FDOT District 6

#### Comments:

SR 826/Palmetto Expressway traverses the U.S. Census Designated Places of Country Club, Doral, Hialeah, Hialeah Gardens, Medley, and Miami Lakes, as well as unincorporated areas of Miami-Dade County. The project area is composed primarily of industrial and commercial/retail/office uses along the southern portion of the corridor, with residential uses located north of West Okeechobee Road. Given that the extent of the project corridor is located within a dense urban area, several community features are present.

Compared to Miami-Dade County, the 500-foot project buffer contains notably higher White and Hispanic population percentages, a lower population percentage of African Americans, a slightly higher percentage of individuals age 65 and over, and a slightly lower percentage of housing units with no vehicle available. The median family income for the census block groups composing the buffer area is slightly higher than the median family income for Miami-Dade County. Limited English Proficiency (LEP) accommodations will likely be required during public involvement efforts of the Project Development phase.

USEPA identified a slightly higher minority population percentage than that of Miami-Dade County. USEPA noted that although the project should not require additional right-of-way, access to proximate residences and businesses may temporarily be affected or modified as a result of the proposed project. USEPA commented that right-of-way acquisition of land, homes, businesses, and other community features may affect quality of life. USEPA stated that since the project may result in adverse effects to minority and low-income populations along the project corridor, FDOT should consider proactive measures so that the affected communities, especially residents, are included and considered in project decisions and mitigation.

No further neighborhood division or social isolation is expected to occur as a result of the project. In the long term, the proposed project is intended to enhance the social environment and community cohesion by improving speed differentials between the general purpose lanes and express lanes, providing greater access to the express lane system for residents of Hialeah, and

increasing safety by providing the minimum 10-foot recommended shoulder widths for both the inside and outside shoulders. Additionally, the frontage road improvements will include continuous sidewalk accommodations. However, a Summary Degree of Effect of Minimal has been assigned to the Social topic given that access to proximate residences, businesses, and community features along the project corridor could temporarily be affected and/or modified as a result of project construction and potential visual impacts to surrounding communities could occur if elevated structure(s) are widened/expanded to accommodate the proposed improvements.

During the Project Development phase, FDOT District Six will engage the general public in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential project effects related to community cohesion and social interaction as well as potential solutions to ensure that both the social and transportation needs of the affected communities are addressed through the project. FDOT District Six will also conduct a Sociocultural Effects Evaluation and a Public Involvement Plan will be implemented in accordance with the FDOT PD&E Manual.

**Degree of Effect:** 2 Minimal assigned 10/29/2020 by Megan McKinney, FDOT District 6

**Coordination Document:**

PD&E Support Document As Per PD&E Manual

**Direct Effects**

**Identified Resources and Level of Importance:**

**500-Foot Buffer:**

**2010 Census Designated Places (6)**

- Miami Lakes
- Doral
- Medley
- Hialeah Gardens
- Hialeah
- Country Club

**Brownfields (4)**

- Sweetwater D
- City of Hialeah Brownfield Area
- Doral Decor District Brownfield Area
- NW 58th Street Corridor Green Reuse Area

**Enterprise Zones (1)**

- Miami-Dade County [EZ-1301]

**Homeowner and Condominium Associations (23)**

**Group Care Facilities (3)**

- Citrus Health Network - Adult Crisis Unit
- Sweet Paradise ALF
- Citrus Health Network - Children's Crisis Unit

**Community Centers (1)**

- After School All Stars South Florida

**Cultural Centers (2)**

- Miami Lakes Library
- Florida National College Library

**Fire Stations (1)**

- Miami-Dade County Fire Department and Rescue Station 1 (Miami Lakes)

**Government Buildings (2)**

- U.S. Post Office - Hialeah Annex
- Florida Department of Highway Safety and Motor Vehicles Service Center

**Hospitals (1)**

- Southern Winds Hospital

**Health Care Facilities (106)**

**Laser Facilities (4)**

- Aran and Holbrook Eye Associates
- Daniel Santoyo, DDS, PA
- Hialeah Wellness and Rehab
- N/A

**Religious Centers (6)**

- Church of Jesus Christ The Almighty
- Church of Christ Hialeah
- Orthodox Cathedral of Christ The Savior
- Bezerra De Menezes Kardec Spiritist Association
- Comunidad Servicios De Cristo
- Iglesia Cristiano Del Alma

**Schools (11)****Social Service Facilities (1)**

- Bezerra De Menezes Food Pantry

**Park and Recreational Facilities (2)**

- Miami Lakes Mini Park (P-50)
- Miami Lakes Mini Park (P-51)

**Office of Greenways and Trails (OGT): Hiking Trail Priorities (1)**

- Miami River Greenway

**Office of Greenways and Trails (OGT): Multi-Use Trail Opportunities (1)**

- Miami River Greenway

**Shared-Use Nonmotorized (SUN) Trail Network in Florida (1)**

- Miami River Greenway

**Golf Courses (1)**

- Don Shula's Golf Club

**Florida Site File Archaeological or Historic Sites (2)**

- NN
- Hialeah 1

**Florida Site File Historic Bridges (4)**

- FDOT Bridge #870257
- FDOT Bridge #870258
- FDOT Bridge #870569
- FDOT Bridge #870259

**Florida Site File Historic Standing Structures (41)****Florida Site File Resource Groups (11)****Florida Site File Field Survey Project Boundaries (36)****1,320-Foot (Quarter-Mile) Buffer:****2010 Census Designated Places (6)**

- Miami Lakes
- Doral
- Medley
- Hialeah Gardens
- Hialeah
- Country Club

**Brownfields (4)**

- Sweetwater D
- City of Hialeah Brownfield Area
- Doral Decor District Brownfield Area
- NW 58th Street Corridor Green Reuse Area

**Enterprise Zones (1)**

- Miami-Dade County [EZ-1301]

**Homeowner and Condominium Associations (67)****Assisted Housing Facilities (1)**

- Meadowgreen Apartments

**Group Care Facilities (8)**

- Golden Palm A.L.F. Inc.
- D P @ B Enterprise
- Citrus Health Network - Adult Crisis Unit
- Sweet Paradise ALF
- Ailin Living Facility
- Our Loving Mother
- New World ALF

- Citrus Health Network - Children's Crisis Unit

### **Community Centers (3)**

- After School All Stars South Florida
- Hialeah Fraternal Order of Police
- Wilde Community Center

### **Cultural Centers (6)**

- Museum of the Americas
- Jay I Kislak Foundation Inc.
- Miami Lakes Library
- Wilde E-Library
- Miami Dade College - Hialeah Campus Library
- Florida National College Library

### **Fire Stations (3)**

- Hialeah Fire Department and Rescue Station 8
- Miami-Dade County Fire Department and Rescue Station 1 (Miami Lakes)
- Miami Lakes West Fire Department Station 64

### **Government Buildings (4)**

- U.S. Post Office - Hialeah Annex
- U.S. Post Office - Miami Periodical Annex
- U.S. Post Office - Promenade
- Florida Department of Highway Safety and Motor Vehicles Service Center

### **Hospitals (2)**

- Southern Winds Hospital
- Palmetto General Hospital

### **Health Care Facilities (152)**

#### **Laser Facilities (8)**

- Aran and Holbrook Eye Associates
- N/A
- N/A
- Palmetto Eye and Laser Center
- Daniel Santoyo, DDS, PA
- Hialeah Wellness and Rehab
- Sunland Therapy Center
- N/A

#### **Law Enforcement Facilities (1)**

- Miami-Dade College Police Department - Hialeah Campus

### **Religious Centers (12)**

#### **Schools (17)**

#### **Social Service Facilities (2)**

- WIC Clinic Main
- Bezerra De Menezes Food Pantry

#### **Park and Recreational Facilities (12)**

##### **Office of Greenways and Trails (OGT): Hiking Trail Priorities (1)**

- Miami River Greenway

##### **Office of Greenways and Trails (OGT): Multi-Use Trail Opportunities (1)**

- Miami River Greenway

##### **Shared-Use Nonmotorized (SUN) Trail Network in Florida (1)**

- Miami River Greenway

#### **Golf Courses (1)**

- Don Shula's Golf Club

#### **Florida Site File Archaeological or Historic Sites (2)**

- NN
- Hialeah 1

#### **Florida Site File Historic Bridges (4)**

- FDOT Bridge #870257
- FDOT Bridge #870258
- FDOT Bridge #870569
- FDOT Bridge #870259

#### **Florida Site File Historic Standing Structures (46)**

**Florida Site File Resource Groups (11)****Florida Site File Field Survey Project Boundaries (44)****GENERALIZED LAND USE:**

- Acreage Not Zoned for Agriculture / 86.23 / 2.38%
- Agricultural / 12.23 / 0.34%
- Centrally Assessed / 7.55 / 0.21%
- Industrial / 879.52 / 24.15%
- Institutional / 86.50 / 2.38%
- Parcels with No Values / 3.97 / 0.07%
- Public/Semi-Public / 156.87 / 4.33%
- Right-of-Way / < 0.1 / 0.00%
- Recreation / 73.57 / 2.02%
- Residential / 597.64 / 16.39%
- Retail/Office / 548.35 / 15.10%
- Vacant Nonresidential / 120.24 / 3.29%
- Vacant Residential / 343.39 / 9.39%
- Water / 22.22 / 0.61%

**Comments on Effects to Resources:**

SR 826/Palmetto Expressway traverses the U.S. Census Designated Places of Country Club, Doral, Hialeah, Hialeah Gardens, Medley, and Miami Lakes, as well as unincorporated areas of Miami-Dade County. The project area is composed primarily of industrial and commercial/retail/office uses along the southern portion of the corridor, with residential uses located north of West Okeechobee Road.

The implementation of operational improvements to correct the identified roadway deficiencies on SR 826/Palmetto Expressway would improve congestion and better align speeds between the express lanes and general purpose lanes. This is expected to better accommodate future travel demand projected as a result of population and employment growth. In addition, the potential project improvements include relocating the existing express lane ingress point from north of NW 154th Street to south of NW 103rd Street. This will create access to the express lane system for residents in this portion of Miami-Dade County and create an important linkage to the EL system. Given that the extent of the project corridor is located within a dense urban area, several community features are present.

Compared to Miami-Dade County, the 500-foot project buffer contains notably higher White and Hispanic population percentages, a lower population percentage of African Americans, a slightly higher percentage of individuals age 65 and over, and a slightly lower percentage of housing units with no vehicle available. The median family income for the census block groups composing the buffer area is \$50,941; this is slightly higher than the median family income for Miami-Dade County as a whole (\$50,065).

The table below presents the demographic data for both the 500-foot project buffer and Miami-Dade County.

Demographic / 500-Foot Project Buffer / Miami-Dade County

White (Race)\* / 90.7% / 73.8%

African-American (Race)\* / 4.6% / 18.9%

Other\*\*\* (Race)\* / 4.7% / 7.3%

Hispanic (Ethnic Group)\* / 87.1% / 65.0%

Age 65+\*\* 16.4% / 14.1%

Under Age 18\*\* / 19.9% / 21.9%

Housing Units w/o Vehicle\*\* / 10.2% / 11.1%

Median Family Income\*\* / \$50,941 / \$50,065

\* Source: US Census Bureau (2010 US Census).

\*\* Source: US Census Bureau (2010 American Community Survey).

\*\*\* Other includes Asian, American Indian, Native Hawaiian & Other Pacific Islander Alone, Some Other Race, & Two or More Races.

**Minority Population Greater than 40%:**

88 out of the 345 census blocks within the 500-foot project buffer contain a minority population greater than 40%; a total of 15,621 individuals out of 16,627 individuals comprise the minority population of these 88 census blocks.

**Limited English Proficiency (LEP) Accommodations:**

Limited English Proficiency (LEP) accommodations will likely be required during public involvement efforts of the Project

Development phase as 44.26% or 23,028 persons within the census block groups containing the project corridor "speak English less than very well" compared to 35.52% of Miami-Dade County's population.

Existing neighborhoods are oriented so that community edges and boundaries are generally defined by major roadways, including SR 826. The proposed modification of lanes and structures within the existing corridor will not serve to divide or isolate an existing population or community.

The project is compatible with the Miami-Dade Comprehensive Development Master Plan (CDMP). Objective TE-2C of the Transportation Element notes that in road construction and reconstruction projects, the design shall promote pedestrian comfort where the Land Use Element seeks to provide activity along roadway frontages. Additionally, TE-2H states that comfortable and safe sidewalk conditions for pedestrians should be ensured by the County. The Build Alternative will do so by infilling sidewalk gaps along the east and west frontage roads along NW 77th Avenue and W 20th Avenue. Fulfilling these objectives should improve the quality of life of residents and visitors of Miami-Dade County.

In the long term, the proposed project is intended to enhance the social environment and community cohesion by improving speed differentials between the general purpose lanes and express lanes, providing greater access to the express lane system for residents of Hialeah, and increasing safety by providing the minimum 10-foot recommended shoulders widths for both the inside and outside shoulders. Additionally, the frontage road improvements will include continuous sidewalk accommodations.

No further neighborhood division or social isolation is expected to occur as a result of the project. However, impacts on the social environment are anticipated to be minimal as access to proximate residences, businesses, and community features along the project corridor could temporarily be affected and/or modified as a result of project construction and potential visual impacts to surrounding communities could occur if elevated structure(s) are widened/expanded to accommodate the proposed improvements.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

##### **Additional Comments (optional):**

##### **CLC Recommendations:**

During the Project Development phase, FDOT District Six will engage the general public in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential project effects related to community cohesion and social interaction as well as potential solutions to ensure that both the social and transportation needs of the affected communities are addressed through the project.

##### **Indirect Effects**

##### **Identified Resources and Level of Importance:**

##### **Comments on Effects to Resources:**

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 3 Moderate assigned 10/23/2020 by Amanetta Somerville, US Environmental Protection Agency

##### **Coordination Document:**

To Be Determined: Further Coordination Required

##### **Direct Effects**

##### **Identified Resources and Level of Importance:**

The Preliminary Environmental Discussion and the 2018 ACS data found in the sociocultural report identified that the demographic characteristics within the 500-foot buffer contain a slightly higher minority population (94%) than that of Miami-Dade County (86.5%). Additionally, the median family income of census block groups within the 500-foot project buffer is slightly higher than that of Miami-Dade County. The proposed widening and addition of access points should not require additional right-of-way within the project corridor. However, access to proximate residences and businesses may temporarily be affected or modified as a result of the proposed project. The EPA assigns a Moderate degree of effect on social impacts until further project development and analysis are available. The impact upon the minority and/or low-income populations along the project corridor will further determine the degree of effect.

##### **Comments on Effects to Resources:**

The proposed widening within the project corridor may impose adverse impacts on local communities within the project corridor. Partial and full acquisition of land, homes, business, and other community features may affect the quality of life. Environmental features and community elements help individuals maintain health and well-being. Executive Order 12898 (Environmental Justice) requires Federal agencies to take the appropriate steps to identify and address any disproportionately high and adverse human health or environmental effects of Federal programs, policies, and activities on minority and low-income populations.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

There is potential for adverse effects on minority and low-income populations, consider proactive measures so that the affected communities, especially residents, are included and considered in the decisions related to alternative selection, impact analysis, and mitigation.

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Mobility**

**Project Effects**

**Coordinator Summary Degree of Effect:** 1 Enhanced assigned 11/13/2020 by FDOT District 6

**Comments:**

SR 826/Palmetto Expressway supports vital regional and local commerce as it is part of Florida's Strategic Intermodal System (SIS) highway network and connects employment and population centers within Miami-Dade County. Additionally, the project corridor connects to I-75, another SIS corridor, as well as provides access via SR 934/NW 74th Street (also known as Hialeah Expressway) and surface streets to the Miami Hialeah FEC Intermodal Terminal, an SIS Freight Rail Terminal.

Per review of the 09-14-2020 Advance Notification Package, the Miami-Dade County Aviation Department (MDAD) reported that it does not object to the proposed project provided that all uses comply with federal, state and local aviation regulations and any temporary or permanent structure reaching or exceeding 45 feet above ground is reviewed by MDAD for impact determination and filed with the Federal Aviation Administration. The Miami-Dade County Department of Transportation and Public Works (DTPW) noted that Metrobus Route 175 runs along SR 826/Palmetto Expressway; DTPW recommends that FDOT evaluate alternatives to alleviate current route deficiencies. DTPW and the Miami-Dade Transportation Planning Organization also stated that FDOT should consider access from the express lanes to the Metrorail Palmetto Station to support the SMART Plan Bus Express Rapid Transit Network, enabling transit buses to access the express lanes coming from I-75 towards the Metrorail Palmetto Station; this would include the NW Miami-Dade Express Route currently in operations. DTPW stated that information about project activities on NW 103rd Street is not currently sufficient to determine impacts to Metrobus Routes 33 and 54 and the City of Hialeah Transit Marlins Route which run on that roadway; DTPW requested further coordination when more information is known in order to determine project impacts to these routes. DTPW further requested that FDOT coordinate with Miami-Dade County on any impacts to county-maintained roadways, removal or replacement of transit amenities or route realignments, and provide general purpose and express lane entry and exit plan changes for review. DTPW Highway Division noted two Transportation Improvement Program projects within the vicinity of this project. The Miami-Dade County Fire Rescue Department (MDFR) stated that SR 826/Palmetto Expressway is frequently used when responding to emergencies and recommends that FDOT consider expanding lane widths and providing emergency vehicle passing lanes to accommodate larger emergency response vehicles. PortMiami stated that the project promotes goals and objectives outlined in the PortMiami Master Plan Sub-element of the Miami-Dade Comprehensive Development Master Plan.

Overall, the project improvements are critical to enhancing regional mobility and key in preserving the operational integrity and regional functionality of the SR 826/Palmetto Expressway corridor as a whole. For these reasons, a Summary Degree of Effect of Enhanced has been assigned to the Mobility topic.

During the Project Development phase, FDOT District Six will conduct public outreach in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input from the general public (targeting special population groups) on potential project effects and enhancements regarding mobility within the area and measures to avoid or minimize possible adverse effects. FDOT District Six will also conduct a Sociocultural Effects Evaluation and a Public Involvement Plan will be implemented in accordance with the FDOT PD&E Manual.

**Degree of Effect:** 1 Enhanced assigned 10/29/2020 by Megan McKinney, FDOT District 6

**Coordination Document:**

PD&E Support Document As Per PD&E Manual

**Direct Effects**

**Identified Resources and Level of Importance:**

**500-Foot Buffer:**

**Bus Routes (15)**

**Fixed-Guideway Transit and Ferry Network (1)**

- Metrorail (Green Line)

**Railroads (Mainline)**

- 1,575 Feet

**FDOT RCI Bridges (66)**

**Facility Crossings (43)**

**Office of Greenways and Trails (OGT): Hiking Trail Priorities (1)**

- Miami River Greenway

**Office of Greenways and Trails (OGT): Multi-Use Trail Opportunities (1)**

- Miami River Greenway

**Shared-Use Nonmotorized (SUN) Trail Network in Florida (1)**

- Miami River Greenway

**Transportation Disadvantaged Service Provider Areas (TDSP) (1)**

- Miami-Dade Transit Authority

**Housing Units with No Vehicle Available (Total / Percentage)**

- 1,778 / 10.20%

**1,320-Foot (Quarter-Mile) Buffer:**

**Bus Routes (16)**

**Fixed-Guideway Transit Network Stations (1)**

- Palmetto

**Fixed-Guideway Transit and Ferry Network (1)**

- Metrorail (Green Line)

**Railroads (Mainline, Spur, & Siding)**

- 5,796 Feet

**Air Transportation Facility (1)**

- Palmetto General Hospital

**FDOT RCI Bridges (77)**

**Facility Crossings (50)**

**Office of Greenways and Trails (OGT): Hiking Trail Priorities (1)**

- Miami River Greenway

**Office of Greenways and Trails (OGT): Multi-Use Trail Opportunities (1)**

- Miami River Greenway

**Shared-Use Nonmotorized (SUN) Trail Network in Florida (1)**

- Miami River Greenway

**Transportation Disadvantaged Service Provider Areas (TDSP) (1)**

- Miami-Dade Transit Authority

**Comments on Effects to Resources:**

SR 826/Palmetto Expressway supports vital regional and local commerce as it is part of Florida's Strategic Intermodal System (SIS) highway network and connects employment and population centers within Miami-Dade County. Additionally, the project corridor connects to I-75, another SIS corridor, as well as provides access via SR 934/NW 74th Street (also known as Hialeah Expressway) and surface streets to the Miami Hialeah FEC Intermodal Terminal, an SIS Freight Rail Terminal.

The project is anticipated to address roadway deficiencies through operational improvements, reducing congestion by increasing throughput and decreasing travel times for the general purpose lanes. This is expected to better accommodate future travel demand projected as a result of population and employment growth. The reduced congestion is intended to better align speeds between the general purpose lanes and express lanes. The reduced delays on the corridor can also enhance evacuation capabilities as well as emergency access and incident response times [especially since SR 826/Palmetto Expressway serves as part of the emergency evacuation route network of the state].

Within the 1,320-foot project buffer, there are sixteen bus routes, 5,796 feet of railroad [mainline, spur, and siding], one air transportation facility, 77 FDOT RCI bridges, 50 facility crossings, and one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [also a part of the Shared-Use Nonmotorized (SUN) Trail Network]. The project is also within one transportation disadvantaged service provider area [Miami-Dade Transit Authority]. Of the bus routes, Route 175 operates within SR 826 and Route HI and Route 29 operate within the frontage road improvement limits. The 500-foot project buffer contains a slightly lower percentage of housing units with no vehicle available [10.20%] compared to Miami-Dade County as a whole [11.06%].

The Palmetto Metrorail Station is located adjacent to SR 826 and NW 77th Street. Metrorail is a dual track heavy rail system that operates throughout Miami-Dade County. The project is not expected to result in any changes to the Metrorail line or the Palmetto Station. The proposed southbound improvements include modification to the NW 103rd Street westbound to southbound flyover ramp pier, thereby widening the SR 826/Palmetto Expressway mainline bridge and mechanically stabilized earth (MSE) walls, which will allow for an additional general purpose lane through the interchange and extension of the auxiliary lane between NW 74th Street and Okeechobee Road interchanges. In an effort to improve the existing weaving condition, three southbound bridges (over NW 74th Street, Metrorail, and Florida East Coast (FEC) Rail) are expected to be widened. One southbound express lane will be converted to provide an additional general purpose lane between NW 103rd Street and NW 154th Street by milling and resurfacing, restriping, and moving the delineators. In addition, the existing southbound express lane ingress currently located at north of NW 154th Street is expected to be relocated to NW 103rd Street in order to provide better access to residents in northern Miami-Dade County. Proposed northbound improvements include widening the mainline within the NW 103rd Street interchange to provide the minimum 10-foot recommended shoulder width for both inside and outside shoulders. Pedestrian access to businesses along east and west frontage roads (NW 77th Avenue and W 20th Avenue) will also be enhanced through continuous sidewalk coverage within the frontage road project limits.

No official public parking for any residence or business is expected to be affected. It should be noted when looking at aerial imagery dated February 1, 2020, cars were parked on the W 20th Avenue frontage road north of W 44th Place in Hialeah, which is within FDOT right-of-way. Based on property appraiser records the parked cars appear to be residents or visitors of the Towers of Westland Condominium Complex. Coordination with the complex is advised.

Overall, the project improvements are critical to enhancing regional mobility and key in preserving the operational integrity and regional functionality of the SR 826/Palmetto Expressway corridor as a whole. Therefore, mobility is expected to be enhanced as a result of the project.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

#### **Additional Comments (optional):**

#### **CLC Recommendations:**

During the Project Development phase, FDOT District Six will conduct public outreach in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input from the general public (targeting special population groups) on potential project effects and enhancements regarding mobility within the area and measures to avoid or minimize possible adverse effects.

#### **Indirect Effects**

#### **Identified Resources and Level of Importance:**

#### **Comments on Effects to Resources:**

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

## Land Use Changes

### Project Effects

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### Comments:

This project is located within the U.S. Census Designated Places of Country Club, Doral, Hialeah, Hialeah Gardens, Medley, and Miami Lakes as well as unincorporated areas of Miami-Dade County. The project area primarily consists of industrial land uses followed by residential and retail/office. Industrial and commercial/retail/office uses are primarily located along the southern portion of the project corridor, with residential uses located north of West Okeechobee Road. Additionally, there are four brownfields [one of which is located within the Miami-Dade County Enterprise Zone (EZ-1301)], one private golf course, and 12 park and recreational facilities within the 1,320-foot project buffer. According to the adopted Miami-Dade County 2020 and 2030 Land Use Plan Map, the area surrounding the project corridor is expected to continue to support urban uses.

FDEO noted that the proposed project appears to be consistent with the comprehensive plans and compatible with the development goals of the surrounding communities as it will alleviate traffic congestion along the SR 826/Palmetto Expressway corridor making the surrounding areas more attractive to development. FDEO further identified specific components of the City of Hialeah Comprehensive Plan which relate to the project. FDEO listed the future land use designations surrounding the project and added that the project is not located within an Area of Critical State Concern or the Coastal High Hazard Area, nor does it encroach on any military installations. FDEO noted that the project is in close proximity to Bucky Dent Park in the City of Hialeah and Miami Lakes Optimist Park in the City of Miami Lakes. Per review of the 09-14-2020 Advance Notification Package, the Miami-Dade Transportation Planning Organization (TPO) provided updates regarding the project's consistency with the 2045 Long Range Transportation Plan (LRTP) and latest Transportation Improvement Program (TIP). The Miami-Dade TPO noted that FDOT needs to coordinate with the TPO as the project advances to ensure that required project and its funding are consistent with and identified in the LRTP and TIP.

The project is anticipated to accommodate existing and proposed development within the area and is consistent with local land use plans. Therefore, the project is not expected to affect the existing or future land use of the area. For these reasons, a Summary Degree of Effect of Minimal has been assigned to the Land Use Changes topic.

During the Project Development phase, FDOT District Six will engage the general public in coordination with the Miami-Dade TPO, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential project effects to the affected communities and work to identify potential solutions. FDOT District Six will also coordinate with the FDOT Office of Environmental Management, Miami-Dade TPO, Miami-Dade County, and the municipalities associated with the project limits, to ensure that required project funding is identified in the LRTP, TIP, STIP, and FDOT Work Program and that the project is consistent with the local government comprehensive plan(s). FDOT District Six will also conduct a Sociocultural Effects Evaluation and a Public Involvement Plan will be implemented in accordance with the FDOT PD&E Manual.

**Degree of Effect:** 0 *None* assigned 10/29/2020 by Matt Preston, FL Department of Economic Opportunity

#### Coordination Document:

No Involvement

#### Direct Effects

##### Identified Resources and Level of Importance:

Comprehensive Plan(s) Reviewed:

*City of Doral Comprehensive Plan*; dated August 2016.

*City of Hialeah Comprehensive Plan 2015-2025*; dated October 10, 2017.

*City of Hialeah Gardens 2025 Comprehensive Plan*; adopted on May 1, 2007.

*Town of Medley Comprehensive Plan*; adopted in 2017.

*Town of Miami Lakes Comprehensive Plan*; adopted on December 19, 2003.

#### Comments on Effects to Resources:

##### Compatibility with Community Development Goals and Comprehensive Plan:

The proposed project appears to be compatible with community development goals. The subject local governments tout their strategic location in regard to access to major highways, such as SR 826/Palmetto Expressway, I-75, the Florida Turnpike, and to SR 924/Gratigny Parkway. Specifically, the project proposes to benefit community development by alleviating traffic congestion along the SR 826/Palmetto Expressway corridor.

The proposed project is in a very early stage of the Project Development. DEO staff did not identify local government strategies contrary to the objectives of the proposed project. The proposed project appears to be consistent with the City of Doral, City of Hialeah, City of Hialeah Gardens, Town of Medley and the Town of Miami Lakes comprehensive plans. These local governments strategically rely on its connectivity to major roads, including the SR 826/Palmetto Expressway. However, the project is currently under review by County staff and comments by Miami-Dade County will be sent directly to FDOT.

It appears that the City of Hialeah Comprehensive Plan makes no explicit or implicit reference in support of or opposition to this project. However, Policy 1.1.5 includes some elements included in the proposed project and Policy 1.6.3 states: *Proposed transportation improvements shall be consistent with the land use patterns on the Future Land Use Map.*

The surrounding land designations are consistent with the proposed project area, which is an area already developed.

**Future Transportation Map:**

It does not appear that the proposed project is included on any of the applicable Future Transportation Maps.

**Land Uses:**

The future land use designations adjacent to the proposed project are predominately Commercial and Industrial.

City of Doral: Industrial and Business.

City of Hialeah FLUM: Commercial; Residential (high and medium density); Major Institutions; and, Industrial.

City of Hialeah Gardens: Commercial Business; General Business; and, Industrial.

Town of Medley: Industrial District, Light Manufacturing (M-1); Industrial; and, Office.

Town of Miami Lakes FLUM: Low Density Residential (LD); Business and Office (BO); Park and Recreation; Industrial and Office; and, Office and Residential.

**Parks:**

The proposed project is located in close proximity to Bucky Dent Park in the City of Hialeah, and Miami Lakes Optimist Park in the City of Miami Lakes.

**Area of Critical State Concern (ACSC), Coastal High Hazard Area (CHHA), and Military Bases:**

The project is not located within an Area of Critical State Concern or the CHHA, nor does it encroach on any military installation.

**Other Planning-Related Items:**

N/A.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 Minimal assigned 10/29/2020 by Megan McKinney, FDOT District 6

**Coordination Document:**

PD&E Support Document As Per PD&E Manual

**Direct Effects**

**Identified Resources and Level of Importance:**

Miami-Dade County Comprehensive Development Master Plan

**500-Foot Buffer:**

**2010 Census Designated Places (6)**

- Miami Lakes
- Doral

- Medley
- Hialeah Gardens
- Hialeah
- Country Club

**Brownfields (4)**

- Sweetwater D
- City of Hialeah Brownfield Area
- Doral Decor District Brownfield Area
- NW 58th Street Corridor Green Reuse Area

**Enterprise Zones (1)**

- Miami-Dade County [EZ-1301]

**Park and Recreational Facilities (2)**

- Miami Lakes Mini Park (P-50)
- Miami Lakes Mini Park (P-51)

**Golf Courses (1)**

- Don Shula's Golf Club

**GENERALIZED LAND USE:**

- Acreage Not Zoned for Agriculture / 26.77 / 1.75%
- Centrally Assessed / 2.72 / 0.18%
- Industrial / 305.91 / 19.98%
- Institutional / 37.76 / 2.48%
- Public/Semi-Public / 43.36 / 2.81%
- Right-of-Way / < 0.1 / 0.00%
- Recreation / 18.11 / 1.20%
- Residential / 159.39 / 10.41%
- Retail/Office / 253.57 / 16.53%
- Vacant Nonresidential / 28.88 / 1.89%
- Vacant Residential / 105.90 / 6.89%
- Water / 7.81 / 0.51%

**1,320-Foot (Quarter-Mile) Buffer:****2010 Census Designated Places (6)**

- Miami Lakes
- Doral
- Medley
- Hialeah Gardens
- Hialeah
- Country Club

**Brownfields (4)**

- Sweetwater D
- City of Hialeah Brownfield Area
- Doral Decor District Brownfield Area
- NW 58th Street Corridor Green Reuse Area

**Enterprise Zones (1)**

- Miami-Dade County [EZ-1301]

**Park and Recreational Facilities (12)****Golf Courses (1)**

- Don Shula's Golf Club

**GENERALIZED LAND USE:**

- Acreage Not Zoned for Agriculture / 86.23 / 2.38%
- Agricultural / 12.23 / 0.34%
- Centrally Assessed / 7.55 / 0.21%
- Industrial / 879.52 / 24.15%
- Institutional / 86.50 / 2.38%
- Parcels with No Values / 3.97 / 0.07%
- Public/Semi-Public / 156.87 / 4.33%
- Right-of-Way / < 0.1 / 0.00%
- Recreation / 73.57 / 2.02%
- Residential / 597.64 / 16.39%

- Retail/Office / 548.35 / 15.10%
- Vacant Nonresidential / 120.24 / 3.29%
- Vacant Residential / 343.39 / 9.39%
- Water / 22.22 / 0.61%

#### **Comments on Effects to Resources:**

This project is located within the U.S. Census Designated Places of Country Club, Doral, Hialeah, Hialeah Gardens, Medley, and Miami Lakes as well as unincorporated areas of Miami-Dade County. The project area primarily consists of industrial land uses followed by residential, and retail/office. Industrial and commercial/retail/office uses are primarily located along the southern portion of the project corridor, with residential uses located north of West Okeechobee Road. Additionally, there are four brownfields [one of which is located within the Miami-Dade County Enterprise Zone (EZ-1301)], one private golf course [Don Shula's Golf Club], and 12 park and recreational facilities within the 1,320-foot project buffer. According to the adopted Miami-Dade County 2020 and 2030 Land Use Plan Map, the area surrounding the project corridor is expected to continue to support urban uses.

#### *Transportation Plan Consistency:*

The proposed improvements on SR 826/Palmetto Expressway from South of NW 25th Street (MP 8.355) to North of NW 154th Street (MP 17.950) are included in the FDOT FY 2020 - FY 2024 Work Program with \$533,000 for Preliminary Engineering and \$643,500 for the Project Development and Environment (PD&E) Study in 2021 and \$6,029,121 for Design Build and Contract Incentives in 2024. The project is also included in the FDOT FY 2020 - FY 2024 State Transportation Improvement Program (STIP) with \$533,000 for Preliminary Engineering in 2021 and \$5,570,110 for Construction in 2023. The project is additionally included in the Miami-Dade Transportation Planning Organization's (TPO) FY 2020/2021 - FY 2024/2025 Transportation Improvement Program (TIP) with \$533,000 for Preliminary Engineering in 2020-21 and \$6,178,000 for Construction in 2023-24. The project is not included in the Miami-Dade TPO's 2045 Long Range Transportation Plan (LRTP) nor in the FDOT's Strategic Intermodal System (SIS) First Five-Year Plan. As the project advances into the PD&E phase, FDOT District Six will coordinate with the Miami-Dade TPO, Miami-Dade County, and FDOT Office of Environmental Management to ensure funding consistency among all plans. While the project is not specifically identified on *Figure 1 - Planned Year 2030 Roadway Network* map, it is consistent with the objectives of the Miami-Dade Comprehensive Development Master Plan (CDMP).

Occurring within the Miami-Dade urban growth boundary, SR 826, a limited access expressway, is consistent with the character and intensity of surrounding development. The project is anticipated to support existing and proposed development within the area and is consistent with local land use plans. Therefore, the project is not expected to affect the existing or future land use of the area. Based on the foregoing, minimal impacts or changes to proximate land uses are anticipated as a result of the project.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

#### **Additional Comments (optional):**

#### **CLC Recommendations:**

During the Project Development phase, FDOT District Six will engage the general public in coordination with Miami-Dade Transportation Planning Organization (TPO), Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential project effects to the affected communities and work to identify potential solutions. FDOT District Six will also coordinate with the FDOT Office of Environmental Management, Miami-Dade TPO, Miami-Dade County, and the municipalities associated with the project limits, to ensure that the project is consistent with the local government comprehensive plan(s) and that required project funding is identified in the LRTP, TIP, STIP, and FDOT Work Program.

#### **Indirect Effects**

#### **Identified Resources and Level of Importance:**

#### **Comments on Effects to Resources:**

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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## **Farmlands**

### **Project Effects**

**Coordinator Summary Degree of Effect:** N/A / No Involvement assigned 11/13/2020 by FDOT District 6

#### **Comments:**

There are no farmlands reported within the 500-foot project buffer. According to the Miami-Dade County 2020 and 2030 Land Use Plan Map, the project area is anticipated to continue to support urban uses. Lands within the project vicinity do not meet the definition of farmland as defined in 7 CFR 658, and the provisions of the Farmland Protection Policy Act of 1981 do not apply because the entire project area is located in the Miami Urbanized Area with no designated farmlands adjacent to the project corridor. For these reasons, a Summary Degree of Effect of N/A / No Involvement has been assigned to the Farmlands topic.

None found

The following organization(s) were expected to but did not submit a review of the Farmlands topic for **Alternative #1: Natural Resources Conservation Service**

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## **Relocation Potential**

### **Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

The project area primarily consists of industrial and commercial/retail/office uses with residential uses located north of West Okeechobee Road. Numerous businesses and residences abut the project corridor. Existing right-of-way along the corridor ranges from approximately 235 feet to 455 feet or more in width near existing interchanges. Additional right-of-way is not expected to be required nor are potential relocations to accommodate the proposed improvements; however, access to proximate residences and businesses may temporarily be affected and/or modified as a result of the project. Encroachment into surrounding parcels, if necessary, will be coordinated with the appropriate property owners. The proposed project improvements will be designed to minimize right-of-way acquisition to the greatest extent practicable. Specific right-of-way requirements will be determined during the Project Development and Environment Study. For these reasons, a Summary Degree of Effect of Minimal has been assigned to the Relocation Potential topic.

During the Project Development phase, FDOT District Six will engage appropriate property owners in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential project effects to adjacent properties and identify alternatives that may minimize impacts. Further assessment of relocation effects will be conducted during the Project Development phase as more detailed and finalized project information regarding right-of-way needs becomes available. FDOT District Six will also conduct a Sociocultural Effects Evaluation and a Public Involvement Plan will be implemented in accordance with the FDOT PD&E Manual.

**Degree of Effect:** 2 *Minimal* assigned 10/29/2020 by Megan McKinney, FDOT District 6

### **Coordination Document:**

PD&E Support Document As Per PD&E Manual

### **Direct Effects**

#### **Identified Resources and Level of Importance:**

##### **500-Foot Buffer:**

##### **Homeowner and Condominium Associations (23)**

##### **Assisted Housing Facilities (18)**

##### **Group Care Facilities (3)**

- Citrus Health Network - Adult Crisis Unit

- Sweet Paradise ALF

- Citrus Health Network - Children Crisis Unit

##### **Park and Recreational Facilities (2)**

- Miami Lakes Mini Park (P-50)

- Miami Lakes Mini Park (P-51)

##### **Office of Greenways and Trails (OGT): Hiking Trail Priorities (1)**

- Miami River Greenway

##### **Office of Greenways and Trails (OGT): Multi-Use Trail Opportunities (1)**

- Miami River Greenway

##### **Shared-Use Nonmotorized (SUN) Trail Network in Florida (1)**

- Miami River Greenway

#### **Florida Site File Archaeological or Historic Sites (2)**

- NN

- Hialeah 1

#### **Florida Site File Historic Bridges (4)**

- FDOT Bridge #870257
- FDOT Bridge #870258
- FDOT Bridge #870569
- FDOT Bridge #870259

#### **Florida Site File Historic Standing Structures (41)**

#### **Florida Site File Resource Groups (11)**

#### **Residential Areas / Acres / Percent**

- Fixed Single Family Units (Less Than 2 Dwelling Units per Acre) / < 0.1 / 0.00%
- Fixed Single Family Units (2 to 5 Dwelling Units per Acre) / 81.15 / 5.30%
- Fixed Single Family Units (6 or More Dwelling Units per Acre) / 7.63 / 0.50%
- Multiple Dwelling Units, Low Rise (2 Stories or Less) / 74.89 / 4.90%
- Multiple Dwelling Units, Low Rise (3 Stories or More) / 39.37 / 2.58%

#### **Comments on Effects to Resources:**

The project area primarily consists of industrial and commercial/retail/office uses with residential uses located north of West Okeechobee Road. Numerous businesses and residences abut the project corridor. Existing right-of-way along the corridor ranges from approximately 235 feet to 455 feet or more in width near existing interchanges. Additional right-of-way is not expected to be required nor are potential relocations to accommodate the proposed improvements; however, access to proximate residences and businesses may temporarily be affected and/or modified as a result of the project. Encroachment into surrounding parcels, if necessary, will be coordinated with the appropriate property owners. The proposed project improvements will be designed to minimize right-of-way acquisition to the greatest extent practicable. Specific right-of-way requirements will be determined during the Project Development and Environment Study. Based on the foregoing, minimal involvement regarding relocation potential is anticipated.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

#### **Additional Comments (optional):**

#### **CLC Recommendations:**

During the Project Development phase, FDOT District Six will engage appropriate property owners in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential project effects to adjacent properties and identify alternatives that may minimize impacts. Further assessment of relocation effects will be conducted during the Project Development phase as more detailed and finalized project information regarding right-of-way needs becomes available.

#### **Indirect Effects**

#### **Identified Resources and Level of Importance:**

#### **Comments on Effects to Resources:**

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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#### **Economic**

#### **Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

SR 826 / Palmetto Expressway from South of NW 25th Street to North of NW 154th Street facilitates the north-south movement of local and regional traffic in northeastern Miami-Dade County. SR 826/Palmetto Expressway supports vital regional and local

commerce as it is part of Florida's Strategic Intermodal System (SIS) highway network and connects employment and population centers within Miami-Dade County. The project corridor connects to I-75, another SIS corridor, as well as provides access via SR 934/NW 74th Street (also known as Hialeah Expressway) and surface streets to the Miami Hialeah FEC Intermodal Terminal, an SIS Freight Rail Terminal. Additionally, the project connects to other major transportation facilities, including SR 948, US 27, and SR 932. Major employers along the project corridor include The Westland Mall, Palmetto General Hospital, as well as multiple industrial and freight warehouses, construction companies, and retailers located between NW 25th Street and SR 932.

FDEO noted that the project is not located within a Rural Area of Opportunity and commented that temporary jobs during construction could be generated as a result of the project.

The proposed project is expected to enhance economic conditions of the area in the long term by better facilitating the movement of local and regional freight and commuter traffic by maintaining critical links to major transportation facilities [I-75, and SR 934/NW 74th Street (also known as Hialeah Expressway)]. However, access to proximate businesses along the corridor may temporarily be affected and/or modified during project construction. For these reasons, a Summary Degree of Effect of Minimal has been assigned to the Economic topic.

During the Project Development phase, FDOT District Six will engage the general public in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential economic enhancements/impacts as a result of the project as well as potential solutions. FDOT District Six will also conduct a Sociocultural Effects Evaluation and a Public Involvement Plan will be implemented in accordance with the FDOT PD&E Manual.

**Degree of Effect:** 0 None assigned 10/29/2020 by Matt Preston, FL Department of Economic Opportunity

**Coordination Document:**

No Involvement

**Direct Effects**

**Identified Resources and Level of Importance:**

Comprehensive Plan(s) Reviewed:

*City of Doral Comprehensive Plan*; dated August 2016.

*City of Hialeah Comprehensive Plan 2015-2025*; dated October 10, 2017.

*City of Hialeah Gardens 2025 Comprehensive Plan*; adopted on May 1, 2007.

*Town of Medley Comprehensive Plan*; adopted in 2017.

*Town of Miami Lakes Comprehensive Plan*; adopted on December 19, 2003.

**Comments on Effects to Resources:**

The project is not located within a Rural Area of Opportunity.

Temporary jobs during the construction phase could possibly be generated. However, the proposed project will not necessarily create new jobs; an analysis of net job creation will be necessary to determine the impact.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 Minimal assigned 10/29/2020 by Megan McKinney, FDOT District 6

**Coordination Document:**

**Direct Effects**

**Identified Resources and Level of Importance:**

**500-Foot Buffer:**

**2010 Census Designated Places (6)**

- Miami Lakes
- Doral
- Medley
- Hialeah Gardens
- Hialeah
- Country Club

**Brownfields (4)**

- Sweetwater D
- City of Hialeah Brownfield Area
- Doral Decor District Brownfield Area
- NW 58th Street Corridor Green Reuse Area

**Enterprise Zones (1)**

- Miami-Dade County [EZ-1301]

**Government Buildings (2)**

- U.S. Post Office - Hialeah Annex
- Florida Department of Highway Safety and Motor Vehicles Service Center

**Hospitals (1)**

- Southern Winds Hospital

**Health Care Facilities (106)**

**1,320-Foot (Quarter-Mile) Buffer:**

**2010 Census Designated Places (6)**

- Miami Lakes
- Doral
- Medley
- Hialeah Gardens
- Hialeah
- Country Club

**Brownfields (4)**

- Sweetwater D
- City of Hialeah Brownfield Area
- Doral Decor District Brownfield Area
- NW 58th Street Corridor Green Reuse Area

**Enterprise Zones (1)**

- Miami-Dade County [EZ-1301]

**Government Buildings (4)**

- U.S. Post Office - Hialeah Annex
- U.S. Post Office - Miami Periodical Annex
- U.S. Post Office - Promenade
- Florida Department of Highway Safety and Motor Vehicles Service Center

**Hospitals (2)**

- Southern Winds Hospital
- Palmetto General Hospital

**Health Care Facilities (152)**

**GENERALIZED LAND USE:**

- Acreage Not Zoned for Agriculture / 86.23 / 2.38%
- Agricultural / 12.23 / 0.34%
- Centrally Assessed / 7.55 / 0.21%
- Industrial / 879.52 / 24.15%
- Institutional / 86.50 / 2.38%
- Parcels with No Values / 3.97 / 0.07%
- Public/Semi-Public / 156.87 / 4.33%

- Right-of-Way / < 0.1 / 0.00%
- Recreation / 73.57 / 2.02%
- Residential / 597.64 / 16.39%
- Retail/Office / 548.35 / 15.10%
- Vacant Nonresidential / 120.24 / 3.29%
- Vacant Residential / 343.39 / 9.39%
- Water / 22.22 / 0.61%

**Comments on Effects to Resources:**

SR 826 / Palmetto Expressway from South of NW 25th Street (MP 8.355) to North of NW 154th Street (MP 17.950) facilitates the north-south movement of local and regional traffic in northeastern Miami-Dade County. The project corridor traverses the U.S. Census Designated Places of Country Club, Doral, Hialeah, Hialeah Gardens, Medley, and Miami Lakes, as well as unincorporated areas of Miami-Dade County.

SR 826/Palmetto Expressway supports vital regional and local commerce as it is part of Florida's Strategic Intermodal System (SIS) highway network and connects employment and population centers within Miami-Dade County. The project corridor connects to I-75, another SIS corridor, as well as provides access via SR 934/NW 74th Street (also known as Hialeah Expressway) and surface streets to the Miami Hialeah FEC Intermodal Terminal, an SIS Freight Rail Terminal. Additionally, the project connects to other major transportation facilities, including SR 948, US 27, and SR 932.

According to the Miami-Dade Transportation Planning Organization's (TPO) 2045 Long Range Transportation Plan (LRTP), Miami-Dade County's population is projected to increase by 34.0% from 2.58 million persons in 2015 to 3.5 million persons in 2045; employment is expected to increase by 38.0% from 1.3 million employees in 2015 to 1.8 million employees in 2045. Traversing two of the seven transportation planning areas (Central and Northwest) identified in the LRTP, the greatest employment growth out of all seven is expected to occur within the Northwest transportation planning area. This area expects an employment increase of 44.1 percent and a population increase of 27.1 percent. The Central transportation planning area expects a lower rate of population and employment growth (32.7% and 36.6% respectively) when compared to the countywide averages. The projected growth in the area will result in a significant increase in travel demand and further deteriorate the conditions on the already congested SR 826 corridor.

Based on figures produced by the US Census Bureau reported in the Longitudinal Employer-Household Dynamics database, the quarter-mile area surrounding the corridor currently supports 33,826 jobs. Health care and social assistance; retail trade; wholesale trade; and administration and support, waste management and remediation are the predominant sectors of employment along the corridor. Multiple Industrial and freight warehouses as well as construction companies and retailers are located between NW 25th Street and SR 932. These include Dolphin Carpet and Tile, Petro Solutions, and FPE Forklift Parts and Equipment. The Westland Mall is adjacent to SR 826 and SR 932, with Palmetto General Hospital located at the corner of SR 826 and NW 122nd Street. Nearly two thirds of the jobs present earn less than \$3,333 dollars per month and 86.1 percent of daily work trips made by residents are made using an automobile.

Because the project does not require additional right-of-way, no loss in tax revenue by Miami-Dade County is expected. Local businesses and neighborhoods will experience improved operational conditions resulting in a reduction in travel times, which may increase property values over time. The proposed project is expected to enhance economic conditions of the area in the long term by better facilitating the movement of local and regional freight and commuter traffic by maintaining critical links to major transportation facilities [I-75, and SR 934/NW 74th Street (also known as Hialeah Expressway)]. However, minimal economic impacts are anticipated due to the fact that access to proximate businesses along the corridor may temporarily be affected and/or modified during project construction.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):**

**CLC Recommendations:**

During the Project Development phase, FDOT District Six will engage the general public in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential economic enhancements/impacts as a result of the project as well as potential solutions.

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

## **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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### **Aesthetic Effects**

#### **Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

SR 826/Palmetto Expressway traverses the U.S. Census Designated Places of Country Club, Doral, Hialeah, Hialeah Gardens, Medley, and Miami Lakes as well as unincorporated areas of Miami-Dade County. The project area is composed primarily of industrial and commercial/retail/office uses along the southern portion of the corridor, with residential uses located north of West Okeechobee Road. Specific community features associated with aesthetics reported within the 1,320-foot project buffer include four brownfields, 67 homeowner and condominium associations, one assisted housing facility, eight group care facilities, 12 park and recreational facilities, one golf course, one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [also part of the Shared-Use Nonmotorized (SUN) Trail Network], and cultural resources.

Per review of the 09-14-2020 Advance Notification Package, the Miami-Dade County Department of Regulatory and Economic Resources - Division of Environmental Resources Management (DERM) noted that the project area is subject to the preservation of specimen tree resources (trees with a trunk diameter equal to or greater than 18 inches) per Miami-Dade County Code. DERM recommends that FDOT preserve the specimen trees to the greatest extent feasible and replace any tree resources impacted by the project. DERM stated that if the proposed project affects any arterial roadways and rights-of-ways that are located outside of the State Highway System or its appurtenances, a Miami-Dade County Tree Removal Permit is required prior to the removal and/or relocation of any tree that is subject to the tree preservation and protection provisions within Miami-Dade County Code.

While elevated structure(s) could be widened/expanded to accommodate the proposed project improvements, impacts to views and vistas within the surrounding area are anticipated to be limited. In addition, proximate land uses along the corridor are not expected to change; as such, the project appears to be consistent with the future land use vision and aesthetic character of the corridor. Context sensitive solutions will also be considered to ensure the project matches local aesthetics and accounts for the community's input on design preferences. Based on the foregoing, a Summary Degree of Effect of Minimal has been assigned to the Aesthetic Effects topic.

During the Project Development phase, FDOT District Six will engage the surrounding communities in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential project effects as well as opinions and preferences regarding general design concepts related to aesthetics. FDOT District Six will also conduct a Sociocultural Effects Evaluation and a Public Involvement Plan will be implemented in accordance with the FDOT PD&E Manual.

**Degree of Effect:** 2 *Minimal* assigned 10/29/2020 by Megan McKinney, FDOT District 6

#### **Coordination Document:**

PD&E Support Document As Per PD&E Manual

### **Direct Effects**

#### **Identified Resources and Level of Importance:**

##### **500-Foot Buffer:**

##### **2010 Census Designated Places (6)**

- Miami Lakes
- Doral
- Medley
- Hialeah Gardens
- Hialeah
- Country Club

##### **Brownfields (4)**

- Sweetwater D

- City of Hialeah Brownfield Area
- Doral Decor District Brownfield Area
- NW 58th Street Corridor Green Reuse Area

#### **Homeowner and Condominium Associations (23)**

##### **Group Care Facilities (3)**

- Citrus Health Network - Adult Crisis Unit
- Sweet Paradise ALF
- Citrus Health Network - Children's Crisis Unit

##### **Religious Centers (6)**

- Church of Jesus Christ The Almighty
- Church of Christ Hialeah
- Orthodox Cathedral of Christ The Savior
- Bezerra De Menezes Kardec Spiritist Association
- Comunidad Servicios De Cristo
- Iglesia Cristiano Del Alma

##### **Social Service Facilities (1)**

- Bezerra De Menezes Food Pantry

##### **Park and Recreational Facilities (2)**

- Miami Lakes Mini Park (P-50)
- Miami Lakes Mini Park (P-51)

#### **Office of Greenways and Trails (OGT): Hiking Trail Priorities (1)**

- Miami River Greenway

#### **Office of Greenways and Trails (OGT): Multi-Use Trail Opportunities (1)**

- Miami River Greenway

#### **Shared-Use Nonmotorized (SUN) Trail Network in Florida (1)**

- Miami River Greenway

##### **Golf Courses (1)**

- Don Shula's Golf Club

#### **Florida Site File Archaeological or Historic Sites (2)**

- NN
- Hialeah 1

#### **Florida Site File Historic Bridges (4)**

- FDOT Bridge #870257
- FDOT Bridge #870258
- FDOT Bridge #870569
- FDOT Bridge #870259

#### **Florida Site File Historic Standing Structures (41)**

#### **Florida Site File Resource Groups (11)**

#### **1,320-Foot (Quarter-Mile) Buffer:**

#### **2010 Census Designated Places (6)**

- Miami Lakes
- Doral
- Medley
- Hialeah Gardens
- Hialeah
- Country Club

##### **Brownfields (4)**

- Sweetwater D
- City of Hialeah Brownfield Area
- Doral Decor District Brownfield Area
- NW 58th Street Corridor Green Reuse Area

#### **Homeowner and Condominium Associations (67)**

##### **Assisted Housing Facilities (1)**

- Meadowgreen Apartments

##### **Group Care Facilities (8)**

- Golden Palm A.L.F. Inc.
- D P @ B Enterprise
- Citrus Health Network - Adult Crisis Unit

- Sweet Paradise ALF
- Ailin Living Facility
- Our Loving Mother
- New World ALF
- Citrus Health Network - Children's Crisis Unit

### **Religious Centers (12)**

#### **Park and Recreational Facilities (12)**

##### **Office of Greenways and Trails (OGT): Hiking Trail Priorities (1)**

- Miami River Greenway

##### **Office of Greenways and Trails (OGT): Multi-Use Trail Opportunities (1)**

- Miami River Greenway

##### **Shared-Use Nonmotorized (SUN) Trail Network in Florida (1)**

- Miami River Greenway

##### **Golf Courses (1)**

- Don Shula's Golf Club

##### **Florida Site File Archaeological or Historic Sites (2)**

- NN
- Hialeah 1

##### **Florida Site File Historic Bridges (4)**

- FDOT Bridge #870257
- FDOT Bridge #870258
- FDOT Bridge #870569
- FDOT Bridge #870259

##### **Florida Site File Historic Standing Structures (46)**

##### **Florida Site File Resource Groups (11)**

##### **Residential Areas / Acres / Percent**

- Fixed Single Family Units (Less Than 2 Dwelling Units per Acre) / 7.7 / 0.21%
- Fixed Single Family Units (2 to 5 Dwelling Units per Acre) / 276.76 / 7.62%
- Fixed Single Family Units (6 or More Dwelling Units per Acre) / 28.73 / 0.79%
- Mobile Home Units (6 or More Dwelling Units Per Acre) / 0.8 / 0.02%
- Multiple Dwelling Units, Low Rise (2 Stories or Less) / 282.68 / 7.77%
- Multiple Dwelling Units, High Rise (3 Stories or More) / 116.48 / 3.21%

##### **Comments on Effects to Resources:**

SR 826/Palmetto Expressway traverses the U.S. Census Designated Places of Country Club, Doral, Hialeah, Hialeah Gardens, Medley, and Miami Lakes as well as unincorporated areas of Miami-Dade County. The project area is composed primarily of industrial and commercial/retail/office uses along the southern portion of the corridor, with residential uses located north of West Okeechobee Road.

Specific community features associated with aesthetics reported within the 1,320-foot project buffer include four brownfields [one of which is located within the Miami-Dade County Enterprise Zone (EZ-1301)], 67 homeowner and condominium associations, one assisted housing facility, eight group care facilities, 12 park and recreational facilities, one golf course, one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [also a part of the Shared-Use Nonmotorized (SUN) Trail Network], one golf course, and cultural resources.

Impacts of views/vistas from/of SR 826/Palmetto Expressway are anticipated to be limited as the roadway is an existing facility. The Build Alternative involves bridge widening at NW 103rd Street; however, the impacts are expected to be minimal. This section of the SR 826 corridor is approximately 9.5 miles and is urbanized, supporting a wide range of land uses at varying densities. Development along the corridor includes larger structures such as the Westland Mall, Palmetto General Hospital, and numerous hotels and office buildings. These larger buildings along with transportation infrastructure including rail lines, bridges, and roadways all contribute to the existing visual character of the area where large built elements are not uncommon. Residential parcels make up approximately 20 percent of the land use within a quarter mile of the project and occur at a smaller scale. These residences are unlikely to experience viewshed effects.

While elevated structure(s) could be widened/expanded to accommodate the proposed project improvements, impacts to views and vistas within the surrounding area are anticipated to be limited. In addition, proximate land uses along the corridor are not expected to change; as such, the project appears to be consistent with the future land use vision and aesthetic character of the corridor. Context sensitive solutions will also be considered to ensure the project matches local aesthetics and accounts for the community's input on design preferences. Overall, minimal involvement regarding aesthetic effects is anticipated.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:****Additional Comments (optional):****CLC Recommendations:**

During the Project Development phase, FDOT District Six will engage the surrounding communities in coordination with the Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit input on potential project effects as well as opinions and preferences regarding general design concepts related to aesthetics.

**Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**ETAT Reviews and Coordinator Summary: Cultural and Tribal****Historic and Archaeological Sites****Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

**Comments:**

Portions of the project corridor were surveyed previously between 1974 and 2019, and two surveys were conducted in 2012 and 2014 covering the project limits. Two previously recorded archaeological sites are within or adjacent to the corridor, including one National Register of Historic Places (NRHP)-eligible archaeological site. There are 41 Florida Site File historic standing structures [one of which is NRHP-eligible], four Florida Site File historic bridges, and 11 Florida Site File resource groups [one of which is NRHP-eligible]. Property appraiser data suggests the potential for additional unrecorded historic resources within 500 feet of the corridor.

FDOS stated that a Cultural Resources Assessment Survey should be conducted and that all cultural resources, historic districts included, should be assessed for NRHP-eligibility. FDOS further stated that the project has the potential to impact cultural resources and requested to review the completed Cultural Resource Assessment Survey (CRAS). FDOS added that consultation with the project sponsor will take place to avoid, minimize, or mitigate any adverse effects to significant cultural resources. Per review of the 09-14-2020 Advance Notification Package, the Miami-Dade County Office of Historic Preservation (OHP) also requested to receive a copy of the CRAS for review and comment.

Due to the fact that no additional right-of-way is expected to be required to accommodate the proposed improvements, a Summary Degree of Effect of Minimal has been assigned to the Historic and Archaeological Sites topic.

During Project Development, FDOT District Six will conduct a CRAS [in accordance with the FDOT PD&E Manual] to determine the presence of historic and archeological resources in the project area; identified resources will be evaluated/re-evaluated for NRHP-eligibility. The resultant CRAS report will be consistent with the specifications set forth in Chapter 1A-46 Florida Administrative Code and will be submitted to the FDOS and Miami-Dade County OHP for review and comment. FDOT District Six will also coordinate with both agencies when conducting the CRAS.

**Degree of Effect:** 2 *Minimal* assigned 09/28/2020 by Adrianne Daggett, FL Department of State

**Coordination Document:**

PD&E Support Document As Per PD&E Manual

**Coordination Document Comments:**

We look forward to reviewing the Cultural Resources Assessment Survey. All cultural resources, including potential historic districts, within the area of potential effect should be documented and assessed for NRHP eligibility. The resultant survey report shall conform to the specifications set forth in Chapter 1A-46 Florida Administrative Code, FDOT PD&E Manual Part 2, Chapter 8 and will need to be

forwarded to this agency (or the appropriate Federal Agency) for review and comment.

#### **Direct Effects**

##### **Identified Resources and Level of Importance:**

As reported.

##### **Comments on Effects to Resources:**

The project has the potential to impact cultural resources within and adjacent to the proposed project.

##### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

This office will consult with the project sponsors to avoid, minimize, or mitigate any adverse effects to significant cultural resources.

#### **Additional Comments (optional):**

We look forward to reviewing the Cultural Resources Assessment Survey. All cultural resources, including potential historic districts, within the area of potential effect should be documented and assessed for NRHP eligibility. The resultant survey report shall conform to the specifications set forth in Chapter 1A-46 Florida Administrative Code, FDOT PD&E Manual Part 2, Chapter 8 and will need to be forwarded to this agency (or the appropriate Federal Agency) for review and comment.

#### **CLC Recommendations:**

#### **Indirect Effects**

##### **Identified Resources and Level of Importance:**

##### **Comments on Effects to Resources:**

##### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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The following organization(s) were expected to but did not submit a review of the Historic and Archaeological Sites topic for

**Alternative #1:** Seminole Tribe of Florida

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## **Recreational and Protected Lands**

#### **Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

The following recreational areas/features are reported within the 500-foot project buffer: one private golf course [Don Shula's Golf Club], two park and recreational facilities [Miami Lakes Mini Park P-50 and P-51], and one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [Miami River Greenway - also part of the Shared Use Nonmotorized (SUN) trail network]. No Acquisition and Restoration Council (ARC) lands are present.

While right-of-way for this project is not currently anticipated, a Summary Degree of Effect of Minimal has been assigned to the Recreational and Protected Lands topic given the fact that access to and enjoyment of these noted features may temporarily be impacted during project construction.

An assessment of potential impacts to identified recreational resources will be conducted during the Project Development phase. FDOT District Six will further coordinate with relevant agencies on any required studies, documentation, and commitments needed to adequately address identified resources in accordance with federal, state, and local laws and regulations. Future environmental documentation will include an evaluation of the primary, secondary, and cumulative impacts of the proposed project on recreational resources and/or public land.

**Degree of Effect:** 0 *None* assigned 10/27/2020 by Chris Stahl, FL Department of Environmental Protection

#### **Coordination Document:**

No Involvement

**Direct Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:****Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** N/A N/A / *No Involvement* assigned 10/20/2020 by Anita Barnett, National Park Service**Coordination Document:**

No Involvement

**Direct Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:****Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 0 *None* assigned 10/19/2020 by Trisha Stone, South Florida Water Management District**Coordination Document:**

No Involvement

**Direct Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:****Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:**

## **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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### **Section 4(f) Potential**

#### **Project Effects**

**Coordinator Summary Degree of Effect:** N/A N/A / *No Involvement* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

This project does not use federal funding; therefore, Section 4(f) does not apply.

None found

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## **ETAT Reviews and Coordinator Summary: Natural**

### **Water Resources**

#### **Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

Within the project corridor, stormwater runoff from SR 826/Palmetto Expressway is currently collected via a series of drains [located on the inside and outside shoulders] and vegetated swales [located on the outside shoulders] and treated prior to offsite conveyance. The project traverses the Miami River/C6 Canal [WBID: 3290], C-7/Little River Canal [WBID: 3287], and C-8/Biscayne Canal [WBID: 3285], all of which have adopted Total Maximum Daily Loads (TMDLs); the last two are impaired for dissolved oxygen and fecal coliform. Additionally, the three noted waterbodies and the C-4/Tamiami Canal [WBID: 3286] are FDEP Waters Not Attaining Standards; both the C-8/Biscayne Canal and the C-4/Tamiami Canal have FDEP Strategic Monitoring Plans. Water resources located within the 200-foot project buffer include Biscayne Aquifer [a sole source aquifer and principal aquifer of the State of Florida], one recharge area of the Floridan Aquifer, four NPDES stormwater permits, and the SFWMD Southern Everglades boundary.

FDEP reported that increased stormwater runoff from increased impervious surface may impact adjacent surface waters by increasing pollutant loads; other impacts resulting from increased impervious surface include the alteration of existing surface water hydrology and natural drainage patterns and the reduction of flood attenuation capacity. FDEP stated that stormwater treatment should be maximized to prevent ground and surface water contamination. SFWMD noted that water quality volume of the 150% of the greater of 1.0 inch over the total project area or 2.5 inches times the percentage of impervious surface over the entire project area will be required. USEPA commented that an increase in impervious or semi-impervious surfaces can contribute to surface drainage and non-point source pollution that will impact surface and groundwater quality; stormwater runoff from the built environment is a principal contributor to water quality impairment of water bodies nationwide. USEPA recommended best management practices to reduce water quality and quantity impacts as a result of the project. USEPA noted that a Sole Source Aquifer Impact Determination Letter will be required. Per review of the 09-14-2020 Advance Notification Package, the Miami-Dade County Department of Regulatory and Economic Resources - Division of Environmental Resources Management (DERM) requested an analysis of and to review stormwater management needs and flooding issues. DERM further noted that if the existing FDOT drainage system is interconnected or related to any Miami-Dade County and/or other municipalities' drainage system there should be no impact to service at any time.

The proposed stormwater management system will be developed to meet the design and performance criteria established in the SFWMD Environmental Resource Permit Applicant's Handbook Volumes I and II for the treatment and attenuation of discharges to impaired waters; the design will make every effort to maximize the treatment of stormwater runoff from the proposed project improvements. Additionally, best management practices will be employed during project activities. For these reasons, a Summary Degree of Effect of Minimal has been assigned to the Water Resources topic.

During the Project Development phase, FDOT District Six will coordinate with the appropriate agencies concerning the necessary studies, documentation, and commitments needed to adequately address all identified resources. A Stormwater Pollution Prevention Plan (SWPPP) will be implemented [as required by a NPDES Construction Generic Permit] to control the effects of stormwater runoff

during construction. A Water Quality Impact Evaluation will also be prepared in accordance with the FDOT PD&E Manual. All necessary permits will be obtained in accordance with federal, state, and local laws and regulations during subsequent project phases.

**Degree of Effect:** 2 Minimal assigned 10/27/2020 by Chris Stahl, FL Department of Environmental Protection

**Coordination Document:**

No Involvement

**Direct Effects**

**Identified Resources and Level of Importance:**

Stormwater runoff from the expressway surface may alter adjacent surface waters through increased pollutant loading. If widened, increased runoff carrying oils, greases, metals, sediment, and other pollutants from the increased impervious surface would be of concern. Natural resource impacts within and adjacent to the proposed road right-of-way may include alteration of the existing surface water hydrology and natural drainage patterns, and reduction in flood attenuation capacity of area creeks, ditches, and sloughs as a result of increased impervious surface within the watershed.

**Comments on Effects to Resources:**

Every effort should be made to maximize the treatment of stormwater runoff from the proposed roadway construction project, as area stormwater discharges to the local water bodies.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 3 Moderate assigned 10/27/2020 by Amanetta Somerville, US Environmental Protection Agency

**Coordination Document:**

Permit or Technical Study Required

**Direct Effects**

**Identified Resources and Level of Importance:**

In the preliminary environmental discussion (PED), FDOT acknowledges that the proposed project area is within the Biscayne Bay Sole Source Aquifer (SSA). Additionally, the proposed project will span the C-4/Tamiami Canal (WBID: 3286), C-6/Miami Canal (WBID:3290), C-7/Little River (WBID: 3287), and C-8/Biscayne Canal (WBID: 3285). The proposed project also involves the design of a stormwater facility that will include the water quality treatment and attenuation requirements for stormwater discharge to Biscayne Bay. The EPA assigns a Moderate Degree of Effect to the Water Quality and Quantity Designations. As the project continues into future phases of development and more detailed information on the impacts to the SSA will determine the degree of effect to this resource.

**Comments on Effects to Resources:**

Healthy waters provide clean drinking water and productive ecosystems which support a healthy environment and quality of life. Heavy rains within the project corridor have the potential to cause degradation in water quality from wildlife, stock, human sewage, and stormwater runoff. Stormwater runoff from the built environment is a principal contributor to water quality impairment of water bodies (including wetlands) nationwide. An increase in impervious or semi-impervious surfaces can contribute to surface drainage and non-point sources that will impact surface and groundwater quality. Common roadway pollutants such as heavy metals, volatile

organic chemicals, petroleum hydrocarbons, and suspended solids degrade near-by water bodies through stormwater runoff.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

The EPA recommends for Water Quality and Quantity impacts:

- Technical Document: Sole Source Aquifer Impact Determination Letter
- Please contact Larry Cole, Water Protection Division, at cole.larry@epa.gov or 404.562.9474 or Khurram Rafi at rafi.khurram@epa.gov or 404-562-9283 for a Sole Source Aquifer Impact Determination Letter.
- Make every effort to maximize the treatment of stormwater runoff from the proposed roadway improvements.
- Explain how adequate sediment and erosion control measures will prevent the discharge of pollutants into the waterbody.
- Stabilize soils to reduce the effects of erosion, sedimentation, and runoff to maintain or improve water quality.
- Reduce the impact of polluted runoff from construction activities

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 Minimal assigned 10/26/2020 by Trisha Stone, South Florida Water Management District

**Coordination Document:**

Permit or Technical Study Required

**Direct Effects**

**Identified Resources and Level of Importance:**

Surface waters

**Comments on Effects to Resources:**

The project will be required to meet the criteria of the Environmental Resource Permit (ERP) Applicant's Handbook, Volume II with respect to water quality and quantity.

Water quality volume of the 150% of the greater of 1.0 inches over the total project area or 2.5 inches times the percentage impervious over the entire project and a site-specific pre- vs. post-development nutrient analysis will be required

Meeting ERP requirements will presume no effect on resources in regard to water quality and quality and floodplains.

A determination of the effect of the project on water quality and quantity and floodplain will be made at the time an ERP application is submitted.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

## **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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### **Wetlands and Surface Waters**

#### **Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

The National Wetlands Inventory database reports a total of 31.74 acres [4.13%] of lacustrine, palustrine, and riverine wetlands within the 200-foot project buffer; palustrine and riverine wetlands compose the majority. The SFWMD Wetlands database identified 2.59 acres [0.34%] of mixed wetland hardwoods within this same buffer area. Since the project area is mostly urban in nature, many of the noted wetlands are predominantly associated with the ponds and canals/creeks that intersect or abut the corridor.

FDEP reported more than 67 acres of wetlands within the 500-foot project buffer. NMFS stated that if impacts to the wetlands associated with the C4 (Tamiami) Canal, the C6 (Miami River) Canal, or C7 (Little River) Canal, and C8 (Biscayne) Canal are unavoidable, sequential minimization and mitigation should take place. NMFS reported that no Essential Fish Habitat (EFH) would be impacted by the project and stated that it is not aware of any threatened or endangered species or critical habitat under the purview of NMFS within the project area. NMFS noted that a "no effect" determination must be made by the action agency and the reason for the determination must be documented in the project file. SFWMD commented that no state jurisdictional wetlands or other surface waters for which protection or mitigation would be required are currently known to be located within the project area. USACE noted that activities crossing SFWMD Canals C-6, C-7, & C-8 could trigger additional 408 reviews. USACE identified wetlands along the corridor and indicated that the level of importance of these resources would be moderate given any impacts from new road construction or improvements. USACE commented, however, that these wetland resources seem to be isolated. USACE stated that no mitigation banks are within close proximity to the project; a mitigation bank where credits are available needs to be identified. USACE further stated that run-off may be a concern as a result of the project; this impact should be accounted for and avoidance/minimization measures should be implemented. USACE stated that a Standard Individual Permit will likely be required for the project; a Nationwide 3 (Maintenance) or a Regional General Permit could possibly be used as the project advances. USEPA stated support of using the Uniform Mitigation Assessment Methodology (UMAM) to determine potential wetland and surface water impacts. USEPA commented that the number of acres of wetlands to be filled, the classification of those filled wetlands, and any mitigation proposed will further determine the assessment of wetland impacts. USEPA noted that the project area is expected to experience an increase in stormwater runoff and thereby an increase of pollutants into surface waters and wetlands as a result of increased impervious surface. FDEP and SFWMD stated that an Environmental Resource Permit (ERP) will be required. FDEP and USEPA recommend avoidance, minimization, and mitigation measures/opportunities to protect wetlands and other surface waters. FWS specifically stated that if wetland impacts are unavoidable, mitigation that fully compensates for the loss of wetlands is recommended.

While additional right-of-way for this project is not expected to be required, a Summary Degree of Effect of Minimal has been assigned to the Wetlands and Surface Waters topic given the proximity of wetlands to the project corridor.

During the Project Development phase, potential wetland impacts will be assessed through a Natural Resources Evaluation [conducted in accordance with the FDOT PD&E Manual]. All necessary measures will be taken to avoid and/or minimize impacts to wetlands during project design. Should avoidance and/or minimization not be practicable, a Mitigation Plan will be prepared. Best management practices will be utilized during construction and compensatory mitigation will be provided in the event that any adverse wetland impacts are identified. In addition, all applicable permits will be obtained or modified in accordance with federal, state, and local laws and regulations. Further, the proposed stormwater management system for the project will be developed to meet the design and performance criteria established in the SFWMD Environmental Resource Permit Applicant's Handbook Volumes I and II for the treatment and attenuation of discharges to nearby waterbodies. The design will also make every effort to maximize the treatment of stormwater runoff from the proposed project.

**Degree of Effect:** 2 *Minimal* assigned 10/27/2020 by Chris Stahl, FL Department of Environmental Protection

#### **Coordination Document:**

PD&E Support Document As Per PD&E Manual

## **Direct Effects**

### **Identified Resources and Level of Importance:**

The National Wetlands Inventory GIS report indicates that there are 67+ acres of wetlands within the 500-ft. project buffer zone.

### **Comments on Effects to Resources:**

The proposed project will require an environmental resource permit (ERP) from the South Florida Water Management District. The ERP applicant will be required to eliminate or reduce the proposed wetland resource impacts of roadway construction to the greatest extent practicable:

- Minimization should emphasize avoidance-oriented corridor alignments, wetland fill reductions via pile bridging and steep/vertically retained side slopes, and median width reductions within safety limits.
- Wetlands should not be displaced by the installation of stormwater conveyance and treatment swales; compensatory treatment in adjacent uplands is the preferred alternative.
- After avoidance and minimization have been exhausted, mitigation must be proposed to offset the adverse impacts of the project to existing wetland functions and values.
- The cumulative impacts of concurrent and future road improvement projects in the vicinity of the subject project should also be addressed.

### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

### **Additional Comments (optional):**

### **CLC Recommendations:**

## **Indirect Effects**

### **Identified Resources and Level of Importance:**

### **Comments on Effects to Resources:**

### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 3 Moderate assigned 10/27/2020 by Amanetta Somerville, US Environmental Protection Agency

### **Coordination Document:**

To Be Determined: Further Coordination Required

## **Direct Effects**

### **Identified Resources and Level of Importance:**

In the preliminary environmental discussion (PED), FDOT acknowledges that there are approximately 67.5 acres of Lacustrine, Palustrine, and Riverine wetlands within a 500-ft. project buffer area. The proposed project corridor lies within the Biscayne Sole Source Aquifer and recharge zone. EPA supports a qualitative analysis of potential wetland and surface water impacts conducted using the Uniform Mitigation Assessment Methodology (UMAM) during project programming. The classification of the impacted wetlands; the mitigation; the economic importance of aquatic resources; and the importance of the aquatic resources to the protection, maintenance, or enhancement of water quality will be determined in the wetlands assessment. Therefore, EPA assign's a Moderate Degree of effect to Wetlands and Surface Waters. The number of acres of wetlands to be filled, the classification of those filled wetlands and any mitigation will further determine the impact of the wetlands assessment.

### **Comments on Effects to Resources:**

Heavy rains within the project corridor have the potential to cause degradation in water quality from wildlife, stock, human sewage, and stormwater runoff. Wetlands are important because they are a critical natural resource and serve several functions, including filtration and treatment of surface water runoff, store floodwaters, and provide erosion control. Stormwater runoff from roadways carries pollutants such as volatile organics, petroleum hydrocarbons, heavy metals, and pesticides/herbicides. With an increase in the impervious surface area, the project area may experience an increase in stormwater runoff and the increase of pollutants into surface waters and wetlands. Contamination by pollutants or sediments can reduce wetland function characteristics and value. Once contaminants reach wetlands, water chemistry changes can damage the ecosystem.

### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

The EPA recommends the following practices for direct wetland and surface water impacts:

- Consistent with Section 404 of the Clean Water Act, the selected site should avoid and minimize to the maximum extent practicable, placement of fill into jurisdictional waters of the U.S., which include wetlands and streams.
- Sufficient stormwater management facilities are of critical importance for the project corridor and should be able to handle significant storm events to prevent degradation of these significant natural resources.
- Divert stormwater runoff from the proposed project from water bodies and/or treated to prevent water quality impacts to nearby wetlands.
- Demonstrate what increases, if any, in flood plain elevation will result from this project and flow of water and debris in the floodplain would alter discharge capacity.

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 3 Moderate assigned 10/24/2020 by Mark M Tamblyn, US Army Corps of Engineers

**Coordination Document:**

To Be Determined: Further Coordination Required

**Coordination Document Comments:**

The proposed project will more than likely have to be permitted using a Standard Individual Permit, but depending on the project development and planning process a different permit might be more applicable. There is a possibility that a Regional General Permit or a Nationwide permit could be used as the project develops and planning moves forward. Please remember activities crossing SFWMD canals C-6, C-7 & C-8 could trigger additional 408 reviews regarding proposed activities at these crossings.

**Direct Effects**

**Identified Resources and Level of Importance:**

The waters of the U.S. (wetlands and surface waters) included in this project consist of: 4.21 acres of lacustrine wetlands, 9.16 acres of palustrine herbaceous wetlands, and 7.01 acres of palustrine forested herbaceous wetlands, 21.55 acres palustrine (fw pond), and 25.59 acres of riverine wetlands exist within a 500 foot buffer; 1.71 acres of lacustrine wetlands, 2.11 acres of palustrine herbaceous wetlands, and 2.23 acres of palustrine forested wetlands, 9.09 acres palustrine (fw pond), and 16.6 acres of riverine wetlands exist within a 200 foot buffer; and 0.1 acres of lacustrine wetlands, 0.75 acres of palustrine herbaceous wetlands, and 0.58 acres of palustrine forested wetlands, and 3.66 acres palustrine (fw pond), and 12.72 acres of riverine wetlands exist within a 100 foot buffer. The level of importance would be moderate for new road construction or improvements through these associated wetlands and surface waters. The wetland types seem to be a bit more isolated along this alternative. Avoidance and minimization measures should always be incorporated into the project's design, and best management practices should be utilized during project planning activities. Compensatory mitigation must be provided for any adverse wetland impacts resulting from the proposed project improvements. Concerns with storm water runoff and additional attenuation and treatment of discharges to nearby waterbodies should always be considered, additional ROW acquisition might be required. Any fill associated with this projects roadway improvements, should first be avoided and minimized to eliminate any additional impacts with the wetland resources found adjacent to this project. Any and all activities will require a Department of Army (DA) Section 404 Corps permit, and possible Compensatory mitigation. It appears that no Mitigation Banks appear to be in a close vicinity to this project (nothing within a mile). The further studies should Identify a Mitigation Bank that can be used, where credits are available.

**Comments on Effects to Resources:**

Any wetlands or surface waters in the project area deemed to be jurisdictional along the existing roadway should be of higher quality

and avoided and impacts minimized. Much of this project's corridor is surrounded by wetlands. Given the jurisdictional wetland resources along the proposed project corridor, any impacts to these resources will be substantial. Run-off concerns may become a new potential problem within this new improvement / expansion and additional studies should look at these types of impacts and attempt to avoid and minimize any and all. These types of impacts will need to be permitted and mitigated for as well.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

The Corps recommends a continued emphasis on wetland avoidance and minimization opportunities throughout the planning process. A wetland survey should be conducted within the study area to identify the wetlands and a jurisdictional determination should be completed. Permittee responsible on-site and/or off-site mitigation options for unavoidable impacts should be considered early on in the project development and planning phases. The proposed project will more than likely have to be permitted using a Standard Individual Permit. There is a possibility that a Regional General Permit might be applicable, or a Nationwide permit could be used as the project development and planning moves forward.

#### **Additional Comments (optional):**

The proposed project will more than likely have to be permitted using a Standard Individual Permit, but depending on the project development and planning process a different permit might be more applicable. There is a possibility that a Regional General Permit or a Nationwide permit could be used as the project develops and planning moves forward. Please remember activities crossing SFWMD canals C-6, C-7 & C-8 could trigger additional 408 reviews regarding proposed activities at these crossings.

#### **CLC Recommendations:**

##### **Indirect Effects**

##### **Identified Resources and Level of Importance:**

See comments for direct effects.

##### **Comments on Effects to Resources:**

See comments for direct effects.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

See comments for direct effects.

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**Degree of Effect:** 2 *Minimal* assigned 10/19/2020 by Trisha Stone, South Florida Water Management District

##### **Coordination Document:**

Permit or Technical Study Required

##### **Permits**

Environmental Resource Permit

##### **Comments:**

An Environmental Resource Permit would be required from the South Florida Water Management District (SFWMD). A pre-application meeting with SFWMD staff is recommended.

##### **Technical Studies**

Location Hydraulics Report

##### **Direct Effects**

##### **Identified Resources and Level of Importance:**

No state jurisdictional wetlands or other surface waters for which protection or mitigation would be required are currently known to be located within the project area.

##### **Comments on Effects to Resources:**

At the time of submittal of an Environmental Resource Permit (ERP) application to the SFWMD, wetlands and other surface water impacts will be evaluated by the SFWMD. Direct and secondary impacts to wetlands and surface waters must meet the criteria in Section 10 of ERP Applicant's Handbook Volume I.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 *Minimal* assigned 10/19/2020 by Jennifer Schull, National Marine Fisheries Service

**Coordination Document:**

No Involvement

**Direct Effects****Identified Resources and Level of Importance:**

Based on our review of the information provided on the EST website, GIS-based effects analysis of wetlands and interpretation of aerial photographs, NOAA's National Marine Fisheries Service (NMFS) has determined that palustrine, lacustrine, and riverine wetlands are located within the project corridor. These wetlands are likely low to moderate in quality.

**Comments on Effects to Resources:**

The project corridor contains wetlands associated with the C4 (Tamiami) Canal, The C6 (Miami River) Canal, C7 (Little River) Canal, and C8 (Biscayne) Canal. The wetlands along the SR 826 corridor provide water quality functions, such as removal of sediments, excess nutrients, and contaminants, which benefit and support these aquatic ecosystems. Through hydrological connections, these wetlands also contribute plant material and other useable nutrients (both dissolved and particulate organic matter) into aquatic food webs that include recreationally, commercially, and ecologically important species within downstream estuaries. If wetland impacts are unavoidable, sequential minimization and mitigation should take place.

In addition to the direct impacts from filling wetlands, construction activities may impact adjacent wetlands through sedimentation and runoff. Best Management Practices should be used to ensure sedimentation and runoff are avoided.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

Magnuson-Stevens Act: Based on the project location, information provided in the ETDM website, and GIS-based analysis of impacts, NOAA's National Marine Fisheries Service (NMFS) concludes that essential fish habitat (EFH) would not be impacted by the proposed road modifications; accordingly, we offer no comments pursuant to the EFH provisions of the Magnuson-Stevens Act (P.L. 104-297); and this project will not require an EFH Assessment. Further consultation on this matter is not necessary unless future modifications are proposed and you believe that the proposed action may result in adverse impacts to EFH.

Endangered Species Act: We are not aware of any threatened or endangered species or critical habitat under the purview of NMFS that occur within the project area. However, it should be noted that a "no effect" determination must be made by the action agency and the reasoning underlying the determination should be documented in a project file. Please coordinate closely with the U.S. Fish and Wildlife Service for other species listed under the Endangered Species Act that may require consultation.

Fish and Wildlife Coordination Act: The comments NMFS provided regarding sequential mitigation are in accordance with the Fish and Wildlife Coordination Act.

**Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 Minimal assigned 09/16/2020 by John Wrublik, US Fish and Wildlife Service

**Coordination Document:**

To Be Determined: Further Coordination Required

**Coordination Document Comments:**

none provided

**Direct Effects**

**Identified Resources and Level of Importance:**

Wetlands

**Comments on Effects to Resources:**

Wetlands provide important habitat for fish and wildlife and may occur within and near the project site. We recommend that these valuable resources be avoided to the greatest extent practicable. If impacts to these wetlands are unavoidable, we recommend the Florida Department of Transportation provide mitigation that fully compensates for the loss of important resources.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):**

none provided

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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## Protected Species and Habitat

**Project Effects**

**Coordinator Summary Degree of Effect:** 2 Minimal assigned 11/13/2020 by FDOT District 6

**Comments:**

The 200-foot project buffer occurs within the South Florida Ecosystem Management Area; FWS Consultation Areas for the American crocodile, Florida bonneted bat, and snail kite; and Rare Range for the Florida black bear. The 200-foot project buffer also contains critical habitat for the West Indian manatee, two rare and imperiled fish [Mountain Mullet and Opossum Pipefish] within the C-4/Tamiami Canal, and Core Foraging Area for the wood stork.

FDACS stated that approximately 7 acres of wetlands are located within the 500-foot buffer and should be conserved. FDACS identified 16 rare and listed plants which may occur within the project area. FDACS further recommended best management practices including silt fences and conducting surveys for rare and listed plants; if any of the plants are found, they should be protected or translocated to a suitable alternative site. FWC stated that no significant wildlife resources are identified in the project area; minimal impacts to fish or wildlife resources are anticipated. FWS reported that the project occurs within the Core Foraging Area of two active nesting wood stork colonies; any lost foraging habitat resulting from the project must be mitigated within the same Core Foraging Area as the affected nesting colony(ies). FWS also reported that the project occurs within the geographic range and FWS consultation area for the endangered Florida bonneted bat and stated that FDOT should consult the FWS's 2019 Florida Bonneted Bat Consultation Key for guidance on surveys, effect determinations, and consultations with FWS. FWS further noted that federally-listed plants have the potential to occur in or near the project site. FWS recommends that FDOT prepare a Biological Assessment during the PD&E phase of the project and use native plants, trees, shrubs, and flowering plants in the landscape design of the roadway to benefit pollinators. SFWMD noted that the project area is accessible to the West Indian manatee via the C-6 Canal

(Miami River), C-7 Canal (Little River Canal) and C-8 Canal (Biscayne Canal). SFWMD identified measures to reduce/eliminate direct and secondary impacts to manatees.

A Summary Degree of Effect of Minimal has been assigned to the Protected Species and Habitat topic given that the area surrounding the project corridor is predominantly urban in nature and limited involvement regarding protected species and habitat is anticipated.

The final design of the project will avoid and/or minimize impacts to wetlands/protected species and habitat to the greatest extent practicable and appropriate mitigation will be provided for unavoidable impacts. Agency guidelines will be followed, and best management practices will be utilized during project activities. A Natural Resources Evaluation will be prepared during the Project Development phase in accordance with the FDOT PD&E Manual. Further, consultation with environmental resource agencies will occur during the Project Development phase in order to address potential project impacts to the noted listed species and determine any site-specific measures required for the project.

**Degree of Effect:** 2 Minimal assigned 10/19/2020 by Jason Hight, FL Fish and Wildlife Conservation Commission

**Coordination Document:**

No Involvement

**Direct Effects**

**Identified Resources and Level of Importance:**

No significant wildlife resources were identified in the project area.

**Comments on Effects to Resources:**

Minimal impacts to fish or wildlife resources are anticipated to result from this project.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

None.

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 Minimal assigned 10/19/2020 by Trisha Stone, South Florida Water Management District

**Coordination Document:**

Permit or Technical Study Required

**Direct Effects**

**Identified Resources and Level of Importance:**

Wetland/marine-dependent wildlife and wildlife habitat potentially affected by the construction and operation of the project are the West Indian manatee (*Trichechus manatus latirostris*).

**Comments on Effects to Resources:**

The C-6 Canal (Miami River), C-7 Canal (Little River Canal) and C-8 Canal (Biscayne Canal) are all accessible by the West Indian manatee.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

Practicable project design modifications to reduce and eliminate direct and secondary impacts to the West Indian manatee would be required to be demonstrated, in accordance with AH I, 10.2.2. Manatee protection measures will be required during all in-water work. In addition, manatee exclusion grates will be required on all existing and proposed outfalls that are associated with the SWM system serving this project.

**Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** N/A N/A / No Involvement assigned 10/05/2020 by Mark Kiser, FL Department of Agriculture and Consumer Services

**Coordination Document:**

No Involvement

**Direct Effects****Identified Resources and Level of Importance:**

Approximately 7 acres of wetlands are located within 500 feet of proposed project activities, and should be conserved to the degree possible. Wetlands are the base for many aquatic and terrestrial food chains, and have high productivity, supporting a high diversity of plant and animal species. Wetlands also provide a vital breeding, feeding, and nursery habitat for many species of wildlife. Wetlands are of great economic importance, as they improve water quality and act as effective flood and erosion buffers.

A number of rare and listed plants may occur within the project footprint, including Blodgett's silverbush (state endangered, endemic to two counties in FL), Cape Sable thoroughwort (state and federal endangered, endemic to two counties in FL), Carter's mustard (state and federal endangered, endemic to south and central FL), Carter's small-flowered flax (state endangered, endemic to one county in FL), crenulate lead-plant (state and federal endangered, endemic to one county in FL), deltoid spurge (state and federal endangered, endemic to one county in FL), Everglades bulby (state threatened, endemic to two counties in FL), Florida brickell-bush (state and federal endangered, endemic to one county in FL), Florida pineland crabgrass (state endangered, endemic to two counties in FL), Florida prairie-clover (state endangered, endemic to south FL), Florida semaphore cactus (state and federal endangered, endemic to two counties in FL), Garber's spurge (state and federal endangered, endemic to south FL), pineland sandmat (state endangered, endemic to two counties in FL), sand flax (state endangered, endemic to two counties in FL), Small's milkpea (state and federal endangered, endemic to one county in FL), tiny polygala (state and federal endangered, endemic to south FL), and Florida bristle fern (state and federal endangered, endemic to two counties in FL).

**Comments on Effects to Resources:**

While Florida does not have a goal of no net loss or gain of wetland acreage, regulatory rules are written in a manner that achieves a programmatic goal through implementation, and a project permitting goal of no net loss in wetland or other surface water functions. The State's Environmental Resource Permit (ERP) standard requires that activities not adversely impact the value of functions provided to fish and wildlife and listed species by wetlands and other surface waters.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

Use best management practices, including silt fencing, to protect wetlands from impacts. Surveys for rare and listed plants should be conducted, and if present, should be protected to the degree possible, or translocated to a suitable alternative site by an organization such as the Florida Native Plant Society.

**Additional Comments (optional):****CLC Recommendations:****Indirect Effects**

**Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 Minimal assigned 09/16/2020 by John Wrublik, US Fish and Wildlife Service

**Coordination Document:**

To Be Determined: Further Coordination Required

**Coordination Document Comments:**

none provided

**Direct Effects****Identified Resources and Level of Importance:**

Federally listed species and fish and wildlife resources

**Comments on Effects to Resources:**

Federally-listed species -

The Service has reviewed our Geographic Information Systems (GIS) database for recorded locations of federally listed threatened and endangered species on or adjacent to the project study area. The GIS database is a compilation of data received from several sources. Based on review of our GIS database, the Service notes that the following federally listed species may occur in or near the project area.

Florida bonneted bat

The project is located in the geographic range and the Service's consultation area for the endangered Florida bonneted bat (FBB; *Eumops floridanus*). If suitable habitat occurs with the project footprint, than surveys based on the Service's guidance should be conducted to determine the status of the FBB within the project footprint. The most current FBB survey guidance is located at: [https://www.fws.gov/verobeach/ProgrammaticPDFs/20191022\\_letter\\_ServiceCorps\\_FBB-ProgrammaticKey.pdf](https://www.fws.gov/verobeach/ProgrammaticPDFs/20191022_letter_ServiceCorps_FBB-ProgrammaticKey.pdf)

Wood Stork

The project corridor is located in the Core Foraging Area (CFA; all lands within 18.6 miles ) of two active nesting colonies of the threatened wood stork (*Mycteria americana*). The Service believes that the loss of wetlands within a CFA due to an action could result in the loss of foraging habitat for the wood stork. To minimize adverse effects to the wood stork, we recommend that any lost foraging habitat resulting from the project be replaced within the CFA of the affected nesting colony. Moreover, wetlands provided as mitigation should adequately replace the wetland functions lost as a result of the action. The Service does not consider the preservation of wetlands, by itself, as adequate compensation for impacts to wood stork foraging habitat, because the habitat lost is not replaced. Accordingly, any wetland mitigation plan proposed should include a restoration, enhancement, or creation component. In some cases, the Service accepts wetlands compensation located outside the CFA of the affected wood stork nesting colony. Specifically, wetland credits purchased from a "Service Approved" mitigation bank located outside of the CFA would be acceptable to the Service, provided that the impacted wetlands occur within the permitted service area of the bank.

For projects that impact 5 or more acres of wood stork foraging habitat, the Service requires a functional assessment be conducted using our "Wood Stork Foraging Analysis Methodology" (Methodology) on the foraging habitat to be impacted and the foraging habitat provided as mitigation. The Methodology can be found at: [https://www.fws.gov/verobeach/BirdsPDFs/20120712\\_WOSTForageAssessmentMethodology\\_Appendix.pdf](https://www.fws.gov/verobeach/BirdsPDFs/20120712_WOSTForageAssessmentMethodology_Appendix.pdf).

The Service also believes that the following federally listed species have the potential to occur in or near the project site: Federally listed plants (<http://www.fws.gov/verobeach>ListedSpeciesPlants.html>). Accordingly, the Service recommends that the Florida Department of Transportation (FDOT) prepare a Biological Assessment for the project (as required by 50 CFR 402.12) during the FDOT's Project Development and Environment process.

Fish and Wildlife Resources -

To benefit fish and wildlife, we recommend that native plants, trees and shrubs be used in the landscaping of the lands within the project footprint. The use of native wildflowers would be especially beneficial to insect pollinators and provide a more aesthetically pleasing environment than sod by itself.

Wetlands provide important habitat for fish and wildlife and may occur within and near the project site. We recommend that these valuable resources be avoided to the greatest extent practicable. If impacts to these wetlands are unavoidable, we recommend the Florida Department of Transportation provide mitigation that fully compensates for the loss of important resources.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):**

none provided

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Coastal and Marine**

**Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

**Comments:**

The project is located over eight miles from the coast and is not within the vicinity of a coastal barrier resource. NMFS stated that if impacts to the wetlands associated with the C4 (Tamiami) Canal, the C6 (Miami River) Canal, or C7 (Little River) Canal, and C8 (Biscayne) Canal are unavoidable, sequential minimization and mitigation should take place. NMFS reported that no Essential Fish Habitat (EFH) would be impacted by the project and stated that it is not aware of any threatened or endangered species or critical habitat under the purview of NMFS within the project area. NMFS noted that a "no effect" determination must be made by the action agency and the reason for the determination must be documented in the project file.

The project [including the proposed stormwater management system] will be designed to meet state water quality and quantity requirements, avoidance and minimization measures will be utilized for the proposed project design, and best management practices will be adhered to during project construction to prevent impacts [primarily siltation] to downstream estuarine habitats. For these reasons, a Summary Degree of Effect of Minimal has been assigned to the Coastal and Marine topic.

During the Project Development phase, potential impacts to sensitive coastal and marine resources will be assessed through a Natural Resources Evaluation conducted in accordance with the FDOT PD&E Manual. The proposed improvements will be designed to meet state water quality and quantity requirements, and best management practices will be utilized during project activities. Further, the proposed stormwater management system for the project will be developed to meet the design and performance criteria established in the SFWMD Environmental Resource Permit Applicant's Handbook Volumes I and II for the treatment and attenuation of discharges to nearby waterbodies. The design will also make every effort to maximize the treatment of stormwater runoff from the proposed project.

**Degree of Effect:** 0 *None* assigned 10/19/2020 by Trisha Stone, South Florida Water Management District

**Coordination Document:**

No Involvement

**Direct Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:****Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 Minimal assigned 10/19/2020 by Jennifer Schull, National Marine Fisheries Service

**Coordination Document:**

No Involvement

**Direct Effects****Identified Resources and Level of Importance:**

Based on our review of the information provided on the EST website, GIS-based effects analysis of wetlands and interpretation of aerial photographs, NOAA's National Marine Fisheries Service (NMFS) has determined that palustrine, lacustrine, and riverine wetlands are located within the project corridor. These wetlands are likely low to moderate in quality.

**Comments on Effects to Resources:**

The project corridor contains wetlands associated with the C4 (Tamiami) Canal, The C6 (Miami River) Canal, C7 (Little River) Canal, and C8 (Biscayne) Canal. The wetlands along the SR 826 corridor provide water quality functions, such as removal of sediments, excess nutrients, and contaminants, which benefit and support these aquatic ecosystems. Through hydrological connections, these wetlands also contribute plant material and other useable nutrients (both dissolved and particulate organic matter) into aquatic food webs that include recreationally, commercially, and ecologically important species within downstream estuaries. If wetland impacts are unavoidable, sequential minimization and mitigation should take place.

In addition to the direct impacts from filling wetlands, construction activities may impact adjacent wetlands through sedimentation and runoff. Best Management Practices should be used to ensure sedimentation and runoff are avoided.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

Magnuson-Stevens Act: Based on the project location, information provided in the ETDM website, and GIS-based analysis of impacts, NOAA's National Marine Fisheries Service (NMFS) concludes that essential fish habitat (EFH) would not be impacted by the proposed road modifications; accordingly, we offer no comments pursuant to the EFH provisions of the Magnuson-Stevens Act (P.L. 104-297); and this project will not require an EFH Assessment. Further consultation on this matter is not necessary unless future modifications are proposed and you believe that the proposed action may result in adverse impacts to EFH.

Endangered Species Act: We are not aware of any threatened or endangered species or critical habitat under the purview of NMFS that occur within the project area. However, it should be noted that a "no effect" determination must be made by the action agency and the reasoning underlying the determination should be documented in a project file. Please coordinate closely with the U.S. Fish and Wildlife Service for other species listed under the Endangered Species Act that may require consultation.

Fish and Wildlife Coordination Act: The comments NMFS provided regarding sequential mitigation are in accordance with the Fish and Wildlife Coordination Act.

**Additional Comments (optional):****CLC Recommendations:****Indirect Effects**

**Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Floodplains****Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

**Comments:**

According to the DFIRM (SFHA) 100 Year Flood Zones data, 360.49 acres [46.84%] of the 200-foot project buffer occurs within the 100-year floodplain [Flood Zones AE and AH]. The 100-year floodplain is primarily associated with the C6/Miami River Canal and ponds that surround the canal and adjacent neighborhoods. SFWMD stated that compensatory storage will be required for any additional encroachment into the 100-year floodplain.

Due to the fact that additional right-of-way for this project is not required and floodplain compensation [associated with project impacts] will be addressed through necessary agency coordination, a Summary Degree of Effect of Minimal has been assigned to the Floodplains topic.

During Project Development, a Location Hydraulic Report will be prepared in accordance with the FDOT PD&E Manual. All necessary permits will be obtained in accordance with federal, state, and local laws and regulations. In addition, impacts to floodplain storage will be compensated in accordance with the SFWMD Environmental Resource Permit Applicant's Handbook Volumes I & II.

**Degree of Effect:** 2 *Minimal* assigned 10/26/2020 by Trisha Stone, South Florida Water Management District

**Coordination Document:**

Permit or Technical Study Required

**Direct Effects****Identified Resources and Level of Importance:**

Surface waters

**Comments on Effects to Resources:**

Storage will be required for any additional encroachment within the floodplain under the 100-year elevation.

Meeting SFWMD Environmental Resource Permit (ERP) requirements will presume no effect on resources in regard to water quality and quality and floodplains.

A determination of the effect of the project on the floodplain will be made at that time of submittal of an ERP application.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:****Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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## ETAT Reviews and Coordinator Summary: Physical

### Contamination

#### Project Effects

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 11/13/2020 by FDOT District 6

#### Comments:

Potential sources of contamination reported within the 200-foot project buffer include four brownfields [Sweetwater D, City of Hialeah, Doral Decor District, and NW 58th Street Corridor Green Reuse Area], 28 hazardous waste facilities, 25 petroleum contamination monitoring sites, 26 storage tank contamination monitoring sites, 17 SUPER Act risk sources, two toxic release inventory sites, 61 USEPA RCRA regulated facilities, eight Miami-Dade County Department of Regulatory and Economic Resources - Division of Environmental Resources Management (DERM) contamination sites, two FDEP dry cleaning program sites, one waste cleanup responsible party site - closed, and two abandoned railways; it should be noted that some of the noted sources may overlap categories.

FDEP stated that the project is not expected to significantly affect potential contaminated sites and that a Contamination Screening Evaluation similar to Phase I and Phase II Audits may need to be performed along the proposed project right-of-way. SFWMD commented that a SFWMD Water Use Permit may be required if dewatering is necessary. USEPA noted potential contamination impacts to the Biscayne Sole Source Aquifer as a concern. USEPA stated that soils, groundwater, and surface waters have the potential to be negatively affected by contaminated sites. USEPA recommended that sampling and analysis of soils and groundwater be conducted should petroleum storage tanks be impacted or removed; corrective action should be completed before commencement of project activities. Per review of the 09-14-2020 Advance Notification Package, the Miami-Dade County Department of Regulatory and Economic Resources - Division of Environmental Resources Management (DERM) stated that in the event contamination is discovered, DERM should be notified within 24 hours of discovery.

A Summary Degree of Effect of Moderate has been assigned to the Contamination topic due to the number of sites and proximity of these sources to the project corridor as well as the potential presence of unreported sources of subsurface contamination.

Contamination [including any required permits] will be evaluated during the Project Development phase in accordance with federal, state, and local laws and regulations. A Contamination Screening Evaluation Report [similar to Phase I and Phase II Audits] will be prepared in accordance with the FDOT PD&E Manual, including site specific surveys to assess existing known subsurface contamination as well as historical contamination release. Any identified sites will be investigated to determine their potential risk, and proper mitigation will take place if medium to high risk sites are identified. Contingency plans will be included in the project's construction contract documents. These provisions will specify procedures to follow in the event any hazardous material or suspected contamination is encountered during construction.

**Degree of Effect:** 2 *Minimal* assigned 10/27/2020 by Chris Stahl, FL Department of Environmental Protection

#### Coordination Document:

No Involvement

#### Direct Effects

#### Identified Resources and Level of Importance:

GIS data indicates that there are four brownfield areas, 69 hazardous waste facilities, six solid waste facilities and four toxic release sites within the 500-ft. project buffer zone.

#### Comments on Effects to Resources:

The proposed project is not expected to significantly affect potential contaminated sites. A Contamination Screening Evaluation similar to Phase I and Phase II Audits may need to be performed along the proposed project right-of-way considering the proximity to potential petroleum and hazardous material handling facilities.

#### Recommended Avoidance, Minimization, and Mitigation Opportunities:

**Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 3 *Moderate* assigned 10/27/2020 by Amanetta Somerville, US Environmental Protection Agency

**Coordination Document:**

To Be Determined: Further Coordination Required

**Direct Effects****Identified Resources and Level of Importance:**

There are potential sources of sub-surface contamination reported within the 500-foot project buffer. There are 58 Petroleum Contamination Monitoring Sites, 69 Hazardous Waste Facilities, 17 Onsite Sewage sites, 71 Storage Tank Contamination Monitoring sites, 6 solid waste facilities, 4 brownfields, 29 State Underground Petroleum Environmental Response (Super) Act risk source, and 38 U.S. EPA National Pollutant Discharge Elimination System sites. EPA assigns a Moderate degree of effect for contamination due to the potential impact on the Biscayne Sole Source Aquifer.

**Comments on Effects to Resources:**

Soils, groundwater, and surface waters have the potential to be affected adversely by contaminated sites. The contamination of groundwater can result in poor drinking water quality and loss of water supply. Petroleum hydrocarbons are the primary focus of site and risk assessments. The petroleum constituents that may negatively impact human health are aromatic hydrocarbons (benzene, ethylbenzene, toluene, and xylenes), polycyclic aromatic hydrocarbons, gasoline additives (MTBE and TBA), and combustion emissions from fuels. If there is an encounter with any subsurface hazardous wastes in the ground, it can contaminate groundwater and degrade land use. If these wastes are not cleaned-up the property may become a brownfield site.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

If any petroleum storage tanks are to be impacted or removed during the construction phase of the project, sampling and analysis of soils and groundwater should be conducted to determine if petroleum and hydrocarbon pollutants are present above regulatory levels. The EPA recommends corrective action is completed before the commencement of project activities.

**Additional Comments (optional):****CLC Recommendations:****Indirect Effects****Identified Resources and Level of Importance:****Comments on Effects to Resources:****Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 *Minimal* assigned 10/19/2020 by Trisha Stone, South Florida Water Management District

**Coordination Document:**

To Be Determined: Further Coordination Required

**Coordination Document Comments:**

If construction dewatering is necessary, a Water Use Permit from the SFWMD may be required. A Water Use General Permit is available under Rule 40E-2.061(2), FAC. Projects that do not qualify for the Water Use General Permit will require a Water Use Permit from SFWMD. A Water Use Permit application must address any contamination issues located within the vicinity of the project

area.

#### **Direct Effects**

##### **Identified Resources and Level of Importance:**

Surface water and ground water.

##### **Comments on Effects to Resources:**

If construction dewatering is necessary, a Water Use Permit from the SFWMD may be required. A Water Use General Permit is available under Rule 40E-2.061(2), FAC. Projects that do not qualify for the Water Use General Permit will require a Water Use Permit from SFWMD. A Water Use Permit application must address any contamination issues located within the vicinity of the project area.

##### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

##### **Additional Comments (optional):**

If construction dewatering is necessary, a Water Use Permit from the SFWMD may be required. A Water Use General Permit is available under Rule 40E-2.061(2), FAC. Projects that do not qualify for the Water Use General Permit will require a Water Use Permit from SFWMD. A Water Use Permit application must address any contamination issues located within the vicinity of the project area.

#### **CLC Recommendations:**

#### **Indirect Effects**

##### **Identified Resources and Level of Importance:**

##### **Comments on Effects to Resources:**

##### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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## **Air Quality**

### **Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

According to the USEPA Green Book as of July 31, 2020, the project is not located within a USEPA-designated Air Quality Maintenance or Non-Attainment Area for any of the six pollutants [ozone, carbon monoxide, sulfur dioxide, nitrogen dioxide, lead, and small particulate matter] specified by the USEPA in National Ambient Air Quality Standards. Therefore, the Clean Air Act conformity requirements do not apply to this project at this time.

USEPA stated that while air quality within the project area could possibly be affected by airborne dust and other ambient air pollutants from project construction, the proposed project is in an attainment area so criteria pollutants under NAAQS are considered to be at acceptable levels. USEPA recommended the use of diesel controls, cleaner fuel, and cleaner construction practices for on-road and off-road equipment to maintain healthy air quality.

While no permanent effects to air quality are anticipated, potential temporary localized impacts to air quality could occur as a result of fugitive dust and exhaust emissions generated from equipment during project construction activities. For these reasons, a Summary Degree of Effect of Minimal has been assigned to the Air Quality topic. During the Project Development phase, an Air Quality Technical Memorandum will be prepared in accordance with the FDOT PD&E Manual.

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**Degree of Effect:** 2 *Minimal* assigned 10/23/2020 by Amanetta Somerville, US Environmental Protection Agency

#### **Coordination Document:**

To Be Determined: Further Coordination Required

#### **Direct Effects**

#### **Identified Resources and Level of Importance:**

A wide variety of air pollutants can emit from stationary and mobile sources. The EPA establishes the National Ambient Air Quality Standards (NAAQS) to protect public health and public welfare and regulates emissions of hazardous air pollutants. The proposed project is in an attainment area for all six criteria pollutants (ozone, carbon monoxide, sulfur dioxide, nitrogen dioxide, lead, and small particulate matter) under the NAAQS. Although the proposed interchange expansion and the addition of access points may temporarily degrade air quality during construction, the EPA assigns a Minimal degree of effect on air quality.

#### **Comments on Effects to Resources:**

The project area air quality can be affected by airborne dust, and other ambient air pollutants from project construction and construction equipment.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

To maintain healthy air quality, consider the use of diesel emission controls, cleaner fuel and cleaner construction practices for on-road and off-road equipment used for transportation, soil movement, or other project activities.

#### **Additional Comments (optional):**

#### **CLC Recommendations:**

#### **Indirect Effects**

#### **Identified Resources and Level of Importance:**

#### **Comments on Effects to Resources:**

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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## **Navigation**

### **Project Effects**

**Coordinator Summary Degree of Effect:** 0 None assigned 11/13/2020 by FDOT District 6

#### **Comments:**

The project crosses three SFWMD maintained canals [Miami River/C6 Canal [WBID: 3290], C-7/Little River Canal [WBID: 3287], and C-8/Biscayne Canal [WBID: 3285]. However, the canals are not considered navigable due to a series of flood control structures located both west and east of the project corridor that hinder access to open water.

USACE stated that no impacts to traditional navigable waters are apparent; therefore, the project should not require a Department of the Army (DA) authorization for impacts to Section 10 of the Rivers and Harbors Act.

Based on the foregoing, a Summary Degree of Effect of None has been assigned to the Navigation topic.

**Degree of Effect:** 2 Minimal assigned 10/24/2020 by Mark M Tamblyn, US Army Corps of Engineers

#### **Coordination Document:**

To Be Determined: Further Coordination Required

#### **Coordination Document Comments:**

This proposed project alternative should not require a Department of the Army (DA) authorization for impacts to Section 10 of the Rivers and Harbors Act. Since no impacts to traditional navigable waters are apparent. This project alternative review only specifies crossing over three (3) SFWMD canals C-6, C-7 & C-8, if any other activities are proposed, further review of this project would need to determine if any other activities would require Department of Army authorization (DA) and possibly a 408 review of the project as

well.

#### **Direct Effects**

##### **Identified Resources and Level of Importance:**

This project alternative has no proposed impacts to any traditional navigation, but it does cross three SFWMD canals C-6, C-7 & C-8. During the present review it appears there are no direct effects with resources regarding navigation. This review recognizes canal crossings only. Any additional proposed activities at the Canal crossing could require additional review by the Department of the Army and (DA) authorization, and possible 408 review for activities which are proposed at the crossings.

##### **Comments on Effects to Resources:**

This project alternatives will have no effects to any resources related to traditional navigation. This projects alternatives cross three (3) SFWMD canals C-6, C-7 & C-8. This review has recognized this project will only cross over these canals. Any other proposed activities at these canal crossings would require Department of Army (DA) authorization, and possibly a 408 review of these activities as well.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

See comments for direct effects.

##### **Additional Comments (optional):**

This proposed project alternative should not require a Department of the Army (DA) authorization for impacts to Section 10 of the Rivers and Harbors Act. Since no impacts to traditional navigable waters are apparent. This project alternative review only specifies crossing over three (3) SFWMD canals C-6, C-7 & C-8, if any other activities are proposed, further review of this project would need to determine if any other activities would require Department of Army authorization (DA) and possibly a 408 review of the project as well.

#### **CLC Recommendations:**

#### **Indirect Effects**

##### **Identified Resources and Level of Importance:**

See comments for direct effects.

##### **Comments on Effects to Resources:**

See comments for direct effects.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

See comments for direct effects.

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**Degree of Effect:** N/A N/A / *No Involvement* assigned 10/13/2020 by Marty Bridges, US Coast Guard

##### **Coordination Document:**

No Involvement

#### **Direct Effects**

##### **Identified Resources and Level of Importance:**

None found.

##### **Comments on Effects to Resources:**

None found.

#### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

None found.

##### **Additional Comments (optional):**

None found.

#### **Indirect Effects**

##### **Identified Resources and Level of Importance:**

None found.

##### **Comments on Effects to Resources:**

None found.

##### **Recommended Avoidance, Minimization, and Mitigation Opportunities:**

None found.

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## **Infrastructure**

### **Project Effects**

**Coordinator Summary Degree of Effect:** 3 *Moderate* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

Infrastructure-related features reported within the 200-foot project buffer include 10 electric power transmission lines, 10 onsite sewage facilities, 11 USEPA water quality data monitoring stations, four Federal Aviation Administration obstructions, two BTS grade level railroad crossings, two RCI railroad crossings, one wireless antenna structure location, and 797.47 linear feet of railway [mainline]. In addition, various pipelines run parallel to and cross the SR 826/Palmetto Expressway project corridor. Fiber optic cable markers, street lights, power lines, and utility cabinet boxes are scattered throughout the length of the corridor. Further, power lines, traffic lights, street lights, utility cabinet boxes, pedestrian signals, etc. are concentrated along the roadways connecting to SR 826/Palmetto Expressway via on- and off-ramps as well as the frontage roads.

Per review of the 09-14-2020 Advance Notification Package, the Miami-Dade County Department of Solid Waste Management (DSWM) stated there are no impacts to solid waste operations. The Miami-Dade County Water and Sewer Department (WASD) noted that they have water mains and sanitary gravity sewer mains that are along or intersect the project area; utility lines of the City of Hialeah, Hialeah Gardens, and Medley are also in the project area. WASD recommends that FDOT coordinate with the WASD Utility Coordination Supervisor as well as utility departments from the noted municipalities. The Miami-Dade County Department of Regulatory and Economic Resources - Division of Environmental Resources Management (DERM) stated that a portion of the project corridor (between NW 50th Street and NW 70th Street) is located within the Alexander Orr Wellfield Protection Area; development within this area is subject to regulations.

Given potential conflicts with the utilities present, a Summary Degree of Effect of Moderate has been assigned to the Infrastructure topic.

An assessment of potential impacts to identified infrastructure-related resources, including utilities, will be conducted during the Project Development phase. A Utility Assessment Package will be prepared in accordance with the FDOT PD&E Manual. FDOT District Six will further coordinate with relevant agencies and utility owners on required studies, documentation, and commitments needed to adequately address potential impacts to identified resources in accordance with federal, state, and local laws and regulations.

None found

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## **Noise**

### **Project Effects**

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### **Comments:**

The project is surrounded by urban development, including residential neighborhoods, industrial parks, and commercial/retail/office complexes. Specific community features within the 200-foot project buffer that may be sensitive to noise and vibration effects include 13 homeowner and condominium associations, two group care facilities, one cultural center, 53 health care facilities, three laser facilities [one of which is an eye care facility], three religious centers, one private golf course, one park and recreational facility, one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [Miami River Greenway - also part of the Shared Use Nonmotorized (SUN) trail network], and cultural resources.

Increased noise levels during construction and presumable noise level increases from higher traffic volumes/enhanced flow as a

result of improved operational conditions along SR 826/Palmetto Expressway could have impacts on nearby residences and recreational features. However, given that the proposed improvements for this project are anticipated to be constructed within the existing right-of-way and additional abatement criteria will be considered to offset any potential unavoidable adverse effects, a Summary Degree of Effect of Minimal has been assigned to the Noise topic.

During the Project Development phase, a Public Involvement Plan will be implemented in accordance with the FDOT PD&E Manual and public outreach will be conducted by FDOT District Six in coordination with Miami-Dade Transportation Planning Organization, Miami-Dade County, and the municipalities associated with the project limits to solicit opinions from residents and business owners on potential noise and vibration effects related to the proposed project improvements. A Noise Study Report will also be prepared in accordance with the FDOT PD&E Manual. Any identified potential effects will be assessed, and noise abatement criteria will be followed in accordance with the FDOT PD&E Manual.

None found

## ETAT Reviews and Coordinator Summary: Special Designations

### Special Designations

#### Project Effects

**Coordinator Summary Degree of Effect:** 2 *Minimal* assigned 11/13/2020 by FDOT District 6

#### Comments:

There is one sole source aquifer [Biscayne Aquifer] located within the 200-foot project buffer. However, there are no Outstanding Florida Waters, Aquatic Preserves, or Wild and Scenic Rivers within the buffer area.

SFWMD noted that Right-of-Way permits or permit modifications will be required for any work within the canal right-of-way for the SFWMD's C-6 Canal (Miami River), the C-7 Canal (Little River Canal) and the C-8 Canal (Biscayne Canal). USEPA noted that the Biscayne Sole Source Aquifer is within the 500-foot project buffer; this resource is most vulnerable to contamination.

Based on the foregoing and the fact that the project [including the proposed stormwater management system] will be designed to meet state water quality and quantity requirements, avoidance and minimization measures will be utilized for the proposed design, and best management practices will be adhered to during construction to prevent impacts to proximate sensitive waters [specifically, the Biscayne Sole Source Aquifer], a Summary Degree of Effect of Minimal has been assigned to the Special Designations topic.

During the Project Development phase, a Water Quality Impact Evaluation and Sole Source Aquifer Checklist will be completed in accordance with the FDOT PD&E Manual.

**Degree of Effect:** 3 *Moderate* assigned 10/23/2020 by Amanetta Somerville, US Environmental Protection Agency

#### Coordination Document:

Permit or Technical Study Required

#### Permits

Sole Source Aquifer

#### Comments:

Sole Source Aquifer Impact Determination Letter

-Please contact Larry Cole, Water Protection Division, at [Cole.larry@epa.gov](mailto:Cole.larry@epa.gov) or 404.562.9474 or Khurram Rafi at [Rafi.khurram@epa.gov](mailto:Rafi.khurram@epa.gov) or 404-562-9283 for a Sole Source Aquifer Impact Determination Letter.

## Direct Effects

### Identified Resources and Level of Importance:

In the preliminary environmental discussion (PED), FDOT acknowledges that the proposed project area is within the Biscayne Bay, an Aquatic Preserve. Although the Biscayne Sole Source Aquifer (SSA) is not listed as an Outstanding Florida Water under Florida Administrative Code 62.302.700, the January 2015 Agency Operating and Funding Agreement for Continuing Participation in Efficient Transportation Decision Making and Transportation Project Development Processes between the EPA and Federal Highway Administration, and FDOT states in Section 4-Statement of Work that USEPA will review issues for Special Designations focusing on

Sole Source Aquifers pursuant to the Safe Drinking Water Act. According to Water Quality and Quantity GIS Analysis in the EST, the Biscayne Sole Source Aquifer and Recharge is within the project 500 ft. buffer and is most vulnerable to contamination. The EPA assigns a Moderate Degree of Effect to Special Designations. As the project continues into future phases of development and more detailed information on the impacts to the SSA will determine the degree of effect to this resource.

**Comments on Effects to Resources:**

Hazardous pollutants can infiltrate the aquifer from known and unreported sources of subsurface contamination.

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

Technical Document: Sole Source Aquifer Impact Determination Letter

Please contact Larry Cole, Water Protection Division, at [Cole.larry@epa.gov](mailto:Cole.larry@epa.gov) or 404.562.9474 or Khurram Rafi at [Rafi.khurram@epa.gov](mailto:Rafi.khurram@epa.gov) or 404-562-9283 for a Sole Source Aquifer Impact Determination Letter.

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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**Degree of Effect:** 2 Minimal assigned 10/19/2020 by Trisha Stone, South Florida Water Management District

**Coordination Document:**

Permit or Technical Study Required

**Direct Effects**

**Identified Resources and Level of Importance:**

The project area includes the SFWMD's C-6 Canal (Miami River), the C-7 Canal (Little River Canal) and the C-8 Canal (Biscayne Canal).

**Comments on Effects to Resources:**

Right-of-Way permits or permit modifications will be required for any work within the canal right-of-way for the SFWMD's C-6 Canal (Miami River), the C-7 Canal (Little River Canal) and the C-8 Canal (Biscayne Canal).

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

**Additional Comments (optional):**

**CLC Recommendations:**

**Indirect Effects**

**Identified Resources and Level of Importance:**

**Comments on Effects to Resources:**

**Recommended Avoidance, Minimization, and Mitigation Opportunities:**

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## Eliminated Alternatives

There are no eliminated alternatives for this project.

## Project Scope

### General Project Recommendations

There are no general project recommendations identified for this project in the EST.

### Anticipated Permits

Permit	Type	Comments	Assigned By	Date
Section 404 - Individual or General	USACE	[or potentially a Nationwide 3 (Maintenance) or a Regional General Permit]	FDOT District 6	11/13/20
National Pollutant Discharge Eliminated System	FDEP		FDOT District 6	09/09/20
Environmental Resource Permit	Water	-Potentially a SFWMD Water Use Permit if dewatering is necessary -Potentially a SFWMD Right-of-Way Permit or permit modification if work is done within SFWMD Canal right-of-way	FDOT District 6	11/13/20

### Permitting Timetable

Federal Permitting Agencies identified are also Co-operating Agencies for the development of this project. Permit application occurs when design plans are developed with sufficient engineering detail to support a complete permit application. This is expected to occur within one year FEIS/ROD approval and Location Design Concept Approval for the selected alternative, unless otherwise agreed upon during project development.

### Anticipated Technical Studies

Technical Study Name	Type	Comments	Assigned By	Date
Location Hydraulics Report	ENGINEERING		FDOT District 6	09/09/2020
Public Involvement Plan	ENVIRONMENTAL		FDOT District 6	09/09/2020
Noise Study Report	ENVIRONMENTAL		FDOT District 6	09/09/2020
Contamination Screening Evaluation Report	ENVIRONMENTAL		FDOT District 6	09/09/2020
Water Quality Impact Evaluation	Other		FDOT District 6	09/09/2020
Sociocultural Effects Evaluation	Other		FDOT District 6	09/09/2020
Air Quality Technical Memorandum	ENVIRONMENTAL		FDOT District 6	09/09/2020
Cultural Resource Assessment Survey	ENVIRONMENTAL		FDOT District 6	09/09/2020
Utility Assessment Package	ENGINEERING		FDOT District 6	09/09/2020
Sole Source Aquifer Letter	ENVIRONMENTAL	(Sole Source Aquifer Checklist)	FDOT District 6	09/09/2020
Natural Resources Evaluation (NRE)	ENVIRONMENTAL		FDOT District 6	09/09/2020

### Issue Resolution Activity Log

There are no dispute actions identified for this project in the EST.

# Appendices

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## Preliminary Environmental Discussion Comments

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### Social and Economic

#### Social

##### Project Level

Refer to Analysis Area PED Comments.

##### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

The project traverses six U.S. Census Designated Places, including Miami Lakes, Doral, Medley, Hialeah Gardens, Hialeah, and Fontainebleau. The project area is composed primarily of industrial and commercial/retail/office uses with residential uses located north of West Okeechobee Road. Within the 500-foot project buffer, there is one Enterprise Zone abutting West Okeechobee Road [EZ 1301], 23 homeowner and condominium associations, three group care facilities [Citrus Health Network - Adult Crisis Unit, Sweet Paradise Assisted Living Facility, and Citrus Health Network - Children's Crisis Unit], one community center [After School All-Stars South Florida], two cultural centers [Miami Lakes Library and Florida National College Library], one fire station [Miami-Dade County Fire and Rescue Station 1], one social service facility [Bezerra De Menezes Food Pantry], one hospital [Southern Winds Hospital], 106 health care facilities, three laser facilities [Aran and Holbrook Eye Associates, Daniel Santoyo, DDS, PA, and Hialeah Wellness and Rehab], two government buildings [US Post Office - Hialeah Annex and Florida Department of Highway Safety and Motor Vehicles Service Center], six religious centers, eight schools, one private golf course [Don Shula's Golf Club], two park and recreational facilities [Miami Lakes Mini Park P-50 and P-51], one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [Miami River Greenway - also a part of the Shared Use Nonmotorized (SUN) trail network], and cultural resources. Compared to the demographic characteristics for Miami-Dade County, the 500-foot project buffer contains notably higher White and Hispanic population percentages, a lower population percentage of African Americans, a slightly higher percentage of individuals age 65 and over, and a slightly lower percentage of housing units with no vehicle available. The median family income of census block groups within the 500-foot project buffer is slightly higher than that of Miami-Dade County. Limited English Proficiency (LEP) accommodations will be required during public involvement efforts of the Project Development phase as 44.26% or 23,028 individuals within the census block groups containing the project corridor "speak English less than very well". No further neighborhood division or social isolation is expected to occur as a result of the project. However, given that access to residences, businesses, and community features could temporarily be affected during project construction and potential visual impacts to surrounding communities could occur if elevated structure(s) are widened/expanded to accommodate the proposed improvements, overall impacts to social elements are anticipated to be minimal. Both a Sociocultural Effects Evaluation and a Public Involvement Plan will be included in the Project Development and Environment Study scoping recommendations.

#### Economic

##### Project Level

Refer to Analysis Area PED Comments.

##### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

SR 826/Palmetto Expressway is part of Florida's Strategic Intermodal System (SIS) highway network, providing regional access to employment centers and residential areas across the state as well as facilitating the movement of significant commuter, visitor, and freight traffic. Specific economic features that occur within the 500-foot project buffer include four brownfields [one of which is located within the Miami-Dade County Enterprise Zone (EZ-1301)]. The proposed project improvements are intended to address existing congestion and higher than expected speed differentials between the general purpose lanes and express lanes along the project corridor. Reduced congestion will serve to maintain and improve viable access to major transportation facilities [such as the Miami Hialeah FEC intermodal Terminal and SIS Freight Rail Terminal] and businesses of the area, including freight activity centers/local distribution facilities. Consequently, the improved operational conditions of the corridor are expected to enhance access to existing employment centers. While the proposed improvements are intended to enhance economic conditions of the surrounding area by better accommodating existing and future travel demand as a result of population and employment growth, access to proximate businesses along the corridor may temporarily be affected and/or modified during project construction. For these reasons, overall economic impacts are anticipated to be minimal. A Sociocultural Effects Evaluation will be included in the

Project Development and Environment Study scoping recommendations.

## Land Use Changes

### Project Level

Refer to Analysis Area PED Comments.

### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

This project is located in north-central Miami-Dade County and traverses six U.S. Census Designated Places, including Miami Lakes, Doral, Medley, Hialeah Gardens, Hialeah, and Fontainebleau. The 500-foot project buffer primarily consists of industrial and commercial/retail/office uses with residential uses located north of West Okeechobee Road. Additionally, there are four brownfields [one of which is located within the Miami-Dade County Enterprise Zone (EZ-1301)], one private golf course [Don Shula's Golf Club], and two park and recreational facilities [Miami Lakes Mini Park P-50 and P-51] within the project buffer. According to the adopted Miami-Dade County 2020 and 2030 Land Use Plan Map, the area surrounding the project corridor is expected to continue to support urban uses. The project is anticipated to accommodate existing and proposed development within the area. For these reasons, minimal impacts or changes to proximate land uses are anticipated as a result of the project. A Sociocultural Effects Evaluation will be included in the Project Development and Environment Study scoping recommendations.

## Mobility

### Project Level

Refer to Analysis Area PED Comments.

### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Enhanced

SR 826/Palmetto Expressway supports vital regional and local commerce as it is part of Florida's Strategic Intermodal System (SIS) highway network and connects employment and population centers within Miami-Dade County. Additionally, the project corridor connects to I-75, another SIS corridor, as well as provides access via SR 934/NW 74th Street (also known as Hialeah Expressway) and surface streets to the Miami Hialeah FEC Intermodal Terminal, an SIS Freight Rail Terminal. The project is anticipated to address roadway deficiencies through operational improvements, reducing congestion by increasing throughput and decreasing travel times for the general purpose lanes. This is expected to better accommodate future travel demand projected as a result of population and employment growth. The reduced congestion is intended to better align speeds between the general purpose lanes and express lanes. The reduced delays on the corridor can also enhance evacuation capabilities as well as emergency access and incident response times [especially since SR 826/Palmetto Expressway serves as part of the emergency evacuation route network of the state]. The project is within one transportation disadvantaged service provider area [Miami-Dade Transit Authority] and traverses the Miami Metrorail line [heavy rail metro service] at SR 934, with the Palmetto Metrorail Station located within 1,320 feet of SR 826/Palmetto Expressway. Further, 15 local bus routes cross or operate within the vicinity of the project section. In addition, one trail intersects the project corridor. Overall, the project improvements are critical to enhancing regional mobility and key in preserving the operational integrity and regional functionality of the SR 826/Palmetto Expressway corridor as a whole. A Sociocultural Effects Evaluation will be included in the Project Development and Environment Study scoping recommendations.

## Aesthetic Effects

### Project Level

Refer to Analysis Area PED Comments.

### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

The project area primarily consists of industrial and commercial/retail/office uses with residential uses located north of West Okeechobee Road. Within the 500-foot project buffer, the corridor traverses six U.S. Census Designated Places [Miami Lakes, Doral, Medley, Hialeah Gardens, Hialeah, and Fontainebleau] and four brownfields. Specific community features associated with aesthetics include 23 homeowner and condominium associations, three group care facilities [Citrus Health Network - Adult Crisis Unit, Sweet Paradise Assisted Living Facility, and Citrus Health Network - Children's Crisis Unit], one private golf course [Don Shula's Golf Club], two park and recreational facilities [Miami Lakes Mini Park P-50 and P-51], one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [Miami River Greenway - also a part of the Shared Use Nonmotorized (SUN) trail network], and cultural resources. While elevated structure(s) could be widened/expanded to accommodate the proposed project improvements, impacts to views and vistas within the surrounding area are anticipated to be limited. In addition, proximate land uses along the

corridor are not expected to change; as such, the project appears to be consistent with the future land use vision and aesthetic character of the corridor. Overall, minimal involvement regarding aesthetic effects is anticipated. A Sociocultural Effects Evaluation will be included in the Project Development and Environment Study scoping recommendations.

## Relocation Potential

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

The project area primarily consists of industrial and commercial/retail/office uses with residential uses located north of West Okeechobee Road. Numerous businesses and residencies abut the project corridor. Existing right-of-way along the corridor ranges from approximately 235 feet to 455 feet or more in width near existing interchanges. Additional right-of-way is not expected to be required nor are potential relocations to accommodate the proposed improvements; however, access to proximate residences and businesses may temporarily be affected and/or modified as a result of the project. Encroachment into surrounding parcels, if necessary, will be coordinated with the appropriate property owners. The proposed project improvements will be designed to minimize right-of-way acquisition to the greatest extent practicable. Specific right-of-way requirements will be determined during the Project Development and Environment Study. Based on the foregoing, minimal involvement regarding relocation potential is anticipated. A Sociocultural Effects Evaluation will be included in the Project Development and Environment Study scoping recommendations.

## Farmlands

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** N/A / No Involvement

There are no farmlands reported within the 500-foot project buffer. According to the Miami-Dade County 2020 and 2030 Land Use Plan Map, the project area is anticipated to continue to support urban uses. Lands within the project vicinity do not meet the definition of farmland as defined in 7 CFR 658, and the provisions of the Farmland Protection Policy Act of 1981 do not apply because the entire project area is located in the Miami Urbanized Area with no designated farmlands adjacent to the project corridor. For these reasons, no involvement regarding farmlands is anticipated as a result of this project.

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## Cultural and Tribal

### Section 4(f) Potential

#### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** N/A / No Involvement

This is a state funded project; therefore, Section 4(f) does not apply.

## Historic and Archaeological Sites

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

Portions of the project corridor were surveyed previously between 1974 and 2019, and two surveys were conducted in 2012 and 2014 covering the project limits. Two previously recorded archaeological sites are within or adjacent to the corridor, including one

National Register of Historic Places (National Register)-eligible archaeological site. There are 41 Florida Site File historic standing structures [one of which is NRHP-eligible], four Florida Site File historic bridges, and 11 Florida Site File resource groups [one of which is NRHP-eligible]. Property appraiser data suggests the potential for additional unrecorded historic resources within 500 feet of the corridor. Due to the fact that no additional right-of-way is expected to be required to accommodate the proposed improvements, minimal involvement regarding historic and archaeological sites is anticipated. A Cultural Resource Assessment Survey will be included in the Project Development and Environment Study scoping recommendations.

## Recreational and Protected Lands

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

The following recreational areas/features are reported within the 500-foot buffer: one private golf course [Don Shula's Golf Club], two park and recreational facilities [Miami Lakes Mini Park P-50 and P-51], and one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [Miami River Greenway - also a part of the Shared Use Nonmotorized (SUN) trail network]. No Acquisition and Restoration Council (ARC) lands are present. While right-of-way for this project is not currently anticipated, minimal involvement regarding recreational areas and protected lands is expected given the fact that access to and enjoyment of these noted features may temporarily be impacted during project construction.

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## Natural

## Wetlands and Surface Waters

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

The National Wetlands Inventory database reports a total of 31.74 acres [4.13%] of lacustrine, palustrine, and riverine wetlands within the 200-foot project buffer; palustrine and riverine wetlands compose the majority. The SFWMD Wetlands database identified 2.59 acres [0.34%] of mixed wetland hardwoods within this same buffer area. Since the project area is mostly urban in nature, many of the noted wetlands are predominantly associated with the ponds and canals/creeks that intersect or abut the corridor. Avoidance and minimization measures will be incorporated into the project's design, best management practices will be utilized during project activities, and compensatory mitigation will be provided for any adverse wetland impacts resulting from the proposed project improvements. Further, the proposed stormwater management system for the project will be developed to meet the design and performance criteria established in the SFWMD Environmental Resource Permit Applicant's Handbook Volumes I and II for the treatment and attenuation of discharges to nearby waterbodies. As such, stormwater runoff from the proposed project will be treated to prevent water quality impacts to nearby wetlands. While additional right-of-way for this project is not expected to be required, minimal involvement regarding wetland resources is anticipated given the proximity of wetlands to the project corridor. A Natural Resources Evaluation will be included in the Project Development and Environment Study scoping recommendations.

## Water Resources

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

Within the project corridor, stormwater runoff from SR 826/Palmetto Expressway is currently collected via a series of drains [located on the inside and outside shoulders] and vegetated swales [located on the outside shoulders] and treated prior to offsite conveyance. The project traverses the Miami River/C6 Canal [WBID: 3290], C-7/Little River Canal [WBID: 3287], and C-8/Biscayne Canal [WBID: 3285], all of which have adopted Total Maximum Daily Loads (TMDLs); the last two are impaired for dissolved oxygen and fecal coliform. Additionally, the three noted waterbodies and the C-4/Tamiami Canal [WBID: 3286] are FDEP Waters Not Attaining Standards; both the C-8/Biscayne Canal and the C-4/Tamiami Canal have FDEP Strategic Monitoring Plans. Water quality

and quantity resources located within the 200-foot project buffer include Biscayne Aquifer [a sole source aquifer and principal aquifer of the State of Florida], one recharge area of the Floridan Aquifer, four NPDES stormwater permits, and the SFWMD Southern Everglades boundary. The proposed stormwater management system will be developed to meet the design and performance criteria established in the SFWMD Environmental Resource Permit Applicant's Handbook Volumes I and II for the treatment and attenuation of discharge into impaired waters; the design will make every effort to maximize the treatment of stormwater runoff from the proposed roadway improvements. A Storm Water Pollution Prevention Plan (SWPPP) will also be implemented [as required by NPDES Construction Generic Permits] to control the effects of stormwater runoff during construction. For these reasons, minimal involvement regarding water resources is anticipated. A Water Quality Impact Evaluation will be included in the Project Development and Environment Study scoping recommendations.

## Floodplains

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

According to the DFIRM (SFHA) 100 Year Flood Zones data, 360.49 acres [46.84%] of the 200-foot project buffer occurs within the 100-year floodplain [Flood Zones AE and AH]. The 100-year floodplain is primarily associated with the C6/Miami River Canal and ponds that surround the canal and adjacent neighborhoods. Due to the fact that additional right-of-way for this project is not required and floodplain compensation [associated with project impacts] will be addressed through necessary agency coordination, minimal involvement regarding floodplains is anticipated. A Location Hydraulic Report will be included in the Project Development and Environment Study scoping recommendations.

## Protected Species and Habitat

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

The 200-foot project buffer occurs within the South Florida Ecosystem Management Area; FWS Consultation Areas for the American crocodile, Florida bonneted bat, and snail kite; and Rare Range for the Florida black bear. The 200-foot project buffer also contains critical habitat for the West Indian manatee, two rare and imperiled fish [Mountain Mullet and Opossum Pipefish] within the C-4/Tamiami Canal, and Core Foraging Area for the wood stork. Avoidance and minimization measures will be implemented for the noted species to the greatest extent practicable. In addition, agency coordination will take place to address potential project impacts to each noted species. For these reasons and given the surrounding urban environment, minimal involvement regarding protected species and habitat resources is anticipated. A Natural Resources Evaluation will be included in the Project Development and Environment Study scoping recommendations.

## Coastal and Marine

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

The project is located over eight miles from the coast and is not within the vicinity of a coastal barrier resource. The project [including the proposed stormwater management system] will be designed to meet state water quality and quantity requirements, avoidance and minimization measures will be utilized for the proposed project design, and best management practices will be adhered to during project construction to prevent impacts [primarily siltation] to downstream estuarine habitats. For these reasons, minimal involvement regarding coastal and marine resources is anticipated. A Natural Resources Evaluation will be included in the Project Development and Environment Study scoping recommendations.

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## Physical

## Noise

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

The project is surrounded by urban development, including residential neighborhoods, industrial parks, and commercial/retail/office complexes. Specific community features within the 200-foot project buffer that may be sensitive to noise and vibration effects include 13 homeowner and condominium associations, two group care facilities [Citrus Health Network - Adult Crisis Unit and Citrus Health Network - Children's Crisis Unit], one cultural center [Florida National College Library], 53 health care facilities, three laser facilities [one of which is an eye care facility], three religious centers, one private golf course [Don Shula's Golf Club], one park and recreational facility [Miami Lakes Mini Park P-50] one Office of Greenways and Trails (OGT) multi-use trail opportunity/hiking trail priority [Miami River Greenway - also a part of the Shared Use Nonmotorized (SUN) trail network], and cultural resources. Increased noise levels during construction and presumable noise level increases from higher traffic volumes/enhanced flow as a result of improved operational conditions along SR 826/Palmetto Expressway could have impacts on nearby residences and recreational features. However, given that the proposed improvements for this project are anticipated to be constructed within the existing right-of-way and additional abatement criteria will be considered to offset any potential unavoidable adverse effects, overall noise and vibration related impacts as a result of the project are anticipated to be minimal. A Noise Study Report will be included in the Project Development and Environment Study scoping recommendations.

## Air Quality

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Minimal

According to the USEPA Green Book as of July 31, 2020, the project is not located within a USEPA-designated Air Quality Maintenance or Non-Attainment Area for any of the six pollutants [ozone, carbon monoxide, sulfur dioxide, nitrogen dioxide, lead, and small particulate matter] specified by the USEPA in National Ambient Air Quality Standards. Therefore, the Clean Air Act conformity requirements do not apply to this project at this time. Minimal, localized impacts to air quality could occur as a result of fugitive dust and exhaust emissions generated from equipment during project construction; no permanent effects to air quality are anticipated. An Air Quality Technical Memorandum will be included in the Project Development and Environment Study scoping recommendations.

## Contamination

### Project Level

Refer to Analysis Area PED Comments.

#### Analysis Area

**Areas:** Alternative #1

**Degree of Effect:** Moderate

Potential sources of contamination reported within the 200-foot project buffer include four brownfields [Sweetwater D, City of Hialeah, Doral Decor District, and NW 58th Street Corridor Green Reuse Area], 28 hazardous waste facilities, 25 petroleum contamination monitoring sites, 26 storage tank contamination monitoring sites, 17 SUPER Act risk sources, two toxic release inventory sites, 61 USEPA RCRA regulated facilities, eight Miami-Dade County Department of Environmental Management (DERM) contamination sites, two FDEP dry cleaning program sites, one waste cleanup responsible party site - closed, and two abandoned railways; it should be noted that some of the noted sources may overlap categories. Moderate involvement regarding contamination is anticipated given the number of sites and proximity of these sources to the project corridor as well as the potential presence of unreported sources of subsurface contamination. A Contamination Screening Evaluation Report will be included in the Project Development and Environment Study scoping recommendations.

## Infrastructure

### Project Level

Refer to Analysis Area PED Comments.

## **Analysis Area**

**Areas:** Alternative #1

**Degree of Effect:** Moderate

Infrastructure-related features reported within the 200-foot project buffer include 10 electric power transmission lines, 10 onsite sewage facilities, 11 USEPA water quality data monitoring stations, four Federal Aviation Administration obstructions, two BTS grade level railroad crossings, two RCI railroad crossings, one wireless antenna structure location, and 797.47 linear feet of railway [mainline]. In addition, various pipelines run parallel to and cross the SR 826/Palmetto Expressway project corridor. Fiber optic cable markers, street lights, power lines, and utility cabinet boxes are scattered throughout the length of the corridor. Further, power lines, traffic lights, street lights, utility cabinet boxes, pedestrian signals, etc. are concentrated along the roadways connecting to SR 826/Palmetto Expressway via on- and off-ramps as well as the frontage roads. Moderate involvement regarding infrastructure-related features is anticipated given potential conflicts with the numerous utilities present. A Utility Assessment Package will be included in the Project Development and Environment Study scoping recommendations.

## **Navigation**

### **Project Level**

Refer to Analysis Area PED Comments.

## **Analysis Area**

**Areas:** Alternative #1

**Degree of Effect:** None

The project crosses three SFWMD maintained canals [Miami River/C6 Canal [WBID: 3290], C-7/Little River Canal [WBID: 3287], and C-8/Biscayne Canal [WBID: 3285]. However, the canals are not considered navigable due to a series of flood control structures located both west and east of the project corridor that hinder access to open water. For these reasons, no impacts to navigation are anticipated.

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## **Special Designations**

### **Special Designations: Outstanding Florida Waters**

#### **Project Level**

Refer to Analysis Area PED Comments.

## **Analysis Area**

**Areas:** Alternative #1

**Degree of Effect:** N/A / No Involvement

There are no Outstanding Florida Waters reported within the 200-foot project buffer; therefore, no involvement regarding these specially-designated resources is anticipated.

### **Special Designations: Aquatic Preserves**

#### **Project Level**

Refer to Analysis Area PED Comments.

## **Analysis Area**

**Areas:** Alternative #1

**Degree of Effect:** N/A / No Involvement

There are no Aquatic Preserves reported within the 200-foot project buffer; therefore, no involvement regarding these specially-designated resources is anticipated.

### **Special Designations: Wild and Scenic Rivers**

#### **Project Level**

Refer to Analysis Area PED Comments.

## **Analysis Area**

**Areas:** Alternative #1

**Degree of Effect:** N/A / No Involvement

There are no Wild and Scenic Rivers nor waters that are part of the Nationwide Rivers Inventory reported within the 200-foot

project buffer; therefore, no involvement regarding these specially-designated resources is anticipated.

## **Special Designations: Sole Source Aquifers**

### **Project Level**

Refer to Analysis Area PED Comments.

### **Analysis Area**

**Areas:** Alternative #1

**Degree of Effect:** Minimal

There is one sole source aquifer [Biscayne Aquifer] located within the 200-foot project buffer. The project [including the proposed stormwater management system] will be designed to meet state water quality and quantity requirements, avoidance and minimization measures will be utilized for the proposed design, and best management practices will be adhered to during construction to prevent impacts to proximate sensitive waters. For these reasons, minimal involvement regarding this specially-designated resource is anticipated. A Water Quality Impact Evaluation and Sole Source Aquifer Checklist will be included in the Project Development and Environment Study scoping recommendations.

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## **Advance Notification Comments**

### **FL Department of Agriculture and Consumer Services Comment --**

No further comments.

--Mark Kiser, 10/5/2020

No response

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### **FL Department of State Comment --**

No comments.

--Adrianne Daggett, 9/28/2020

No response

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**Miami-Dade Expressway Authority Comment --**

1) SR 826/Palmetto Expressway between north NW 154 Street and south of NW 25 Street includes an interchange with SR 924/Gratigny Parkway (under the jurisdiction of Miami-Dade Expressway Authority (MDX)). The southern limit of the project is located one interchange to the north of interchange of SR 826 with SR 836/Dolphin Expressway. Therefore, proposed improvements as part of the SR 826 project should be closely coordinated with MDX.

2) MDX will require an access/construction permit for construction activities within MDX ROW.

3) MDX will require a Memorandum of Understanding (or similar instrument) executed by FDOT and MDX detailing FDOT's responsibilities during and after construction including long term maintenance of improvements, in the vicinity of the interchanges with SR 924 and SR 836.

4) MDX will require review and approval of all proposed improvements in the vicinity of SR 924 and SR 836 during the project development and final design phases.

5) MDX will require review and approval of the project's Traffic Methodology Letter of Understanding (MLOU).

6) MDX will require review and approval of all new interchange movements, connection or modifications to the existing SR 924 and SR 836 facilities via an IMR/IJR document.

7) MDX will not approve modifications or improvements that affect the safety, throughput, level of service, and/or mobility of existing MDX customers.

--Albert Sosa, 10/21/2020

No response

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## GIS Analyses

Since there are so many GIS Analyses available for Project #14455 - SR 826/Palmetto Expressway, they have not been included in this ETDM Summary Report. GIS Analyses, however, are always available for this project on the Public ETDM Website. Please click on the link below (or copy this link into your Web Browser) in order to view detailed GIS tabular information for this project:

<http://etdmpub.fla-etat.org/est/index.jsp?tpID=14455&startPageName=GIS%20Analysis%20Results>

**Special Note:** Please be sure that when the GIS Analysis Results page loads, the **Programming Screen Summary Report Published on 11/13/2020 by Lauren Brooks Milestone** is selected. GIS Analyses snapshots have been taken for Project #14455 at various points throughout the project's life-cycle, so it is important that you view the correct snapshot.

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## Degree of Effect Legend

Color Code	Meaning	ETAT	Public Involvement
N/A	Not Applicable / No Involvement	There is no presence of the topic in relationship to the project, or the topic is irrelevant in relationship to the proposed transportation action.	
0	None (after 12/5/2005)	The topic is present, but the project will have no impact on the topic; project has no adverse effect on ETAT resources; permit issuance or consultation involves routine interaction with the agency. The <i>None</i> degree of effect is new as of 12/5/2005.	No community opposition to the planned project. No adverse effect on the community.
1	Enhanced	Project has positive effect on the ETAT resource or can reverse a previous adverse effect leading to environmental improvement.	Affected community supports the proposed project. Project has positive effect.
2	Minimal	Project has little adverse effect on ETAT resources. Permit issuance or consultation involves routine interaction with the agency. Low cost options are available to address concerns.	Minimum community opposition to the planned project. Minimum adverse effect on the community.
2	Minimal to None (assigned prior to 12/5/2005)	Project has little adverse effect on ETAT resources. Permit issuance or consultation involves routine interaction with the agency. Low cost options are available to address concerns.	Minimum community opposition to the planned project. Minimum adverse effect on the community.
3	Moderate	Agency resources are affected by the proposed project, but avoidance and minimization options are available and can be addressed during development with a moderated amount of agency involvement and moderate cost impact.	Project has adverse effect on elements of the affected community. Public Involvement is needed to seek alternatives more acceptable to the community. Moderate community interaction will be required during project development.

4	Substantial	The project has substantial adverse effects but ETAT understands the project need and will be able to seek avoidance and minimization or mitigation options during project development. Substantial interaction will be required during project development and permitting.	Project has substantial adverse effects on the community and faces substantial community opposition. Intensive community interaction with focused Public Involvement will be required during project development to address community concerns.
5	Potential Issue (Planning Screen)	Project may not conform to agency statutory requirements and may not be permitted. Project modification or evaluation of alternatives is required before advancing to the LRTP Programming Screen.	Community strongly opposes the project. Project is not in conformity with local comprehensive plan and has severe negative impact on the affected community.
5	Issue Resolution (Programming Screen)	Project does not conform to agency statutory requirements and will not be permitted. Issue resolution is required before the project proceeds to programming.	Community strongly opposes the project. Project is not in conformity with local comprehensive plan and has severe negative impact on the affected community.
	No ETAT Consensus	ETAT members from different agencies assigned a different degree of effect to this project, and the ETDM coordinator has not assigned a summary degree of effect.	
	No ETAT Reviews	No ETAT members have reviewed the corresponding topic for this project, and the ETDM coordinator has not assigned a summary degree of effect.	

## Project-Level Hardcopy Maps

No Project-Level Hardcopy Maps Available.